

# 13 Major Nodes

## 13.1 Introduction

There are three Major Node Character Areas in Mississauga:

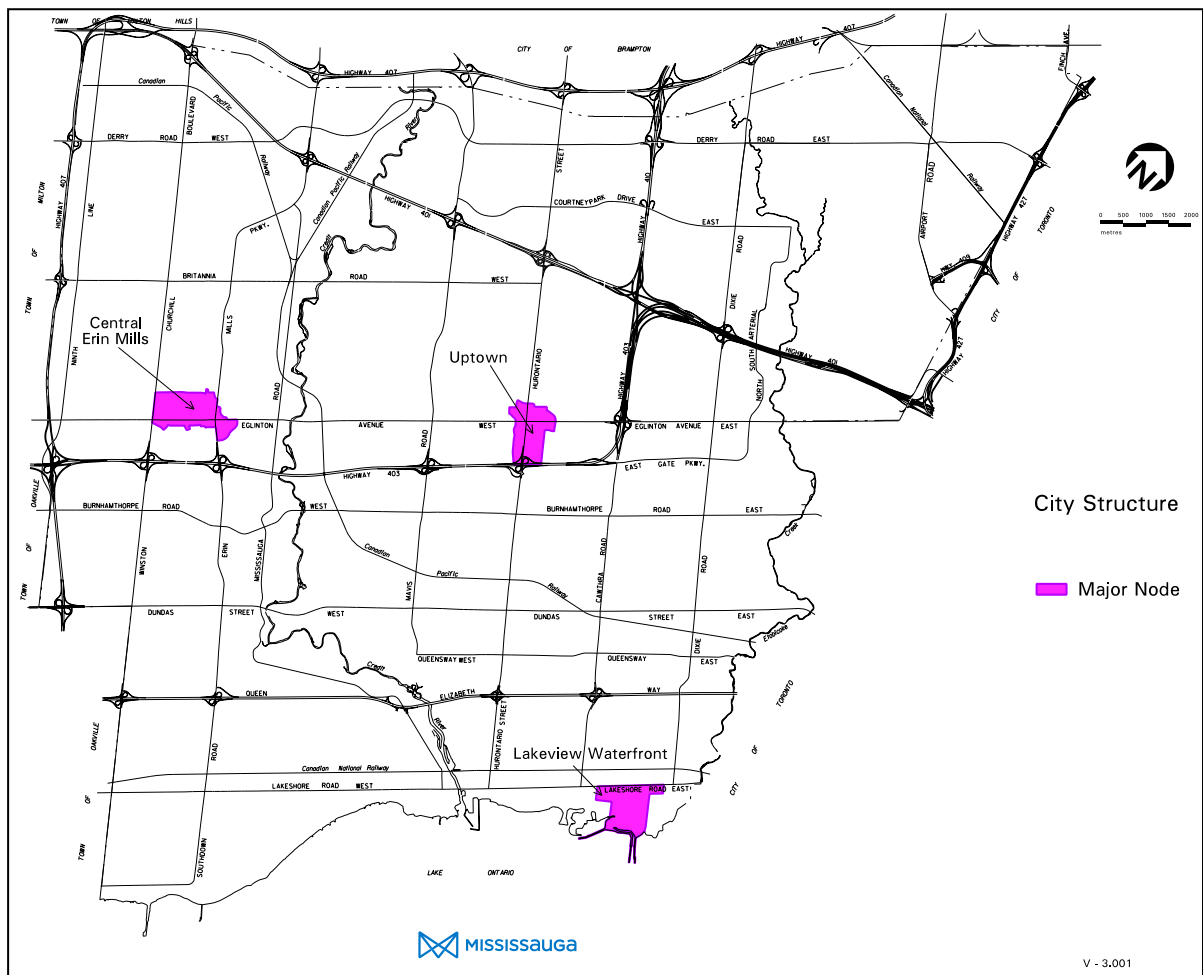
- Central Erin Mills;
- Lakeview Waterfront; and
- Uptown.

This section identifies the modifications to the General Land Use designations in Chapter 11 that apply to all Major Nodes.

## 13.1.1 General

13.1.1.1 Proponents of development applications within a Major Node may be required to demonstrate how the new development contributes to the achievement of the residents and jobs density target and the population to employment ratio.

13.1.1.2 For lands within a Major Node, a minimum building height of two storeys to a maximum building height of 25 storeys will apply, unless Character Area policies specify alternative building height requirements or until such time as alternative building heights are determined through the review of Character Area policies.



Map 13-1: City Structure – Major Nodes

13.1.1.3 Proposals for heights less than two storeys, more than 25 storeys or different than established in the Character Area policies will only be considered where it can be demonstrated to the City's satisfaction, that:

- a. an appropriate transition in heights that respects the surrounding context will be achieved;
- b. the development proposal enhances the existing or planned development;
- c. the City Structure hierarchy is maintained; and
- d. the development proposal is consistent with the policies of this Plan.

### **13.1.2 Residential**

13.1.2.1 The Residential Low Density I and Residential Low Density II designations will not be permitted, except for lands designated Low Density I and Residential Low Density II at the time this Plan comes into effect.

13.1.2.2 Notwithstanding the Residential Medium Density policies of this Plan, the following additional uses will be permitted:

- a. low-rise apartment dwellings.

### **13.1.3 Mixed Use**

13.1.3.1 Notwithstanding the Mixed Use policies of this Plan, the following additional uses will be permitted in the Central Erin Mills and Uptown Major Node Character Areas:

- a. Major office.

### **13.1.4 Office**

13.1.4.1 Notwithstanding the Office policies of this Plan, the following additional uses will be permitted:

- a. Post-secondary educational facilities.

### **13.1.5 Motor Vehicle Commercial**

13.1.5.1 The Motor Vehicle Commercial designation will not be permitted, except for lands designated Motor Vehicle Commercial at the time this Plan comes into effect.

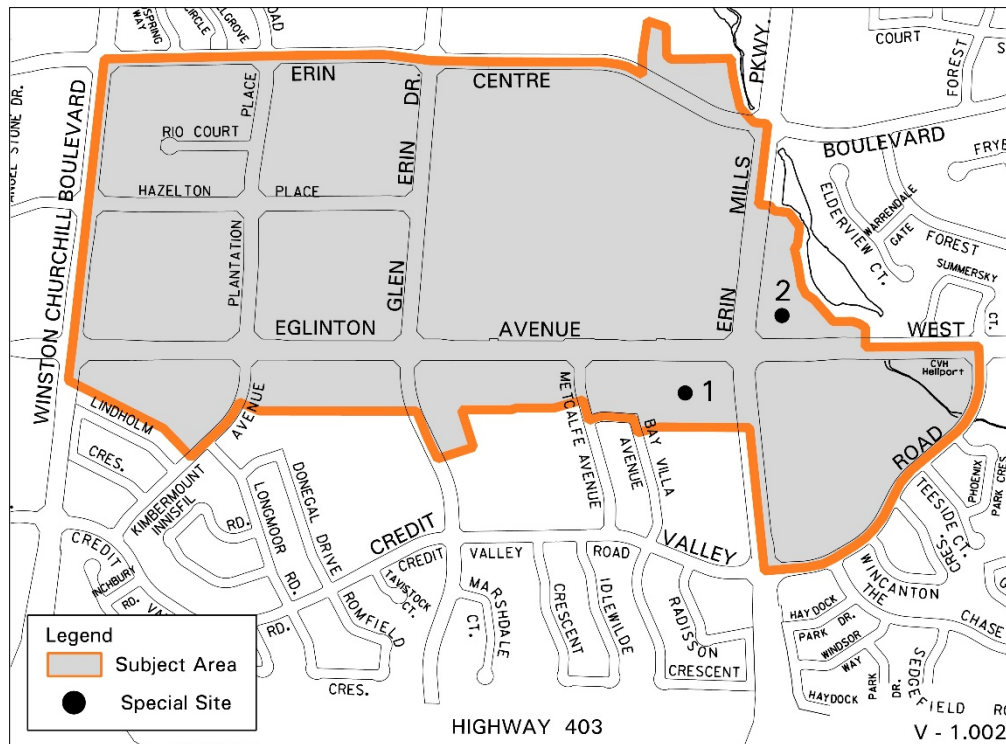
### **13.1.6 Business Employment**

13.1.6.1 The Business Employment designation will not be permitted.

### **13.1.7 Industrial**

13.1.7.1 The Industrial designation will not be permitted.

## 13.2 Central Erin Mills



Map 13-2: Central Erin Mills Major Node Character Area

### 13.2.1 Introduction

The Central Erin Mills Major Node has a concentration of retail and service commercial uses, community facilities and transit facilities that serve the existing and planned uses within the Node and the broader regional community.

Erin Mills Town Centre is a two storey indoor mall surrounded by large surface parking lots and vacant parcels of land. Other types of retail and service commercial uses located in the Node include single storey and large format stores and strip plazas. It is anticipated that the Erin Mills Town Centre will remain as the retail anchor of the Node. However, the surface parking areas serving the mall, vacant lands and the single storey retail uses have development and intensification potential. The existing transit facilities will remain with plans to improve customer amenities.

The Node is to evolve into a healthy sustainable complete community with:

- its role as the centre of community life for persons of all abilities, incomes and ages preserved and enhanced through the provision of amenities, facilities, employment, social spaces and an attractive public realm;
- its planned function as a focal point for retail and service commercial uses, community facilities and bus facilities retained;
- the provision of a mixture of residential built forms and tenures suitable to a variety of income levels and household types including below-market priced housing;
- active transportation modes that are prioritized within the Node and connect to the surrounding residential neighbourhoods;

- public transit that is prioritized over vehicular traffic and connects to the surrounding regional community;
- densities and a mix of uses that allow people to meet many of their needs locally and within walking distance;
- an attractive and well-connected built environment that promotes physically active lifestyles; and
- environmentally resilient development that includes the use of **stormwater best management practices and green infrastructure**.

## 13.2.2 General

13.2.2.1 Community infrastructure within the Node should be retained and, where appropriate, enhanced to meet the needs of the existing and planned residential population of the Node and surrounding neighbourhoods.

13.2.2.2 Public and private elements, including cultural uses, public art and the design of buildings and spaces should be unified and create a sense of place that is accessible, age friendly, comfortable and welcoming at all times of the year and contributes to the identity of the Node.

13.2.2.3 Parkland should be designed and located to create a central focus for the Node. Parkland may also provide gathering spaces and linear connections throughout the Node, to existing open spaces, commercial developments, community facilities and to surrounding neighbourhoods.

13.2.2.4 Playgrounds should be provided within an unobstructed 400m walking distance from residential areas within the Node.

13.2.2.5 Privately owned publicly accessible space that enhances and connects the public open space system is encouraged.

13.2.2.6 **Tactical urbanism** is encouraged to enliven the Node on a temporary basis or to test ideas for long term changes.

13.2.2.7 Transit infrastructure and service within the Node should be retained and, where appropriate, enhanced to meet the needs of the ridership demand of the Node in alignment with MiWay standards.

## 13.2.3 Height and Density

13.2.3.1 A minimum building height of three storeys and a maximum building height of 25 storeys will apply. Buildings without a residential component may have a minimum height of one storeys.

13.2.3.2 In order to guide the form, massing and density of proposed buildings, individual properties will be limited to a **floor space index (FSI)** maximum of 4.0.

13.2.3.3 A gross density of between 200 and 300 residents and jobs combined per hectare measured across the Node will be achieved.

13.2.3.4 A combination of residential and employment uses are encouraged

## 13.2.4 Urban Design

13.2.4.1 Development will provide for a range of building types and heights, including mid-rise buildings to create diversity of urban form and housing choice.

13.2.4.2 Buildings will be designed and located to:

- a. frame and animate streets and public spaces;
- b. create a coherent built environment;
- c. provide setbacks along internal roads that provide a generally consistent building edge and relate to the scale of other buildings and width of roads;
- d. provide reduced building setbacks on internal roads with grade-related retail and service commercial uses where appropriate;

- e. provide significant and generally consistent setbacks along collector and arterial streets to promote landscape boulevards that enhance the pedestrian experience; and
- f. create a street-wall along collector and arterial streets with appropriate openings for street-facing courtyards, where applicable, that provide usable landscape areas for residents and pedestrians.

13.2.4.3 **Tall buildings** should have **podiums** and be located along arterial or collector roads or near transit facilities.

13.2.4.4 In order to promote adequate natural light, sky views and privacy, the following will be required:

- a. appropriate transitions between buildings, to open spaces and to adjacent neighbourhoods; and
- b. a minimum separation distance of 30 metres will be provided between the tower portion of **tall buildings** to prevent clustering.

## 13.2.5 Residential Uses

13.2.5.1 Residential development permitted by any land use designation will include:

- a. a minimum 10 percent of housing units that are below-market for each development application proposing more than 50 residential units. This will be comprised of units targeted for a range of middle income households. Approximately half of these units will be larger, family-sized dwellings containing more than one bedroom.

For the purposes of this section:

- middle income is defined as Mississauga households with annual earnings between the lowest 40 to 60 percent of income distribution
- below-market ownership housing means housing for which the purchase price results in annual accommodation costs which do not

exceed 30 percent of gross annual household income

- below-market rental housing means a unit for which the rent does not exceed 30 percent of gross annual household income

13.2.5.2 Affordable housing for low income households will be encouraged. It is recognized that affordable housing provision is subject to landowners being able to secure access to adequate funding and collaboration with the Region of Peel as Service Manager for subsidized housing.

13.2.5.3 Reduced parking requirements will be considered for the below-market and affordable housing units described in policies 13.2.5.1 and 13.2.5.2 as an incentive to encourage their development.

13.2.5.4 The below-market housing units described in Policy 13.2.5.1 are to be comprised of a mix of both below-market rental and below-market ownership housing when considered across the Node. Individual development applications are encouraged wherever possible to include a mix of both below-market rental and below-market ownership housing.

13.2.5.5 Land conveyance to a non-profit housing provider such as the Region of Peel will be considered in lieu of the direct provision of some or all of the below-market housing units described in Policy 13.2.5.1. Land parcel size, configuration, location, estimated unit yield and adherence to all other policies of this Plan will be included in this consideration.

13.2.5.6 Any existing below-market rental housing units that are retained under the provisions of the City's Rental Housing Protection By-law will count towards the below-market housing unit requirements described in Policy 13.2.5.1.

13.2.5.7 Residential built forms, unit types and sizes will accommodate the needs of a diverse population. This includes people at all stages of life and ability, particularly older adults, families and those with special needs.

13.2.5.8 Notwithstanding the Residential Medium Density policies of this Plan, mid-rise buildings are also permitted.

### 13.2.6 Mixed Use Designation

13.2.6.1 Lands designated Mixed Use will provide a variety of retail and service commercial uses that meet the needs of everyday living for people residing and working within the Node and in the surrounding neighbourhoods.

13.2.6.2 Retail and service commercial uses should be located to animate streets and public spaces.

13.2.6.3 Official plan amendments for the redesignation of lands designated Mixed Use may be considered provided the planned function of the non-residential uses is maintained.

13.2.6.4 Redevelopment that results in a loss of retail and service commercial floor space may be permitted if it can be demonstrated that the planned function of the existing non-residential component will be maintained.

13.2.6.5 For the purposes of the policies in this section, maintenance of the non-residential planned function of the Mixed Use designation means:

- a. the role of the Major Node in the City Structure hierarchy is maintained;
- b. community facilities and gathering space functions of the floor space that would be lost, if any, are maintained or replaced with similar facilities and functions;
- c. a significant concentration of convenient, easily accessible retail and service commercial uses that meet the needs of the local population is maintained;
- d. commercial opportunities, such as office and institutional jobs, are encouraged; and
- e. a grocery store use is maintained within the Node

13.2.6.6 Expansion of retail and service commercial uses are supported. Interim development that

involves additions to or new stand-alone non-residential buildings is permitted as an interim condition. Policies 13.2.8.1, 13.2.9.12, and 13.2.9.13 do not apply to such interim development.

### 13.2.7 Office Uses

13.2.7.1 Redevelopment that results in the loss of office space will be discouraged.

13.2.7.2 Office development may be excluded from the calculation of maximum **Floor Space Index (FSI)** requirements without an amendment to this Plan.

### 13.2.8 Environment

13.2.8.1 To achieve a sustainable community and reduce reliance on fossil fuels, development will be designed to include sustainable measures such as:

- a. designing and orienting buildings to be “solar ready” and to take advantage of passive heating and cooling;
- b. connecting to district energy systems, where available;
- c. using **renewable energy** sources such as solar or geothermal energy;
- d. managing stormwater run-off through innovative methods including **stormwater best management practices** and **green infrastructure**; and
- e. installing green roofs or white roofs.

### 13.2.9 Transportation

13.2.9.1 A well-connected road system will be required in order to provide connectivity and encourage walking and cycling as the predominant modes of transportation within the Node..

13.2.9.2 Blocks will have a maximum perimeter of 520 metres. Roads surrounding blocks will be public and meet City right-of-way and design standards. Permeability will be provided within blocks through means such as pedestrian walkways, cycling paths,

parks and other publicly accessible spaces and vehicular access and servicing routes.

13.2.9.3 Private roads may be permitted instead of a public road to facilitate underground services such as deliveries and parking, subject to the following:

- a. public easements will be required;
- b. required right-of-way widths will be provided; and
- c. appropriate terminus may be required for maintenance and operations where a public road connects with a private road.

13.2.9.4 New roads will connect and align with existing roads in surrounding neighbourhoods.

13.2.9.5 Roads will be designed as complete streets. New roads will be designed to incorporate active transportation and transit infrastructure. Existing arterial and collector roads within and surrounding the Node will be redesigned to incorporate active transportation and transit infrastructure, as appropriate.

13.2.9.6 Landscaping, street furniture and building setbacks will be used to animate roads and create a positive pedestrian, cycling and transit oriented experience.

13.2.9.7 Vehicular access from roads will be coordinated and consolidated in order to minimize driveways and disruption to pedestrians, cyclists and transit.

13.2.9.8 Pedestrian and cycling connections to transit facilities will be prioritized.

13.2.9.9 Transit services will be enhanced as ridership demands increase. Transit stations and facilities will be incorporated into redevelopment plans adhering to MiWay standards.

13.2.9.10 Bicycle parking will be required and will be located throughout the Node and at transit facilities.

13.2.9.11 On-street parking will be provided as appropriate and integrated into the **streetscape** design, balancing the needs of all modes of

transportation and the public realm elements that share the right-of way.

13.2.9.12 Surface parking areas will be replaced by structured parking on a phased basis as redevelopment proceeds. Limited surface parking will be permitted to accommodate matters such as accessibility parking spaces, car-share spaces and pick-up/drop-off point delivery services.

13.2.9.13 Where surface parking is permitted its impact should be minimized by being located at the rear or side of buildings, by using screening and employing low impact development techniques, and by providing pedestrian amenities, where appropriate.

13.2.9.14 Underground parking structures are preferred, however, where above grade parking structures are permitted they will be screened in such a manner that vehicles are not visible from public view and have appropriate direction signage to the structure. Along prominent streets, parking structures should be screened by liner buildings that incorporate a mix of uses between the parking structure and the street.

## 13.2.10 Implementation

13.2.10.1 The need for a development master plan will be determined through a pre-application meeting and in consultation with staff prior to application submission. Redevelopment of the existing Erin Mills Town Centre mall property will require a development master plan. Matters to be addressed by the development master plan may, among other matters, include the following:

- a. delineation of development blocks;
- b. road alignment and characteristics;
- c. distribution of density;
- d. building heights and massing;
- e. land uses and estimated number of people and jobs;
- f. phasing plans;

- g. relationship to surrounding areas;
- h. servicing requirements;
- i. a public realm plan, including parkland;
- j. vehicular and active transportation circulation plan;
- k. vehicular and bicycle parking;
- l. animation at grade, particularly for retail focus areas and in proximity to transit services and along major roads;
- m. environmentally sustainable measures;
- n. existing and proposed transit infrastructure; and
- o. pedestrian Network Plan

13.2.10.6 Applicants are encouraged to prepare development master plans jointly with other landowners in the Node. Where joint plans are not prepared, City staff may consult with other landowners in the Node.

13.2.10.2 The City will work with development proponents to integrate public and private investments to achieve the objectives of this Plan including the provision of a focal point for the residents and employees within the Node and surrounding neighbourhoods, housing choices for people of various income levels and household types and improvements to multi-modal transportation assets and facilities.

13.2.10.3 The City may require a retail and service needs assessment study when development applications propose a reduction of existing commercial space. The study will address how the planned function of the Node as the focal point for retail and service commercial uses and community facilities for existing and planned residents of the Node and surrounding neighbourhoods is retained.

13.2.10.4 Where the redevelopment of retail and service commercial uses is proposed, phasing of development may be required to ensure that the planned function of the Node is maintained during redevelopment.

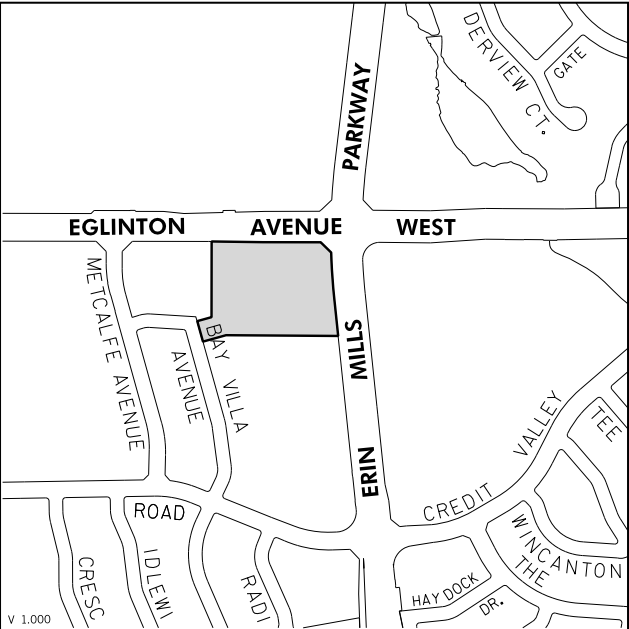
13.2.10.5 When a public road is required or a private road is permitted instead of a required public road, development will occur by way of plan of subdivision to secure the location and size of development blocks and the alignment of roads.



**13.2.11 Special Site Policies**

There are sites within the Character Area that merit special attention and are subject to the following policies.

**13.2.11.1 Site 1**



13.2.11.1.1 The lands identified as Special Site 1 are located at the southwest corner of Eglinton Avenue West and Erin Mills Parkway.

13.2.11.1.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. a total maximum **floor space index (FSI)** of 3.2 will be permitted;
- b. townhouses will be permitted; and,
- c. a maximum of 4000 m<sup>2</sup> of retail commercial and office space will be permitted.

**13.2.1.2 Site 2**

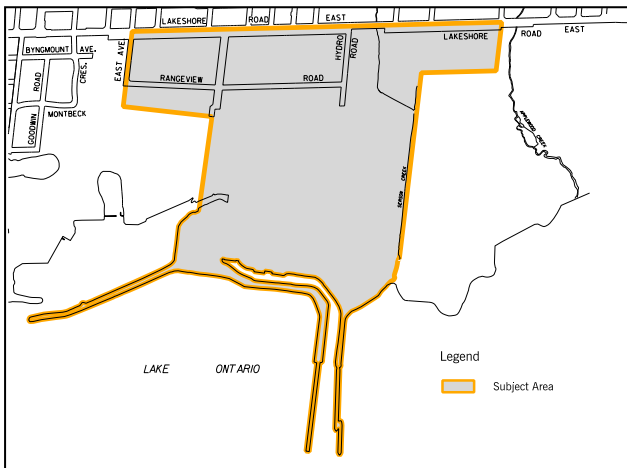


13.2.11.2.1 The lands identified as Special Site 2 are located at the northeast corner of Eglinton Avenue West and Erin Mills Parkway.

13.2.11.2.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. a total maximum **floor space index (FSI)** of 3.4 will be permitted.
- b. Policies of Sections 13.2.1 to 13.2.10 will not apply.

## 13.3 Lakeview Waterfront



Map 13-3.1: Lakeview Waterfront Major Node Character Area

The Lakeview Waterfront Major Node Character Area (“Lakeview Waterfront”) policies elaborate on, or provide exceptions to the policies or schedules of the Plan. In the event of conflict with the Plan, the Lakeview Waterfront policies take precedence.

### 13.3.1 Inspiration Lakeview Master Plan

The Lakeview Waterfront policies are based on the Inspiration Lakeview Master Plan (“Master Plan”), dated June 2014, which was first inspired by a citizen driven project known as the “Lakeview Legacy.” Creating the Master Plan was a collaborative process with community and stakeholder input including representation from the Province of Ontario and Ontario Power Generation (OPG) through a series of consultation events beginning in 2010. The outcome of the process culminated in a collective community vision and a Master Plan document that visualizes the transformation of the Lakeview Waterfront area from an industrial area into a new mixed use waterfront community.

### 13.3.2 Historical and Current Context

The Lakeview lands are an important part of Mississauga’s history. Prior to nineteenth century settlement by the Europeans, the area was home to

indigenous peoples for thousands of years. The last indigenous peoples to occupy and live off the lands were the Mississauga of the Anishinabek Nation. Over the past two hundred years, the site has been transformed from rural pastoral lands to an aerodrome, rifle range, wartime barracks, small arms manufacturing facility and postwar temporary housing, to its forty-year lifespan as the OPG coalburning Lakeview Generating Station and current light industrial uses.

Lands surrounding the Lakeview Waterfront include:

- to the east - the G.E. Booth Wastewater Treatment Facility (WWTF);
- to the west - the Lakeview Water Treatment Facility (LWTF) and residential neighbourhood;
- to the north - residential neighbourhood and mixed uses along the frontage of Lakeshore Road East; and
- along the waterfront - parkland (Douglas Kennedy Park, Lakefront Promenade, and A.E. Crooks Parks).

In addition, the Lakeview Waterfront Connection project will create a new naturalized waterfront area extending from the former OPG lands to Marie Curtis Park. New conservation and wetland areas will provide a migratory stop over for birds and butterflies, and a waterfront trail will be constructed along the shoreline for public access.

The existing road network consists of the following east-west road connections: Lakeshore Road East and Rangeview Road. The north-south road connections are: East Avenue, Lakefront Promenade, and Hydro Road.

Given current and historic uses, there is a potential for contamination in some areas of the Lakeview Waterfront lands. All future development applications will address any contamination issues and appropriate mitigation.

### 13.3.3 Vision

The Vision for the Lakeview Waterfront area is to be a model green, sustainable and creative community on the waterfront. It will be planned as a mixed use community with a vibrant public and private realm including generous open spaces, cultural and recreational amenities, and employment opportunities.

#### 13.3.3.1 Guiding Principles

The Vision is based on the following Guiding Principles:

1. Link: connect the city and the water, including the provision of a continuous waterfront park system along the shores of Lake Ontario;
2. Open: open the site with accessible public spaces for all, with a public realm of different sizes and function, working together to provide a distinctive cultural and ecological community landscape. Create green, public open spaces with enhanced streetscapes;
3. Green: promote a green sustainable innovative model community that may include integrated, water features that provide aesthetic, pedestrian connections and stormwater functions in both the public and private realm (e.g. water themed open spaces, walkways, and stormwater spines). Explore opportunities for innovative waste/recycling collection (e.g. vacuum collection) and energy production systems that promote conservation;
4. Vibrant: create a mixed use community, affordable and welcoming to all, including cultural uses, housing, retail, office and community amenities. Develop an urban village at a human-scale, near the waterfront that will be a focal point for the community. Provide a range of building typologies with an emphasis on mid-rise development, orient buildings for optimal exposure, microclimate and maximize at grade light. Enable improved health and wellbeing through provision of affordable and accessible public transportation and housing;
5. Connect: provide multiple ways to get around - walk, cycle, transit and vehicles. Design a safe, convenient mobility system that encourages all transportation modes and innovative parking solutions. A new street and block pattern including multi-use pathways and mews will connect various neighbourhoods and precincts and create a permeable community. Enhanced transit will bring residents, employees, and visitors into the area and support long term sustainability and vitality;
6. Destination: create a special place to draw visitors where people can enjoy cultural areas with unique venues, waterfront attractions and opportunities for expression. Provide incubator space to promote cultural and creative industries;
7. Remember: commemorate history while creating a new legacy. This will be reflected in public art and other opportunities to interpret the area's history; and
8. Viable: balance public and private investment to be economically sustainable. Explore opportunities for flexible and creative approaches to infrastructure planning. Encourage and facilitate safe redevelopment to be protective of human health and the environment to ensure the environmental conditions are suitable for the proposed use.



Figure 1: Eight guiding principles developed with the community through City initiated Inspiration Lakeview visioning.

Provide for a sustainable economic centre that will attract people. Promote a smart community including the use of innovative technology (e.g. fibre optic network), green industry that incorporates research and development type jobs, and a post-secondary institutional campus.

### 13.3.4 Direct Growth

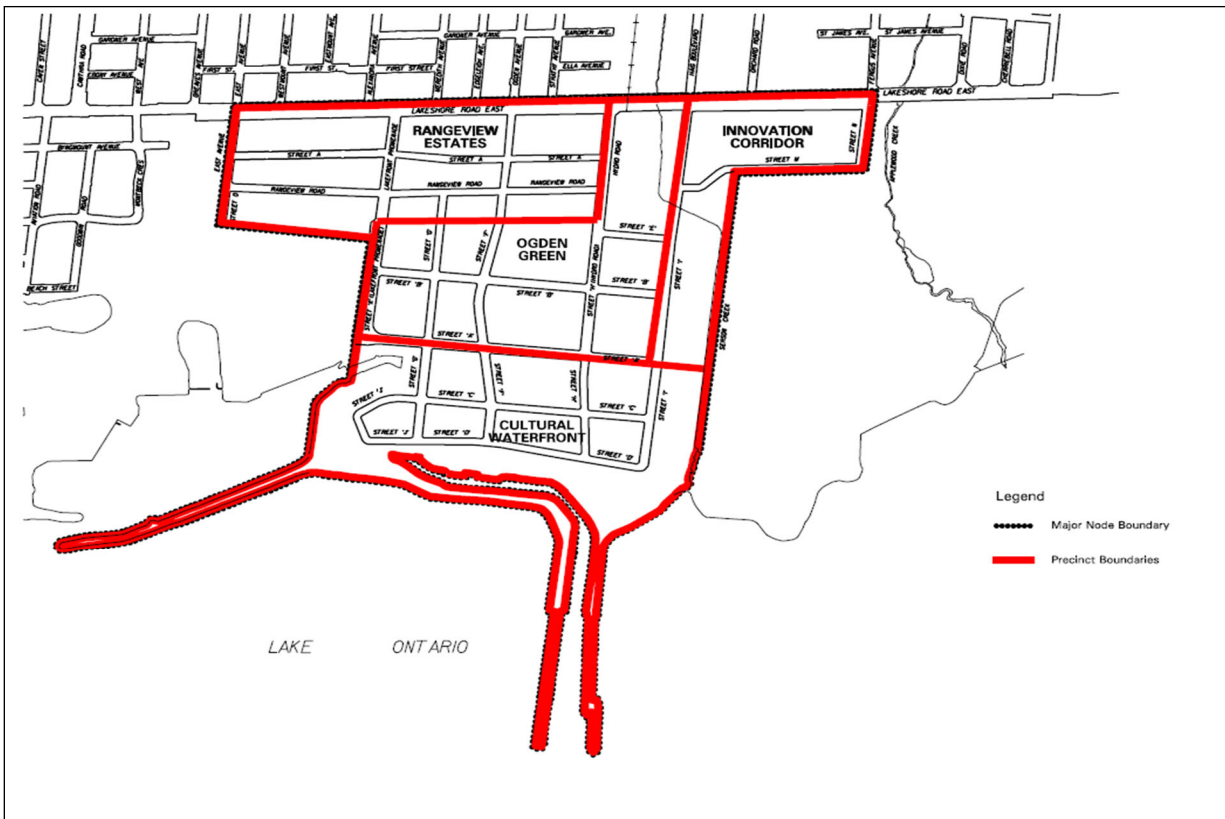
The Lakeview Waterfront is a Major Node and is intended to accommodate a variety of housing, employment, cultural activities, and an extensive open space network that provides access to Lake Ontario.

The lands adjacent to Lakeshore Road East are part of a planned higher order transit corridor, with a future enhanced transit route also planned to extend into the site to support a transit oriented community. Lands adjacent to Lake Ontario are within the City's Green System and will play an important role in the ecological, economic, and social well-being of the area.

The area is divided into precincts in order to reflect differences in the planned function and character and consists of: Rangeview Estates; Ogden Green; the Innovation Corridor; and the Cultural Waterfront; as shown on Map 13-3.2: Lakeview Waterfront Major Node Character Area Precincts.

The Major Node, among other things, is intended to:

- be an area of intensification including a mix of uses such as housing, retail, office, culture, institutional, and recreational uses;
- provide opportunities to draw people from a broader area to take advantage of unique uses such as cultural space, innovative employment and institutional uses and waterfront activities along the waterfront;
- achieve a targeted gross density between 200 and 300 residents plus jobs combined per hectare; and strive to reach a population to employment ratio of 2:1 across the entire Major Node;
- provide a range of building typologies predominately of a mid-rise in height;
- provide a pedestrian oriented environment and promote **active transportation** and ensure transit is convenient;
- provide a variety of housing choices including affordable, assisted and special needs; and
- strive to be a sustainable community and incorporate green development standards such as Leadership in Energy and Environmental Design (LEED) or other custom green development standards.



Map 13-3.2: Lakeview Waterfront Major Node Character Area Precincts.

Approximately 99 hectares in size, the area employs 1,500 people (2017). At full build out, the targeted population and number of jobs is approximately

15,000 to 22,000 people and 7,000 to 9,000 jobs to achieve a 2:1 population to employment ratio.

13.3.4.1 Development master plans will confirm the planned population and employment targets and

	<b>2017</b>	<b>Planned Target<sup>1</sup></b>
Land Area <sup>2</sup> (ha)	99 ha	99 ha
Population	0	15,000 -22,000 <sup>3</sup>
Employment	1,500 <sup>4</sup>	7,000 – 9,000 <sup>5</sup>
Population Plus Jobs per hectare	15	200 – 300 residents and jobs
Population to Employment Ratio	0:1	2:1

<sup>1</sup>Planned targets to be confirmed through further study (e.g. transportation study, Innovation Corridor study)

<sup>2</sup>Land area is a gross figure and includes everything within the defined boundary such as bodies of water, roads, and all other land uses.

<sup>3</sup>Population target is from the Inspiration Lakeview Master Plan (2014) and adjusted to account for a maximum unit capacity of 10,500.

<sup>4</sup>2017 Employment figures from City of Mississauga, Mississauga Employment Survey (2017)

<sup>5</sup>Employment target is from the Inspiration Lakeview Master Plan (2014)

Figure 2: LAKEVIEW WATERFRONT MAJOR NODE, STATISTICS AND TARGETS SUMMARY

housing unit distribution by built form that respects the overall vision for the area.

13.3.4.2 Development may be phased to ensure that growth is managed responsibly and new planned infrastructure is in place.

13.3.4.3 If satisfactory arrangements for the implementation of enhanced transit and transportation demand management (TDM) measures are not made, development may be phased.

### 13.3.5 Value the Environment

The Lakeview Waterfront is planned to be an innovative, green model community that incorporates sustainable best practices for buildings and neighbourhoods.

Some of the innovative and sustainable elements may exceed typical development standards, and will require stakeholder collaboration, potential coordination across multiple properties (e.g. district energy, integrated stormwater management throughout public realm), and innovative financing arrangements.

Given current and historic uses the extent of remediation, remains unknown. Remediation will be addressed through the development review process.

#### 13.3.5.1 Living Green

13.3.5.1.1 To achieve a sustainable community, development will be designed to include sustainable measures such as:

- orienting buildings to be “solar ready” to take advantage of passive heating and cooling;
- connecting to district energy systems, where available;
- using **renewable energy** sources such as solar or geothermal energy or deep water source cooling;

- managing run-off through innovative methods in keeping with **stormwater best management practices**;
- naturalizing landscapes with native, noninvasive species;
- planting trees;
- installing green roofs or white roofs;
- supporting urban agriculture;
- preventing and reducing pollution; and
- mitigating the impact of development on sensitive land uses.



Figure 3: An example of a stormwater management approach in Portland, Oregon.

13.3.5.1.2 Development will incorporate sustainable measures in their design and will strive to meet LEED or other custom green development standards for all buildings.

13.3.5.1.3 The development of a district energy system will be encouraged in the area. Where a district energy system cannot be provided, all development will be encouraged to include on-site renewable or alternative energy systems.

13.3.5.1.4 New land uses may require one or more Records of Site Condition prior to site development, in accordance with Part XV.1 (Records of Site Condition) of the *Environmental Protection Act*.

13.3.5.1.5 A Financial/Sustainability Strategy will be required to address how infrastructure, new buildings and public and private realm can be designed to achieve a model sustainable community. The strategy will address the necessary mechanisms to implement these features.

### 13.3.5.2 Green System

An interconnected network of open spaces will provide linkages, both within and to surrounding areas. This network includes parks, trails, and ***natural hazard lands***.

The current Serson Creek and the Lake Ontario shoreline flood and erosion hazard limits need to be confirmed through future study and/or review of development applications.

13.3.5.2.1 The limits of Serson Creek, as determined through further study, may impact the alignment of future roads in the creek's vicinity.

13.3.5.2.2 Innovative ***stormwater best management practices***, including low impact development techniques, may be integrated with open spaces and pedestrian walkways that will contribute to the overall network of open spaces. The location and design of these open spaces/spines will be determined through a master servicing plan and identified in development master plans.

### 13.3.6 Complete Communities

Lakeview Waterfront is planned as a sustainable new community with public access to the waterfront. The area is anticipated to draw people from within and beyond Mississauga.

A mixed use focal point is planned for the southeast quadrant of the community, where cultural commercial, office, institutional, and recreational open spaces converge. The area is envisioned as a vibrant, pedestrian oriented space with outdoor cafes, unique retail, attractive street furniture and sitting areas. The area may accommodate special events and uses, waterfront attractions, and art and culture incubator space.

Street 'I' (extension of Ogden Avenue) and the adjacent linear public open spaces represent an important north-south green corridor. This central area is where a public realm of different sizes and functions work together to provide a distinctive landscape that is safe, attractive and inviting with direct access and views to the waterfront. The area provides opportunities for physical fitness, leisure, and social interaction in order to promote healthy and active lifestyles. Adjacent land uses and built form will support this key element of the community.

Lakefront Promenade, Hydro Road/Street 'J', and Street 'K' (extension of Haig Boulevard) are important corridors that provide access from Lakeshore Road East to the waterfront and will incorporate enhanced streetscape.

Activities that benefit from being near the shoreline, parks and the Waterfront Trail are also envisioned for the area.

13.3.6.1 Affordable housing will be required in accordance with the City's housing strategy.

13.3.6.2 A minimum of 5% of the overall units within the Ogden Green and Cultural Waterfront Precincts will be affordable housing units, or equivalent satisfactory provisions, as deemed acceptable by the City.

13.3.6.3 The City will work in collaboration with the school board(s) to determine the need for educational facilities. The location of these facilities will be determined through the development application process.

13.3.6.4 The City will encourage partnerships and collaboration with the local community, professional artists, arts organizations and creative enterprises to further develop the cultural amenities in the Cultural Waterfront Precinct, including creative industries and commercial opportunities.

13.3.6.5 The Cultural Waterfront Precinct will be the preferred location for cultural uses with the potential for a museum, artist supportive housing and galleries, festival and creative urban spaces, and artist studios. Buildings providing flexible floor plates that are

amenable to a variety of cultural uses and ground level incubator space, including **makerspace** will be encouraged.

13.3.6.6 Development within the Cultural Waterfront Precinct will create a unique identity by providing distinctive architecture, high quality public art and streetscape, and cultural infrastructure and spaces that will reinforce, where appropriate, the history of the site.

13.3.6.7 The City will continue to pursue public uses on the waterfront lands, including parkland and a waterfront trail connection along the water's edge, and public access to the pier. The City will work in collaboration with the Region of Peel and other levels of government and agencies to achieve these public uses.

13.3.6.8 Public parks will front onto a public road to maximize street frontage and accessibility.

13.3.6.9 Publicly accessible private open spaces may be incorporated into developments provided that:

- the design of private open space will integrate seamlessly with the adjoining street network, pedestrian environment and overall open space system; and
- the private open space interfaces with existing and/or proposed development in a legible and cohesive manner.

13.3.6.10 The City, in consultation with Credit Valley Conservation (CVC), will investigate the extent to which the lands along the shoreline can be developed for a vibrant destination space at the water's edge.

### 13.3.7 Multi-Modal City

The Lakeview Waterfront community is designed to encourage multi-modal transportation with emphasis on transit and active transportation, to reduce traffic delays, congestion, energy consumption and pollution. The community will have a highly connected network of streets and routes for active transportation to support walking and cycling.

As the area develops and site specific applications are submitted, the City will monitor implementation of the multi-modal network to ensure transit and active transportation are incorporated and the overall network functions efficiently. As development in the Lakeview Waterfront community progresses, increased traffic delays may be experienced if the complementary improvements and/or investments to the overall network are not made.

The City will continue to work with partners from other levels of government, including Metrolinx and the private sector, to explore sustainable transportation solutions. The area's proximity to existing and expanded all day two-way GO Rail transit service, proposed **higher order transit** along Lakeshore Road East and future enhanced transit into the site will provide increased levels of service in the future.

Future enhanced transit is the provision of a range of transit services and infrastructure based on demand.

As a fully realized community, transit and **active transportation** are intended to be viable alternatives to vehicular use and will help shape and support the future development of the Lakeview Waterfront area.

The existing and future conceptual road network is shown on Schedule 5: Long Term Road Network. A future higher order transit corridor along Lakeshore Road East and a future enhanced transit route extending into the site is identified on Schedule 6: Long Term Transit Network. The Lakeshore Road Transportation Master Plan will examine transportation issues on the corridor including a review of higher order transit needs and any necessary improvements to the transportation system for all modes of travel. Future local roads have been conceptually identified.

Bringing enhanced transit into the site is considered fundamental to implementing the Vision and Guiding Principles for Lakeview Waterfront. An assessment of the preferred transit solution, including its alignment and overall road network, will be subject to further study.



### 13.3.7.1 Road, Transit and Active Transportation Network

13.3.7.1.1 The Lakeview Waterfront area will be developed with a fine-grain network of roads and connections that will support active transportation and create a well connected and healthy community.

13.3.7.1.2 An area-wide transportation study for the Lakeview Waterfront is required that will examine among other things: future enhanced transit including its alignment and design; multi-modal splits between transit, active transportation and vehicle use; TDM; additional roads; and potential traffic infiltration impacts on adjacent neighbourhoods. This study will build upon the work completed for the Lakeshore Road Transportation Master Plan and will make recommendations on infrastructure and density, Figure 4: Lakeview Waterfront Major Node Character Area Future Roads modal splits, and any required phasing of development.

13.3.7.1.3 Development master plans and development applications will demonstrate how the

findings of the area-wide transportation study have been incorporated into site specific development proposal.

13.3.7.1.4 Roads will be designed to provide connectivity within and between precincts in the Lakeview Waterfront area and ensure that adequate road right-of-way widths are maintained for municipal servicing, utilities, and a desirable public realm.

13.3.7.1.5 Streets will be designed to incorporate active transportation and provide views to the waterfront. Lakefront Promenade, Street 'I', Hydro Road/Street 'J' and Street 'K' will be designed with enhanced streetscapes that may include among other things, wide sidewalks, street trees, planting, furniture.

13.3.7.1.6 The City may acquire and protect for a public transit right-of-way for an enhanced transit route (Schedule 6: Long Term Transit Network) where the creation of a public transit right-of-way separate from, adjacent to, or in addition to, a road right-of-way is deemed appropriate.

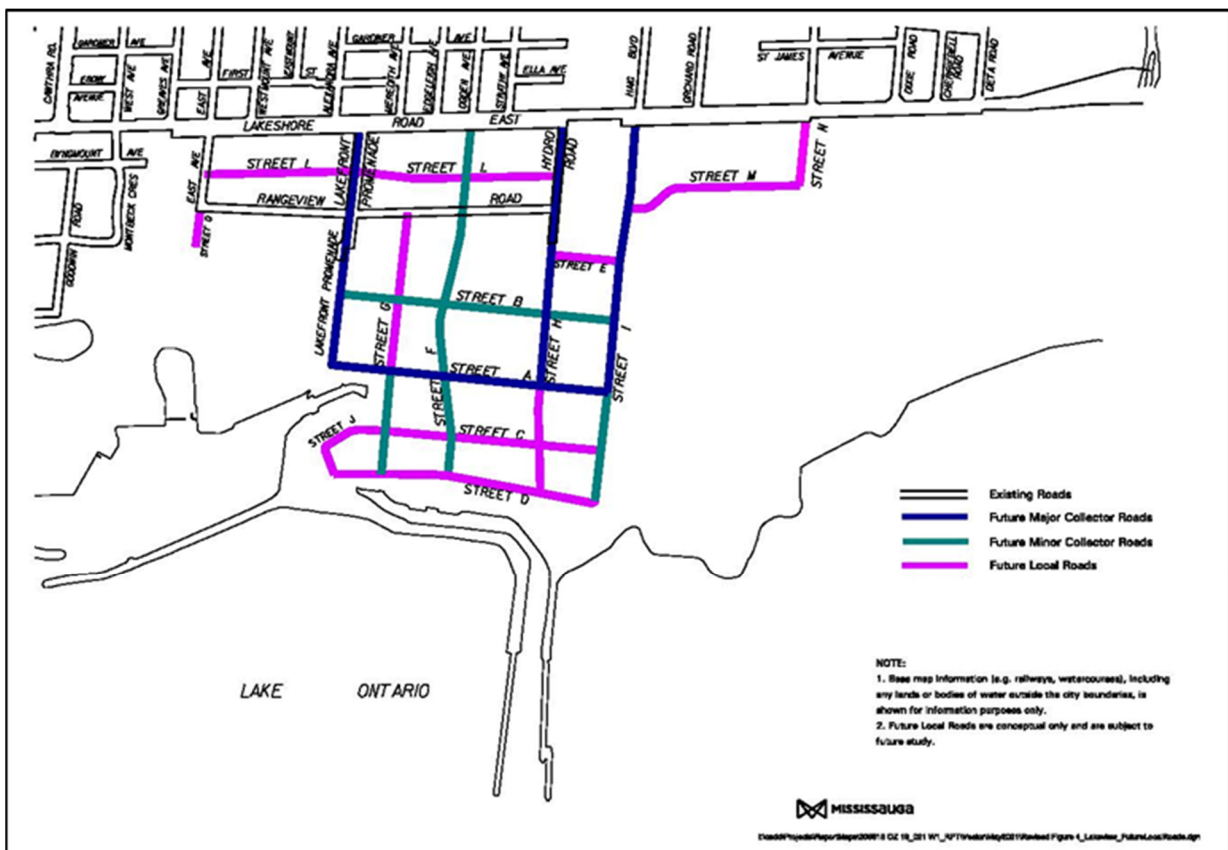


Figure 4: Lakeview Waterfront Major Node Character Area Future Roads

13.3.7.1.7 The City will, through the review of development applications, eliminate and/or consolidate vehicular ingress/egress locations and turning movements to and from Lakeshore Road East and direct traffic towards signalized intersections, where appropriate.

13.3.7.1.8 Development applications will be accompanied by traffic impact studies and/or parking utilization studies that will address, among other things, strategies for limiting impacts on the transportation network such as:

- a. reduced parking standards;
- b. transportation demand management;
- c. transit oriented development and design;
- d. pedestrian/cycling connections; and
- e. access management plan.

13.3.7.1.9 The final detailed alignment of the road network will be incorporated into development master plans and reflected in plan(s) of subdivision.

13.3.7.1.10 Future additions to the road network will be public roads, unless arrangements for private roads are made that are satisfactory to the City. Private roads may be considered subject to the following:

- a. public easements will be required;
  - b. required right-of-way widths will be provided;
- and
- c. appropriate terminus may be required for maintenance and operations where a public road connects with a private road.

13.3.7.1.11 The City may consider alternative road design standards to achieve community design objectives.

13.3.7.1.12 The City may expropriate for roads if necessary, with the costs recovered through landowner cost sharing agreements.

### 13.3.7.2 Connectivity

13.3.7.2.1 Development master plans and development applications will demonstrate how the proposal will contribute to the creation of a permeable and connected community for active transportation (for pedestrians and cyclists) in order to promote healthy communities. Connectivity may be provided in a number of ways, including but not limited to the following:

- small block sizes;
- fine-grain road patterns;
- multi-use paths;
- pedestrian mews;
- frequent intersections; and
- development framing parkland or privately owned publicly accessible open spaces.

13.3.7.2.2 Active transportation connections will be confirmed through the Lakeview Waterfront Transportation Study and development master plan(s).

13.3.7.2.3 Public easements will be required where connections are proposed on private lands. Where appropriate, connections should also accommodate innovative stormwater best management practices including low impact development techniques.



Figure 5: An example of a pedestrian mew.

### 13.3.7.3 Parking

13.3.7.3.1 Parking will be provided as follows:

- a. on-street parking will be provided as appropriate and integrated into the **streetscape** design, balancing the needs of all modes of transportation and the public realm that share the right-of-way;
- b. underground parking will be encouraged on all sites, however, a limited amount of surface parking may be considered on a site by site basis;
- c. underground and/or integrated above grade structured parking will be required for residential development exceeding four storeys and all mixed use developments;
- d. surface parking may be considered for:
  - townhouse dwellings;
  - low rise apartment dwellings not exceeding four storeys;
  - cultural, recreational and institutional uses; and
  - Innovation Corridor Precinct; and
- e. freestanding and above grade structured parking will incorporate elevated design elements (e.g., façade wraps, integrated into buildings). Structures will be compatible with the surrounding area and will be encouraged to

incorporate active uses at ground level in order to reduce negative impacts on the public realm.

13.3.7.3.2 Reduced and/or maximum parking standards may be considered throughout the area, in conjunction with the provision of mixed use developments, enhanced transit and active transportation facilities. The extent of the reduction may be considered through a parking utilization study.

### 13.3.8 Desirable Urban Form

Built form will be human scaled, create places that reinforce a sense of community, foster innovation, and contribute to an improved quality of life. The community is envisioned to:

- have a built form that is predominantly mid-rise in scale;
- provide opportunities for ground related housing (e.g. all types of townhouses);
- permit limited taller buildings that must “earn the sky” through design excellence and protection of skyviews and address sun and wind;
- ensure design excellence that incorporates sustainable design (e.g. LEED or custom green development standards); and
- a connected and well designed open space system that provides opportunities for public gathering and passive outdoor recreation.

#### 13.3.8.1 General Policies

13.3.8.1.1 The distribution of height and density will achieve the following:

- a. a gradual transition to existing adjacent residential neighbourhoods;
- b. reinforce a pedestrian scale along Lakeshore Road East;

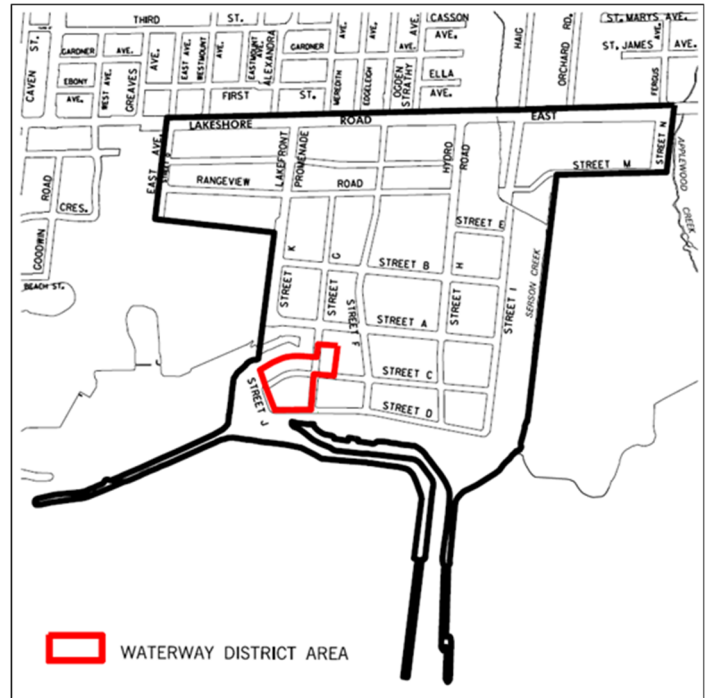
- c. protect and enhance view corridors along Lakefront Promenade/Street 'K', the linear park along Street 'F', Hydro Road/Street 'H', and Street 'I'
- d. greatest heights and densities will be located at the southwestern edge of the community as identified on Map 13-3.3 Waterway District Area, and will include select, architecturally significant buildings, subject to an architectural competition
- e. buildings located in the Waterway District Area will provide an appropriate transition in height to adjacent development in surrounding areas;
- f. provide a pedestrian scale with appropriate step backs that includes a transition towards adjacent parks and the waterfront;
- g. provide appropriate transition between private development and public open space;
- h. ensure permeability and views towards the waterfront; and
- i. ensure a variety of built forms to create a varied skyline that promotes views to Lake Ontario.

13.3.8.1.2 Development within the Ogden Green and Cultural Waterfront Precincts will be guided by the Lakeview Village Design Guidelines, as found in Appendix I of Chapter 13, Major Nodes.

### 13.3.8.2 Development Master Plans

For the purposes of these policies a development master plan is a non-statutory policy document prepared to City Council's satisfaction that further articulates the policies of this Plan at a precinct level. It outlines development principles and guidelines at a more detailed level than the Lakeview Waterfront policies and Mississauga Official Plan. The development master plan provides a link between Official Plan policies, and subsequent plans of subdivision, rezoning and site plan applications.

13.3.8.2.1 Development master plans will provide direction and contain built form guidelines to be prepared to the City's satisfaction, addressing issues including, but not limited to:



Map 13-3.3 Waterway District Area

- a. distribution of height and density that ensures the envisioned range and mix of built form typologies are provided including townhouses, mid-rise, and taller buildings (as shown on Table 1: Distribution of Housing and Unit Targets by Precinct);
- b. design criteria including appropriate setbacks, stepbacks, height, massing and location of buildings. These measures are intended to:
  - reduce any “wall effect;”
  - define gateway locations and street edges;
  - provide skyviews and sunlight; and
  - ensure a pedestrian friendly environment;
- c. adequate spatial separation between taller buildings (over 8 storeys) to address overcrowding of skyline and the potential loss of light and skyviews, protection of view corridors, privacy, and overlook of occupants;
- d. use of public and private open spaces to accommodate innovative **stormwater best management practices**, including low impact

- development techniques, reinforce view corridors, enhance the aesthetic quality of the area, increase the tree canopy, and enhanced connections (i.e. connections to the adjoining street network);
- e. how proposed developments incorporate green development standards and sustainable infrastructure and implement the directions/recommendations of the Financial/Sustainability Strategy;
  - f. final alignment of future roads with identified right-of-way widths;
  - g. **streetscape** and upgraded boulevard treatments that provide appropriate setbacks to reflect planned function, minimize vehicular access points, create an attractive public and private realm and provide opportunities for tree planting;
  - h. provision of public access and protection of views to Lake Ontario;
  - i. provision of public art in locations that enhance the built environment and enrich the culture and history of the community;
  - j. the location and amount of space dedicated to, community infrastructure, and cultural facilities including creative industry incubator space; and
  - k. interface of development between precincts; and

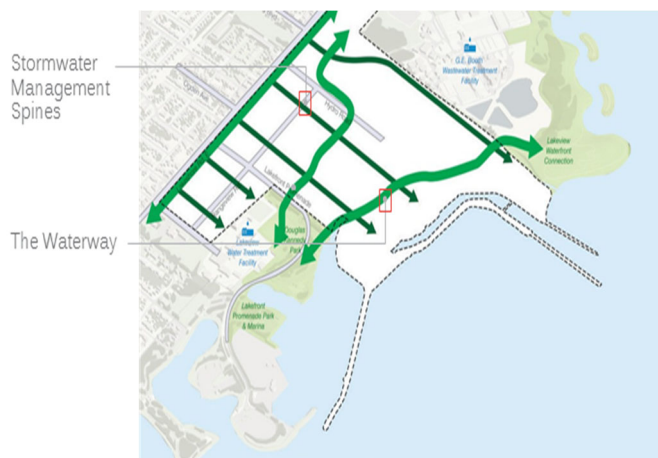


Figure 6: An innovative approach to stormwater management - series of north-south and east-west spines adds natural elements to the public and private realm while providing an important water collection function.

- l. phasing of development within and between precincts including the use of one or more plans of subdivision to ensure the provision of adequate services.

13.3.8.2.2. The Lakeview Waterfront Development Master Plan represents the development master plan for the Ogden Green, Cultural Waterfront and Innovation Corridor Precincts. Development within these precincts will be in keeping with the layout, built form and principles outlined in the Lakeview Waterfront Development Master Plan.

### 13.3.8.3 Precincts

Lakeview Waterfront will be predominantly mid-rise in form but will include some lower and taller buildings to provide a variety of building types.

13.3.8.3.1 Permitted building heights will range as follows:

- townhouses (all types) ranging from 2 to 4 storeys;
- low-rise apartment buildings up to 4 storeys;
- mid-rise apartment buildings from 5 to 8 storeys; and
- taller buildings from 9 to 15 storeys.

13.3.8.3.2 Notwithstanding policy 13.3.8.3.1, a limited number of buildings from 16 to 25 storeys in height may be permitted in the Rangeview Estates and Ogden Green Precincts, subject to a Height Study(s). Development master plans will demonstrate how a range of heights from 9 to 15 storeys will be maintained across these precincts,

13.3.8.3.3 Notwithstanding policy 13.3.8.3.1, one building with a maximum height of 28 storeys will be permitted within the Ogden Green Precinct at the northeast corner of Street 'A' and the central north-south park, generally in keeping with the Lakeview Waterfront Development Master Plan.

13.3.8.3.4 Notwithstanding policy 13.3.8.3.1, three taller buildings may be permitted in the Waterway

District Area (Map 13-3.3) with the tallest building permitted to a maximum height of 40 storeys. The buildings will be in keeping with the configuration identified in the Lakeview Waterfront Development Master Plan.

13.3.8.3.5 A Height Study will address among other things:

- appropriate height;
- floor plate size;
- number and location of buildings;
- appropriate separation distance including siting to preserve view corridors;
- transition to adjacent development; and
- variations in height to create visual interest.

13.3.8.3.6 Development within the Ogden Green and Cultural Waterfront Precincts will be in accordance with the minimum and maximum height limits as shown on Map 13-3.4. The appropriate heights within these ranges will be determined by the other policies of the Lakeview Waterfront Major Node Character Area.

13.3.8.3.7 Podiums of tall buildings should provide appropriate transition to adjoining low to mid rise buildings

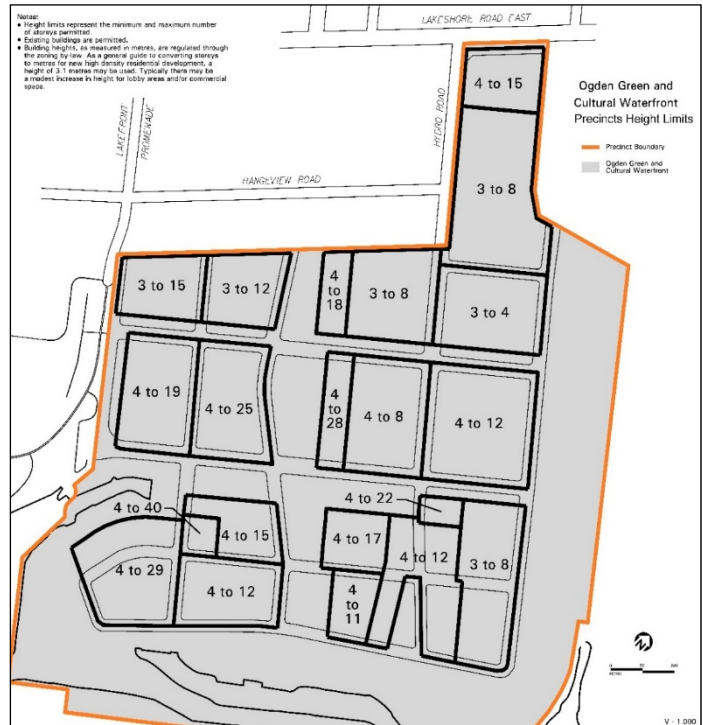
13.3.8.3.8 Table 1: Distribution of Housing and Unit Targets by Precinct provides a guide to how housing units by type and precinct are to be distributed across the Lakeview Waterfront area. Development master plans will be consistent with Table 1.

13.3.8.3.9 To increase the amount of public and private amenity space, heights in excess of the limits identified in Table 1 and the policies of this plan may be considered subject to demonstration of the following:

- the total maximum units will not exceed 11,750 and will be distributed within each precinct as per Table 1;

- taller buildings will be combined with mid-rise and/or low-rise buildings on the same block and will generally be built at the same time; and
- an urban design control document and agreement that is registered on title may be required and will include among other things, phasing, height and distribution of buildings.

Lakeview Waterfront is divided into the following precincts: Rangeview Estates, Ogden Green, Cultural Waterfront and Innovation Corridor Precincts (Map 13-3.2).



Map 13-3.4, Ogden Green and Cultural Waterfront Precincts Height Limits

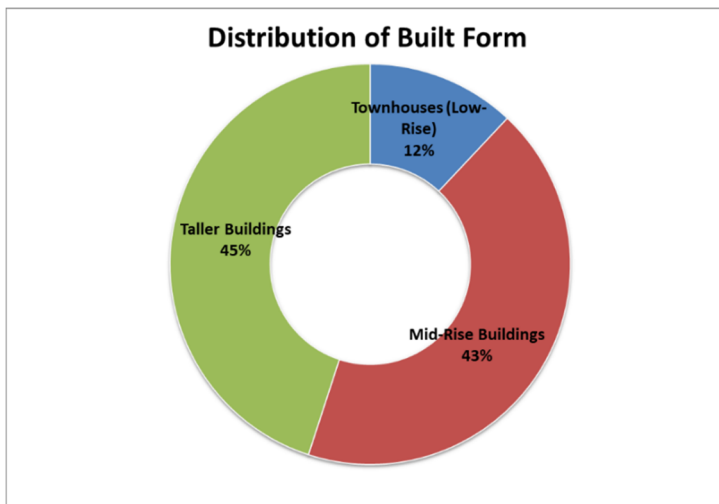


Figure 7: The planned distribution of built form is a combination of townhouses, mid-rise apartments and taller buildings (Table 1)

### Criteria for Taller Building Heights

13.3.8.3.10 Development master plans will identify key locations where taller buildings (9 – 15 storeys), may be considered, including the following:

- in proximity to a **Major Transit Station Area** on Lakeshore Road East taller buildings will be located beyond a mid-rise building;

- at Ogden Avenue and Lakeshore Road East provided the taller building is located beyond a mid-rise building fronting Lakeshore Road East;
- and
- along the future enhanced transit route.
- Along the central north-south park;
- At the eastern terminus of the east-west park adjacent to Street 'A'.

### Rangeview Estates Precinct

The Rangeview Estates Precinct will primarily be a residential neighbourhood with a mix of townhouses and mid-rise buildings. Taller buildings may be considered based on the criteria for taller building heights. Commercial uses will be directed to Lakeshore Road East.

13.3.8.3.11 Mid-rise buildings will be required to front Lakeshore Road East and encouraged along future enhanced transit route and along Street 'I' (extension of Ogden Avenue).

Precinct	Total Residential Unit	Built Form					
		Townhouses (All Types) <sup>1</sup>		Mid-Rise Buildings (5 to 8 storeys)		Taller Buildings (9 to 15 storeys) <sup>2</sup>	
		Number of Units	% of Townhouses (Precinct)	Number of Units	% of MidRises (Precinct)	Number of Units	% of Taller Buildings (Precinct)
Rangeview Estates	3,700	925	25%	1,850	50%	925	25%
Ogden Green, Cultural Waterfront	8,050	480	6%	3,184	40%	4,386	54%
Innovation Corridor	n/a	n/a	n/a	n/a	n/a	n/a	n/a
<b>Total – Lakeview Waterfront Major Node</b>	<b>11,750</b>	<b>1,405</b>	<b>12%</b>	<b>5,034</b>	<b>43%</b>	<b>5,311</b>	<b>45%</b>

<sup>1</sup> Townhouses (all types) and low-rise apartments up to 4 storeys

<sup>2</sup> A limited number of taller buildings from 16 to 25 storeys may be considered in Rangeview Estates and Ogden Green Precincts subject to a Height Study.

A limited number of taller buildings above 25 storeys may be considered in the Waterway District Area subject to a Height Study.

13.3.8.3.12 Commercial uses are permitted along Lakeshore Road East and will be required between Lakefront Promenade and Hydro Road.

13.3.8.3.13 Notwithstanding policy 13.4.8.3.9 freestanding commercial buildings may be less than 5 storeys.

### Ogden Green Precinct

The Ogden Green Precinct is centrally located in the Lakeview Waterfront Community. Linear parks along Street 'A', Street 'B' and Street 'F' provide connections to the waterfront and surrounding uses. Mid-rise and taller buildings will be directed to the future enhanced transit route and along the central north-south park.

13.3.8.3.14 Mid-rise buildings and taller buildings will be permitted along the future enhanced transit route and along the north-south central park.

13.3.8.3.15 Taller buildings should be designed with podiums distinct from the tower in order to emphasize the mid-rise character of the community. The tower component should step back from the podium in order to help achieve this distinction

### Innovation Corridor Precinct

The Innovation Corridor Precinct is at the eastern limit of the Lakeview Waterfront area and is intended to be a high-tech green campus that accommodates office, business employment uses, institutional uses, and research and development activities.

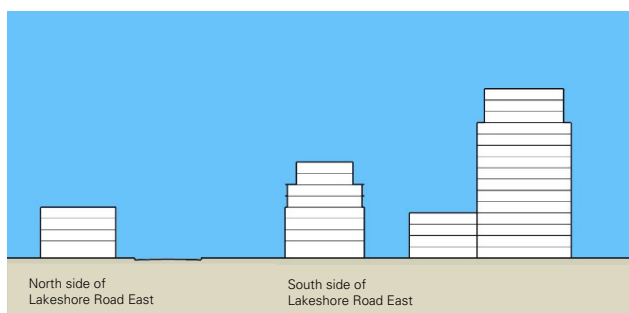


Figure 8: Development in the Lakeview Waterfront along Lakeshore Road East with an 8 storey building and a 15 storey building behind.

13.3.8.3.16 Mid-rise buildings with underground parking or screened parking structures will be encouraged.

### Cultural Waterfront Precinct

The Cultural Waterfront Precinct is located along the Lake Ontario shoreline. This precinct provides direct public access to the waterfront. A broad mix of uses including residential, commercial, institutional, open space, community and cultural uses will create a vibrant character. A mixed use focal point of a pedestrian scale is envisioned with generous public realm including attractive street furniture, patios and public art.

13.3.8.3.17 Notwithstanding policy 13.3.8.3.1, and with the exception of the Waterway District Area, building heights will be kept to a mid-rise form in scale and height in keeping with the Lakeview Waterfront Development Master Plan

13.3.8.3.18 Buildings fronting Street 'D' will have a maximum height of six storeys with stepbacks above the fourth storey.

13.3.8.3.19 Notwithstanding policy 13.3.8.3.1, a 22 storey building will be permitted at the eastern terminus of the central east-west park. The building will be subject to an architectural competition to ensure architectural excellence, appropriate contextual integration and the creation of an appropriate terminating vista along the park.

13.3.8.3.20 Taller buildings should be designed with podiums distinct from the tower in order to emphasize the mid-rise character of the community. The tower component should step back from the podium in order to help achieve this distinction.

13.3.8.3.21 Built form will provide a generous public realm and promote an active and vibrant waterfront respecting the relationship to the water e.g. public access and views.

13.2.8.3.22 Notwithstanding policy 13.3.8.3.1, the Waterway District Area (Map 13-3.3) may permit three taller buildings, with the tallest building permitted to a maximum height of 40 storeys. These



buildings will demonstrate an appropriate transition in height, particularly to the waterfront park along the west and south sides, with the tallest being located the furthest away from the waterfront park. The buildings will be in keeping with the configuration identified in the Lakeview Waterfront Development Master Plan and will provide generous setbacks from the waterfront park.

13.3.8.3.23 An architectural competition will be required for the Waterway District to ensure a high level of architectural excellence and to ensure the sensitive integration of height adjacent to the waterfront park. The architectural competition will take into account the concept derived through the Lakeview Waterfront Development Master Plan.

13.2.8.3.24 The precinct will include:

- a. series of water themed open spaces intended to accommodate a range of uses (e.g., pedestrian streets, outdoor cafes, skating rinks, splash pads and programmable spaces);
- b. buildings with flexible floor plates that are suitable for a variety of cultural activities such as, but not limited to museums, art galleries, studios, markets and incubator space to promote cultural and creative industries; and
- c. a mixed use focal point on the eastern portion of the precinct that provides an active and vibrant area primarily for commercial, cultural, institutional (e.g. post-secondary campus) and recreational activities. This area will be designed as an urban village near the waterfront, with an emphasis on pedestrian oriented development and the public realm.

13.3.8.3.25 Institutional buildings (e.g., postsecondary institutional campus) will be innovatively designed to take advantage of its location along Lake Ontario.

### 13.3.9 Strong Economy

The Lakeview Waterfront area has historically been home to a range of employment uses. The 2015 Municipal Comprehensive Review of Employment

Lands concluded that because of its waterfront location, the area could be better utilized for a mixture of residential, commercial, office and other employment generating land uses at a higher density.

It is recognized that existing businesses may remain in the area until such time as redevelopment occurs. Although existing businesses may relocate, a range of employment uses is essential to achieving the vision for the area. New employment uses are planned for the area and the primary location for these jobs will be in the Innovation Corridor Precinct.

13.3.9.1 Commercial space providing employment opportunities as well as serving residents and people attracted to destination uses in the area, will be directed to the following locations:

- a. Lakeshore Road East between Lakefront Promenade and Street 'K'; and
- b. Cultural Waterfront Precinct.

13.3.9.2 The Innovation Corridor Precinct is intended to be the location of the greatest number of office and light industrial jobs. The amount of space and any required incentives to attract major employers will be determined through the Inspiration Lakeview Innovation Corridor Feasibility Study.

13.3.9.3 The City will work with the Province to attract post-secondary institutional uses to the Lakeview Waterfront area.



Figure 9: The Lakeview Waterfront can provide an active and vibrant waterfront and opportunities for cultural activities such as Vancouver's Granville Island.

13.3.9.4 A study that will identify opportunities to incorporate cultural infrastructure and creative spaces into the community may be undertaken.

13.3.9.5 Strategies to encourage and support employment uses may be pursued including Community Improvement Plans, Bonus Zoning and other incentives.

## **13.3.10 Land Use Designations**

### **13.3.10.1 General**

13.3.10.1.1 Notwithstanding the policies of this Plan, business employment uses will be permitted as they existed on the day these policies come into effect.

13.3.10.1.2 Notwithstanding the land use designation policies, remediation of any contaminated lands will have to be addressed to ensure that it is suitable for the intended uses.

13.4.10.1.3 Precincts are planned to be developed for a range of land uses. Development master plans will identify the appropriate mix of residential and employment uses and have regard for the planned function and character envisioned for each of the precincts and Table 1: Distribution of Housing and Unit Target by Precinct.

13.3.10.1.4 Alternative waste collection infrastructure (e.g. vacuum pipes, chutes, disposal points) will be permitted in all land use designations, A central waste collection terminal associated with an alternative waste collection system will only be permitted in the Business Employment land use designation.

### **13.3.10.2 Residential Medium Density**

13.3.10.2.1 Notwithstanding the Residential Medium Density policies of this Plan, apartment dwellings of a low, mid-rise and taller built form will be permitted up to 12 storeys.

13.3.10.2.2 For lands fronting Lakeshore Road East, Street 'D' or Street 'F', commercial uses will be permitted on the ground level of buildings.

g. transportation facilities;

### **13.3.10.3 Mixed Use**

13.3.10.3.1 Notwithstanding the Mixed Use policies of this Plan, the following will apply:

- a. commercial uses will be required on the ground level of buildings fronting Lakeshore Road East and Street 'H';
- b. single use residential buildings may be considered on lands not fronting Lakeshore Road East or Street 'H'; and
- c. science and technology facilities, creative industry incubator spaces, cultural infrastructure facilities, and banquet and conference centres will be permitted in the Cultural Waterfront Precinct. These spaces may be located in stand-alone buildings or in conjunction with other uses.

### **13.3.10.4 Business Employment**

13.3.10.4.1 Notwithstanding the Major Node policies of this Plan, the Business Employment designation will be permitted.

13.3.10.4.2 Notwithstanding the Business Employment policies of this Plan, the following additional uses will be permitted:

- a. major office;
- b. institutional use excluding any associated residential dwellings; and

13.3.10.4.3 Notwithstanding the Business Employment policies of this Plan, the following uses will not be permitted:

- a. adult entertainment establishment;
- b. animal boarding establishment which may include outdoor facilities;
- c. body rub establishment;
- d. cardlock fuel dispensing facility;
- e. Motor Vehicle Commercial;
- f. motor vehicle body repair facilities;
- h. trucking facilities; and

- i. **waste processing stations or waste transfer stations** and composting facilities (not associated with an alternative waste collection system).

### 13.3.11 Implementation

An innovative approach will be required to implement the vision and guiding principles for Lakeview Waterfront. The development approvals process is generally described as follows:

- preparation of area wide studies;
- preparation of development master plan(s) including supporting documentation and public consultation to the satisfaction of the City;
- endorsement of development master plan(s) by City Council; and
- submission of development application(s) and studies (including any contribution to cost sharing agreement for studies).

13.3.11.1 Development applications will be considered premature and not “complete” under the Planning Act until the area wide studies, have been completed and development master plan(s) have been endorsed by Council.

13.3.11.2 Notwithstanding policy 13.3.11.1, development applications may be submitted in conjunction with development master plans and area wide studies, all of which may be reviewed by staff, however in such case, the development applications shall not be considered “complete” under the Planning Act until the Council endorsement referred to in Section 13.3.11.1 has been obtained. Should modifications be required by the City to development master plans, development applications shall be revised prior to Council approval.

13.3.11.3. The preparation of development master plans will include a public consultation component, including input from the local ratepayer’s association, to the City’s satisfaction.

13.3.11.4 That to ensure the proper and orderly development of lands, consolidation or assembly of lands may be required.

13.3.11.5 The area identified on Schedule 10 Land Use Designations as being subject to employment conversion through the Region of Peel's Municipal Comprehensive Review will not permit nonemployment uses until confirmation of approval of employment conversion is received from the Region of Peel.

#### Lakeview Waterfront Studies

A number of studies are required to be completed prior to development as well as those typically required for a complete development application.

#### Area Wide Studies

13.3.11.6 Area wide studies including but not limited to the following will be completed prior to City Council’s endorsement of development master plan(s):

- a. Transportation Study (e.g. road network and capacity analysis, **active transportation** connections and enhanced transit assessment); and
- b. Financial/Sustainability Strategy (e.g. mechanisms to achieve sustainable best practices).

#### Development Master Plan Studies

13.3.11.7 Development master plan(s) will be required to provide supporting documentation, analysis and studies that demonstrate how the Desirable Urban Form policies for this area have been satisfied.

13.3.11.8 A sustainability plan will be prepared as part of the planning for each precinct area to ensure that development is consistent with the vision of creating a model green, sustainable and creative community and address the findings of the Financial/Sustainability Strategy. The plan will address issues, including but not limited to, the use of green development standards and sustainable

measures and applicable mechanisms to implement such as area specific levies, alternate parking standards, and bonus zoning.

13.3.11.9 Additional studies may be required to be completed but will not be limited to:

- a. Active and Urban Shoreline Study;
- b. Coastal Studies;
- c. Cultural Incubator Study.
- d. Economic Development Business Case Study (Innovation Corridor and Cultural Waterfront Precincts);
- e. Height Study(s);
- f. Land Use Compatibility Assessment(s);
- g. Post-Secondary Institution Business Case Study; and
- h. Serson Creek Study.

### **Contamination and Land Use Compatibility**

In addition to the policies of the Plan, the following provide further guidance on issues of contamination and site remediation.

13.3.11.10 Development applications may be required to undertake a study to assess contamination in the area in accordance with Provincial Government regulations and standards and City policies.

13.3.11.11 If the study indicates potential for soil or ground water contamination, an assessment of the conditions will be required. If contamination is confirmed, a remedial action plan in accordance with Provincial Government regulations and standards appropriately addressing contaminated sites will be required. Constraints with respect to proposed land uses will be identified.

development charge by-laws enacted pursuant to the Development Charges Act, 1998, as amended.

13.3.11.12 Environmental site assessments will be required to confirm existing site conditions and if contamination has been identified, all requirements for remediation to ensure lands can be redeveloped for their intended land uses.

13.3.11.13 Provincial Government restrictions regarding the buffering of new residential uses will apply to the redevelopment of lands in proximity to existing industries and the wastewater treatment plant.

13.3.11.14 Land use compatibility assessments are to be undertaken for new residential and other sensitive land uses at the time of a development application. This is necessary in order to determine whether such uses are compatible with remaining employment uses, and to identify what mitigation measures, if any, are required in order to ensure the operational viability of these remaining businesses. In these circumstances, the onus of responsibility for mitigation will fall on new residential and other sensitive uses.

### **Planning and Financing Tools**

13.3.11.15 In the event that there are multiple landowners, to ensure the appropriate and orderly development of the site and to ensure that the costs associated with development are equitably distributed among all landowners, the City will require that a cost sharing agreement and/or front end agreement has been executed to address distribution of costs and municipal and community infrastructure, lands and facilities associated with development in a fair and equitable manner. Individual developments will generally not be approved until the subject landowner becomes party to the landowners' cost sharing agreement.

13.3.11.16 The City will not be a party to any landowner cost sharing agreement but may be a party to a front end agreement. Where necessary for the purposes of facilitating a front ending agreement, the City may utilize area specific

13.3.11.17 In order to ensure the proper and orderly development in accordance with this Plan, development will occur by way of one or more plans

of subdivision which will determine the final alignment of public roads, the location and size of development lots and blocks, and parkland. Development may be phased as necessary. Land consolidation will be encouraged.

13.3.11.18 In exchange for increased height above 15 storeys or density permissions above the targets established in Table 1: Distribution of Housing and Unit Targets by Precinct, a community benefits contribution pursuant to Section 37 of the Planning Act will be required.

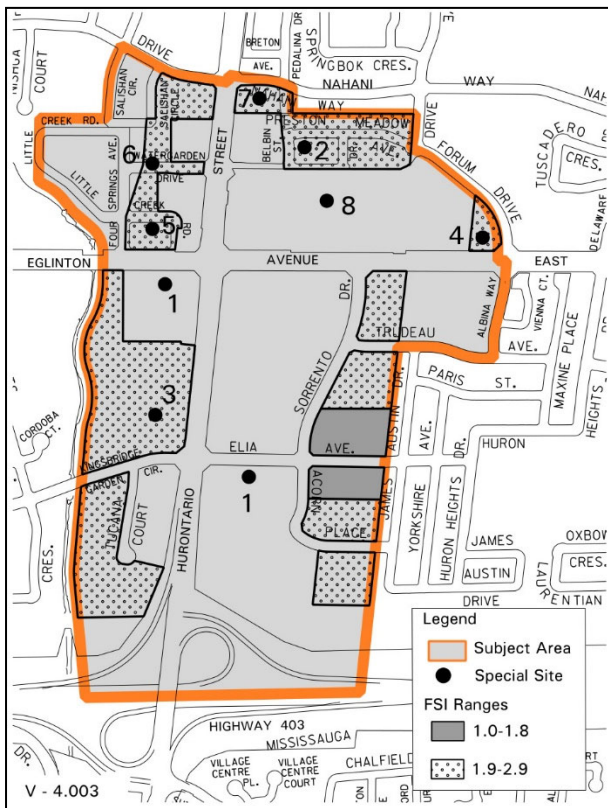
13.3.11.19 Notwithstanding 13.3.11.18 in the Waterway District Area, in exchange for increased height above 25 storeys, a community benefits contribution pursuant to Section 37 of the Planning Act will be required.

13.3.11.20 Notwithstanding policy 13.3.11.18, Section 37 will not be applicable for lands designated Business Employment and Institutional within the Innovation Corridor and Cultural Waterfront Precinct, respectively.

13.3.11.21 Eligible community benefits may include, but are not limited to:

- sustainable measures above and beyond those typically provided through development charges and parkland dedication contributions;
- community facility/infrastructure;
- affordable housing; and
- public art.

## 13.4 Uptown



Map 13-4: Uptown Major Node Character Area

### 13.4.1. Introduction

Uptown will evolve to be a connected community that supports walking, cycling, transit and vehicular options for people of all ages to get around.

Uptown will support a vibrant public and private realm with a mix of offices, retail uses and a range of employment opportunities, along with medium to high density residential development.

Uptown will incorporate a high quality and well-designed network of parkland, and new development will consider environmental sustainability.

Uptown will also evolve with the support of innovative partnerships, where they will benefit the community.

### 13.4.2 Housing

13.4.2.1 Uptown is encouraged to develop with a range of housing choices in terms of unit type, unit size, tenure, and price, to accommodate changes in community needs over time.

13.4.2.2 Mississauga will encourage the provision of affordable housing, and in particular, affordable rental housing and apartments with two or more bedrooms.

13.4.2.3 Mississauga will encourage partnerships and collaborations to support the creation of affordable housing in Uptown.

### 13.4.3 Land Use

13.4.3.1 Uptown will be developed as a compact, mixed use community that supports offices, retail uses and a range of employment opportunities.

13.4.3.2 Redevelopment that results in a loss of office floor space will not be permitted, unless the office floor space is replaced as part of the redevelopment.

13.4.3.3 Redevelopment that results in a loss of retail and service commercial floor space will not be permitted, unless it can be demonstrated that the planned function of the non-residential component will be maintained or replaced as part of the redevelopment.

13.4.3.4 For lands designated Residential Medium Density, building heights will not exceed four storeys.

### 13.4.4 Urban Design

13.4.4.1 Built form in Uptown will create a sense of place, community and contribute to an improved quality of life.

13.4.4.2 Uptown will be developed to:

- a. create a fine-grain grid network of urban scale blocks and streets;

- b. provide high quality design for streets and intersections that achieve definition, enclosure and comfort for pedestrians and street life;
- c. incorporate landscaping, street furniture, street trees, lighting systems and signage to animate roads and create a positive pedestrian, cycling and transit-oriented experience; and
- d. incorporate and integrate on-street parking into streetscape design, as appropriate, to balance the needs of all modes of transportation and the public realm that share the right-of-way.

13.4.4.3 Buildings will be designed to consider the street hierarchy and streetscape, as follows:

- a. high priority streets: High priority streets are major roads and streets that may front onto public amenities, open spaces or parks. High priority streets will have the highest standard of design in the public and private realms, with a mixture of uses and pedestrian oriented built form. Building frontages on high priority streets will be developed to incorporate:
  - i. commercial uses at grade, where appropriate;
  - ii. connections to parks, public spaces and retail uses at grade, where appropriate; and
  - iii. a substantial amount of transparent vision glass at grade, where appropriate.
- b. residential streets: Residential streets primarily support housing and local connectivity. Residential streets will be designed to ensure a quality pedestrian environment. Building frontages on residential streets will incorporate residential units at grade that provide direct access to the street, where appropriate. Buildings will contribute to a quality public and private realm.
- c. service streets: Service streets will provide necessary access to parking facilities, loading, service and utility areas serving development blocks. Buildings will accommodate for service and vehicular access, and utilities along service streets.

13.4.4.4 Developments are encouraged to incorporate sustainable measures in their designs and consider opportunities to reduce Green House Gas (GHG) emissions and improve storm water management.

### 13.4.5 Transportation

13.4.5.1 South of the Character Area between the utility corridor and Highway 403, a new east-west roadway is proposed to provide access to the Downtown area. The proposed Highway 403 North Collector would be connected to Highway 403 west of Hurontario Street and east of Mavis Road and would be developed within the Parkway Belt West corridor as a one-way westbound two lane roadway with grade separations at Hurontario Street, Duke of York Boulevard, Confederation Parkway, the Highway 403/Mavis Road westbound off-ramp, and Mavis Road.

#### Road Network

13.4.5.2 The road network identified in Map 13-4.1 will provide connectivity and a fine grained multi-modal network to encourage walking and cycling within the Node.

- a. all roads shown will be public.
- b. the design, access requirements and public/private responsibilities for roads and pedestrian connections will be determined through the development application process.
- c. adjustments to the road network may be made without amendment to Map 13-4.1 at the City's discretion to accommodate block development, while maintaining the goal of breaking up large blocks with roads and pedestrian easements.

13.4.5.3 Notwithstanding 13.4.5.2.a, a limited number of private roads may be considered subject to the following:

- a. public easements will be required;

- b. required right-of-way widths for the classification of the road that is constructed will be provided; and
- c. appropriate terminus may be required for maintenance and operations where a public road connects with a private road.

13.4.5.4 Future additions to the road network will be public roads, unless arrangements for private roads are made that are satisfactory to the City.

13.4.5.5 New roads will connect to existing roads in surrounding neighbourhoods.

#### Road Design

13.4.5.6 Roads will be designed as complete streets. New roads will be designed to incorporate active transportation and transit infrastructure. Existing arterial and collector roads dissecting and surrounding the Character Area will be redesigned to incorporate active transportation and transit infrastructure, as appropriate.

13.4.5.7 Vehicular access from roads will be coordinated and consolidated in order to minimize driveways and disruption to pedestrians, cyclists and transit.

13.4.5.8 Pedestrian and cycling connections to transit facilities will be prioritized.

13.4.5.9 Transit stations and facilities will be incorporated into redevelopment plans adhering to the standards of the applicable transit agency.

13.4.5.10 Bicycle parking will be required and should be located throughout the Character Area and at transit facilities.

13.4.5.11 Cycling facilities will be incorporated per the Mississauga Cycling Master Plan. The City may in the future identify secondary cycling routes to be integrated with the design of the public realm.

13.4.5.12 Development applications will be accompanied by traffic impact studies and/or parking utilization studies that will address, among other

things, strategies for limiting impacts on the transportation network such as:

- a. transportation demand management;
- b. transit oriented development and design;
- c. pedestrian/cycling connections; and
- d. access management plan.

13.4.5.13 The City may consider alternative road design standards to achieve community design objectives.

#### Parking

13.4.5.14 Underground parking is preferred, however, where above grade parking structures are permitted they will be screened in such a manner that vehicles are not visible from public view and have appropriate directional signage to the structure. Along prominent streets, parking structures should be screened by buildings that incorporate a mix of uses between the parking structure and the street.

13.4.5.15 Limited surface parking will be permitted to accommodate matters such as accessibility parking spaces, car-share spaces and pick-up/drop-off areas for delivery services.

13.4.5.16 Where surface parking is permitted its impact should be minimized by being located at the rear or side of buildings, by using screening and employing low impact development techniques, and by providing pedestrian amenities, where appropriate.

### 13.4.6 Open Space Network

13.4.6.1 The park network identified in Map 13-4.1 will form a connected parkland system that is green, safe, attractive, and supports a range of social and recreation activities. The location, configuration and size of the parkland block(s) will be determined in conjunction with the development application process.

13.4.6.2 Parkland should be designed and located to create a central focus for Uptown. Parkland may also



provide gathering spaces and connections throughout Uptown, to existing and future open spaces, commercial developments, community facilities and to surrounding neighbourhoods.

13.4.6.3 Playgrounds should be provided within an unobstructed 400m walking distance from residential areas within Uptown, unimpeded by major pedestrian barriers such as rivers or major roads.

13.4.6.4 Development that has frontage to a park will be built to maximize sun exposure onto parkland.

13.4.6.5 Publicly accessible private open spaces (POPS) may be incorporated into developments provided that:

- a. the design of private open space will integrate seamlessly with Uptown's parkland system, adjoining street network, and pedestrian environment;
- b. the private open space interfaces with existing and/or proposed development in a legible and cohesive manner; and
- c. the private open space is maintained as universally accessible and open to the public.

13.4.6.6 Notwithstanding 13.4.6.1 opportunities to obtain additional parkland and space for recreational and library services will be explored through purchase by the City, establishment of easements, land dedication, or future agreements with land owners, where appropriate, to ensure the adequate provision of community services.

### 13.4.7 Community Infrastructure

13.4.7.1 Mississauga will encourage partnerships and collaborations to identify needs and develop community infrastructure in Uptown.

13.4.7.2 Community infrastructure is encouraged to adopt a compact form.

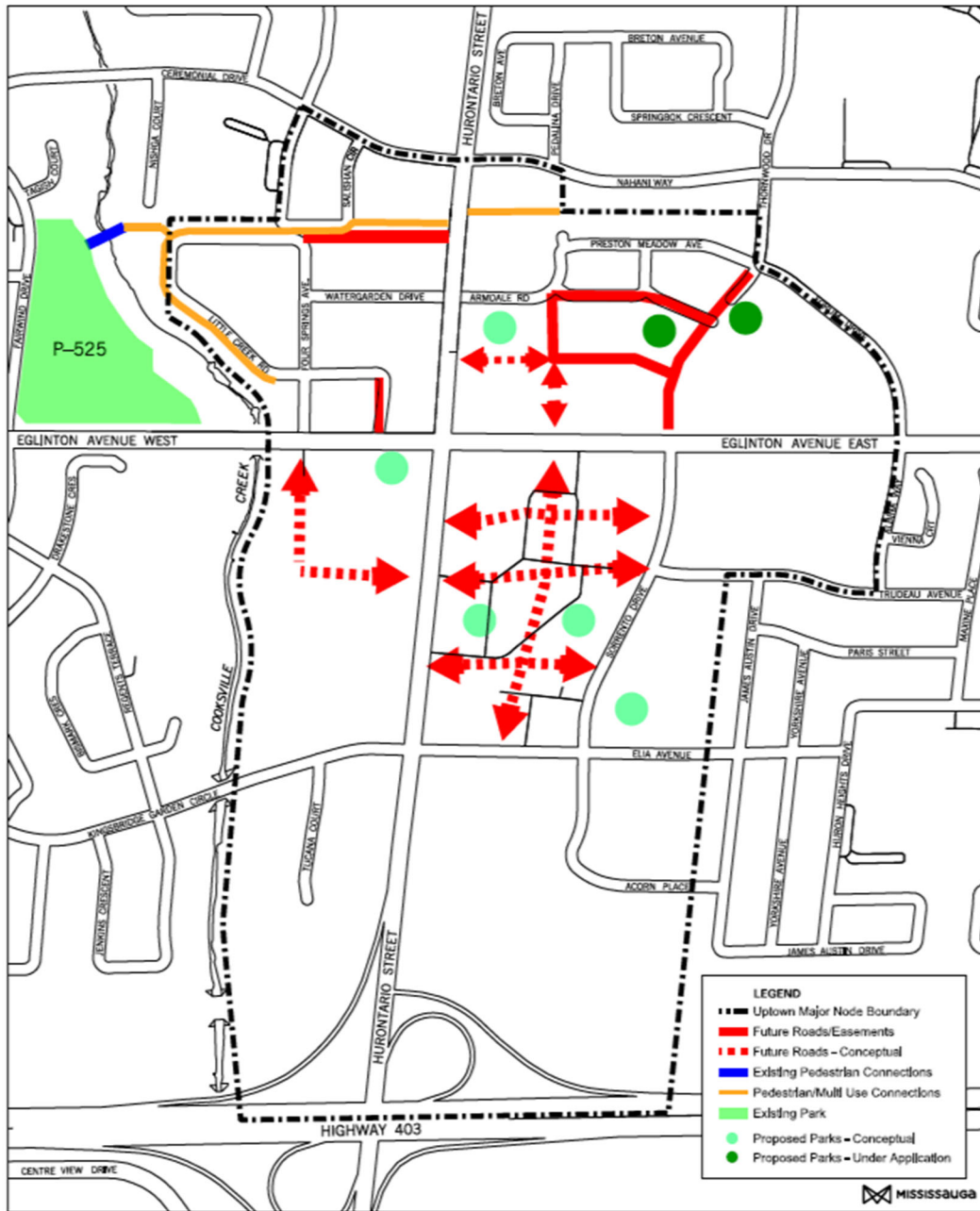
### 13.4.8 Implementation

13.4.8.1 Development master plans may be required for large scale developments. The need for a development master plan will be determined through the pre-application meeting and in consultation with staff prior to development application submission.

13.4.8.2 The development master plan should provide a link between Official Plan policies, and subsequent plans of subdivision, rezoning and site plan applications.

13.4.8.3 Development master plans and development applications will demonstrate how the proposal will contribute to the creation of a complete, healthy and connected community in Uptown. This includes, but is not limited to, consideration for how the new proposal supports:

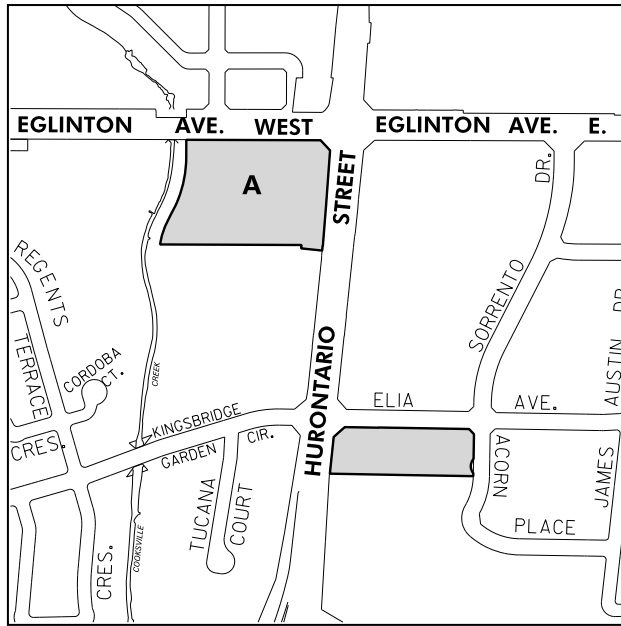
- small block sizes and a fine grained road pattern;
- creation of the road network depicted in Map 13-4.1;
- creation of the park network identified in Map 13-4.1;
- provision of affordable housing;
- future provision of community infrastructure, where applicable; and
- high quality design outcomes for the public realm.



### 13.4.9 Special Site Policies

There are sites within the Character Area that merit special attention and are subject to the following policies.

#### 13.4.9.1 Site 1



13.4.9.1.1 The lands identified as Special Site 1 are located east of Hurontario Street and south of Elia Avenue.

13.4.9.1.2 Notwithstanding the provisions of the Residential High Density and Office designations, the following additional policies will apply:

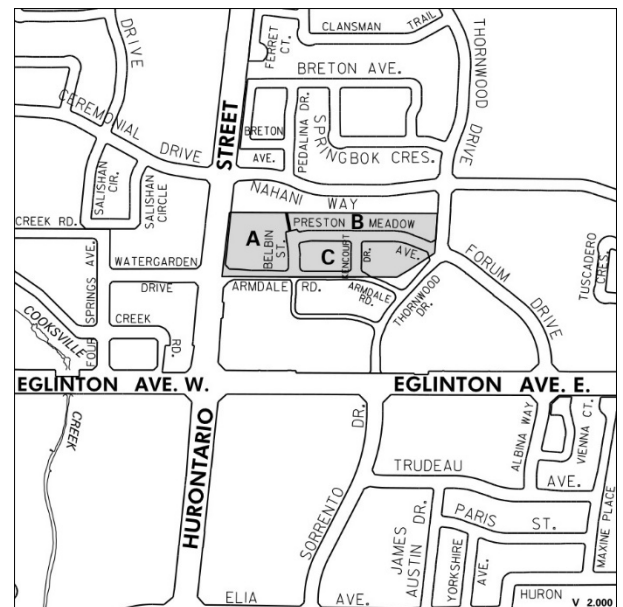
- Residential High Density development in combination with office uses will also be permitted, either within one building or in separate buildings. When in combination within a building or on a single site, an integrated development will be required;
- the preferred location of office development will be adjacent to Hurontario Street;
- architectural building and site plan solutions will be developed to create an interesting and identifiable street edge along Hurontario Street;

- special consideration may be given to reducing building setbacks and other requirements of the Zoning By-law as a means of increasing the opportunities for creative development; and
- in order to achieve a continuous street frontage, buildings will be encouraged to minimize setbacks from street lines.

13.4.9.1.3 Notwithstanding the policies of this Plan, the lands identified as Area A will permit the following uses:

- apartment dwellings with a maximum height of 36 storeys;
- commercial school;
- major office;
- funeral establishment; and,
- post-secondary education facility.

#### 13.4.9.2 Site 2



13.4.9.2.1 The lands identified as Special Site 2 are located east of Hurontario Street and north of Eglinton Avenue East.

13.4.9.2.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. a concept plan will be required to address, among other matters: compatibility of building form and scale with existing and proposed surrounding land uses; and acceptable ingress and egress arrangements for Hurontario Street, Eglinton Avenue East, and Thornwood Drive;
- b. a maximum of 1 511 dwelling units will be permitted collectively in Areas identified as A and C;
- c. a minimum of 2 280 m<sup>2</sup> and a maximum of 6 300 m<sup>2</sup> of retail commercial and office space will be provided, contained within the first three floors of the apartment buildings within Area A;
- d. the lands identified as Area A will be permitted to develop to a maximum **floor space index (FSI)** of 7.26, and have a maximum building height of 38 storeys; and
- e. semi-detached dwellings will also be permitted on the lands identified as Area B.

### 13.4.9.3 Site 3



13.4.9.3.1 The lands identified as Special Site 3 are located west of Hurontario Street, north of Kingsbridge Garden Circle.

#### Area 3A

13.4.9.3.2 Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 3A will be permitted to develop to a maximum **floor space index (FSI)** of 3.9.

#### Area 3B

13.4.9.3.3 Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 3B will be permitted to develop to a maximum **floor space index (FSI)** of 3.6.

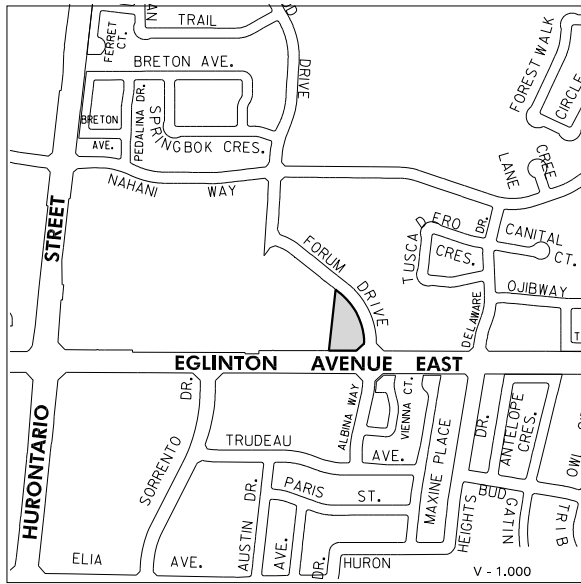
#### Area 3C

13.4.9.3.4 Notwithstanding the provisions of the Residential High Density designation, the lands identified as Area 3C will be permitted to develop to a maximum **floor space index (FSI)** of 3.6. Offices will also be permitted.

#### Area 3D

13.4.9.3.5 Notwithstanding the provisions of the Residential High Density designation, on the lands identified as Area 3D offices will also be permitted.

### 13.4.9.4 Site 4



13.4.9.4.1 The lands identified as Special Site 4 are located at the northwest corner of Eglinton Avenue East and Forum Drive.

13.4.9.4.2 Notwithstanding the provisions of the Residential High Density designation, an apartment building to a maximum **floor space index (FSI)** of 3.7 and a maximum height of 20 storeys, will be permitted.

### 13.4.9.5 Site 5

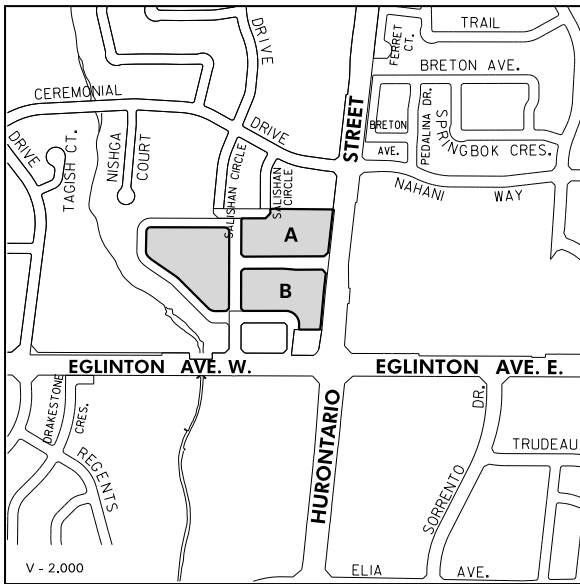


13.4.9.5.1 The lands identified as Special Site 5 are located on the north side of Eglinton Avenue West, west of Hurontario Street.

13.4.9.5.2 Notwithstanding the provisions of the Residential High Density designation, the maximum **floor space index (FSI)** permitted for all development shall be 7.5.

13.4.9.5.3 Notwithstanding the maximum height provisions, a maximum height of 28 storeys will be permitted.

13.4.9.6 Site 6



13.4.9.6.1 The lands identified as Special Site 6 are located within the northwest quadrant of Eglinton Avenue West and Hurontario Street.

13.4.9.6.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. a maximum of 2 370 dwelling units will be permitted;
- b. a minimum of 9 400 m<sup>2</sup> of retail commercial and office space will be located within the first three floors of the buildings within Areas 6A and 6B;
- c. a maximum building height of 35 storeys will be permitted on the lands identified as Area 6A, and,
- d. a maximum building height of 38 storeys will be permitted on the lands identified as Area 6B.

13.4.9.7 Site 7

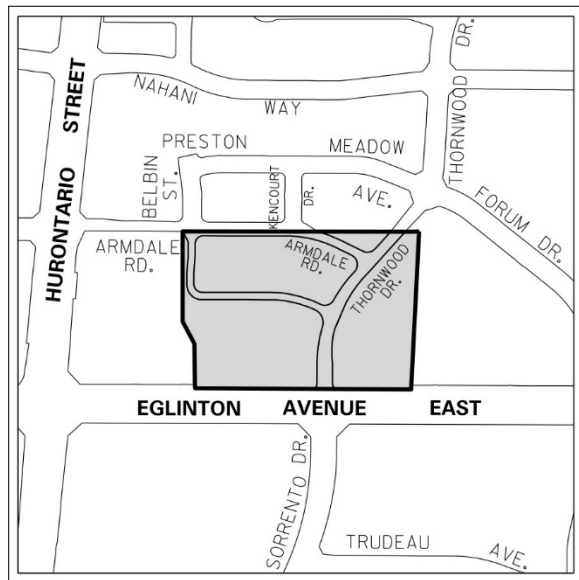


13.4.9.7.1 The lands identified as Special Site 7 are located east of Hurontario Street and south of Nahani Way.

13.4.9.7.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- e. a minimum retail commercial floor space of 313 m<sup>2</sup> will be permitted;
- f. an apartment building to a maximum **floor space index (FSI)** of 5.1 will be permitted; and,
- g. a maximum building height of 33 storeys will be permitted.

### 13.4.9.8 Site 8



13.4.9.8.1 The lands identified as Special Site 8 are located on the north side of Eglinton Avenue East, east of Hurontario Street.

13.4.9.8.2 Notwithstanding the policies of this Plan, the following additional policies will apply:

- a. a maximum **floor space index (FSI)** of 5.6 will be permitted;
- b. a minimum of 1 300 m<sup>2</sup> of retail commercial and office space will be required.

13.4.9.8.3 Notwithstanding the maximum height provisions, three apartment buildings with maximum building heights of 35, 35 and 37 will be permitted.

# LAKEVIEW VILLAGE

DEVELOPMENT MASTER PLAN DESIGN GUIDELINES

SEPTEMBER 2021

The logo for Sasaki, featuring the word "SASAKI" in a bold, sans-serif font. Each letter is a different color: S (red), A (orange), S (yellow), A (green), K (blue), and I (purple).



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# Introduction

- 1.1 Purpose of the Guidelines
- 1.2 Objective of the Guidelines
- 1.3 Design Integrity and Deviation from the Design Guidelines
- 1.4 Non-Conventional Building Forms
- 1.5 How to Use the Guidelines

## 1.1 Purpose of the Guidelines

The purpose of the Urban Design Guidelines is to provide further explanation and guidance based on the design thinking embedded into the Lakeview Development Master Plan. While the Lakeview Development Master Plan establishes the overall foundational framework, it is not a static vision.

Lakeview Village will continue to evolve as each block goes through a project design level of detail. This document lays out a set of guiding principles for the developments within privately owned lands to implement the related official plan policies, the endorsed Lakeview Development Master Plan, and applicable zoning by-law. It is also to ensure the intent of the Lakeview Development Master Plan district-scale design logic is captured and honoured.

While this document is appended to the Mississauga Official Plan (MOP), it is not intended to be applied as plan policy or zoning by-law in future Site Plan Review processes for projects at Lakeview Village. Applicants should also refer to the Mississauga Official (MOP) Plan principal document, Mississauga Zoning By-law, City's other design guidelines including Back to Back and Stacked Townhouses Guidelines, Low-Rise Multiple Dwellings Design Handbook, and Ontario Building Code to ensure that the applicable policies and requirements in these documents have been met. Furthermore, other City initiatives and special projects should be consulted to determine applicability, such as the City of Mississauga's Strategic Plan, Green Development Strategy, 2015 Facility Accessibility Design Standards, City of Mississauga Standards for Shadow Studies, Pedestrian Wind Comfort and Safety Studies, and the Lakeshore East Corridor

Study.

This document sets up the norm for developments of individual parcels within Lakeview Village. It should be referenced in its entirety in the design and review of all projects. It is not the intention of the guidelines to limit creativity but provide baseline guidance, within which creativity can be realized.

## 1.2 Objective of the Guidelines

Chapter 9 of Mississauga Official Plan, 'Build a Desirable Urban Form,' and the endorsed Lakeview Development Master Plan are the foundation for the Guidelines, which support the vision for Lakeview as a vibrant and predominantly mid-rise neighbourhood on the waterfront. These urban design guidelines act to articulate the policies in addition to the following objectives:

- Support the City of Mississauga's Strategic Plan and its Strategic Pillars for Change. A paramount goal is to create a vibrant and human-scale neighbourhood on the waterfront that provides diverse housing types, a rich mix of cultural, commercial and recreational amenities and services, and connecting Lake Ontario back to the City's urban fabric;
- Promote development in a coordinated, comprehensive fashion and facilitate a fair and consistent application of design principles;
- Integrate a mix of uses, through appropriate built form, including retail and commercial uses, offices, residential, cultural, entertainment and institutional uses – the whole of which is intended to put people in close proximity to a

broad range of urban amenities and experiences;

- Achieve a high-quality built form and strengthen the continuity of buildings that contribute to the Lakeview Village;
- Ensure that development is resilient, environmentally friendly, safe and universally accessible; and
- Foster compact, pedestrian, cyclist and transit-oriented development that achieves vibrant street level activities and a public realm of the highest standard.

## 1.3 Design Integrity and Deviation from the Design Guidelines

Lakeview Community Partners (LCPL) is, through this document, as well as review mechanisms, putting in place its own measures to ensure that the development of individual parcels within Lakeview Village conforms with the intent of the master plan and the high standards of design required within the community, while providing flexibility and opportunity to achieve distinctive buildings that stand out.

All buildings within Lakeview Village shall meet high standards of design, so that the district is elevated by each of the architectural projects contained within it. LCPL shall, as master developer in coordination with future builder partners, have final authority over aesthetic considerations (with advisement from the City of Mississauga), to ensure that individual architectural projects meet the high standards of the districts and conform with LCPL's and the City of Mississauga's vision for Lakeview

Village.

LCPL has committed to the City to hold an invited, international architectural competition for the Marina District. These buildings will be the signature (from a height point of view), skyline-defining elements of Lakeview Village. As such, an extremely high degree of architectural excellence will be required.

LCPL is also planning to host a Canada-wide architectural competition for the tower anchoring the eastern end of Waterway Common. This building's position at the head of this key open space equally suggests a very high level of architectural design.

## 1.4 Non-Conventional Building Forms

The Guidelines for Lakeview Village have generally been written to guide “normative” design and construction within the community - blocks employing traditional perimeter mid-rise podium and vertical mid-high-rise and high-rise elements. The zoning is crafted to guide these types of buildings. The development and design teams recognize and encourage creativity in design, and as such do not want to deter innovation through the Guidelines. For this reason, it is anticipated that variances may be made to these Guidelines for non-traditional strategies to block development. (Refer to Section 5.7 Non-Traditional/Non-Conventional Building Types for additional information.)

## 1.5 How to Use the Guidelines

This document contains four parts in addition to this introduction:

- The second chapter provides a description of the overall district and block structure
- The third chapter is focused on the ground floor(s) that directly interact with the public realm beyond the development block. Design guidelines are provided based on different types of ground floor use and their public realm context
- The fourth chapter offers guidelines for the design of mid-rise elements
- The final chapter speaks to the taller elements in terms of their locations, orientation, and design

The builder of certain block(s) should follow the guidelines that are applicable to the subject block(s). Significant deviation from the applicable guidelines should be justified with supporting studies and rationale to explain that the establishment of unique conditions supports the goals and public realm of Lakeview Village.

# District and Block Structure

- 2.1 Section Overview
- 2.2 Master Plan Sub-Precincts
- 2.3 Block Typologies
- 2.4 Physical Form of Blocks

## 2.1 Section Overview

Lakeview Village is an urban district of neighbourhoods, that each have form-giving and character-giving adjacencies that are important to maintain sight of during the implementation of development parcels. This section provides a description of the sub-precincts, shown in the map to the right, as well as overview description of the building typologies found throughout Lakeview Village, and guidance on the physical form of blocks.

## 2.2 Master Plan Sub-Precincts

The boundaries between sub-precincts have been determined through a blend of different criteria, ranging from predominant use, to geographical location within Lakeview Village, to built typologies that will comprise the majority of building types within the sub-precinct.

Each of the sub-precincts is intended to achieve the following primary goals, through its distinctiveness from other sub-districts:

1. Establish a legible, cohesive localized identity for the sub-precinct within the larger identity of Lakeview Village;
2. Respond sensitively and creatively to adjacent conditions, both within, and external to, Lakeview Village;
3. Provide a rational ordering system for the explanation of attributes for each sub-precinct, so that sub-developers within Lakeview Village are able to attain desired outcomes for localized portions of the larger district.

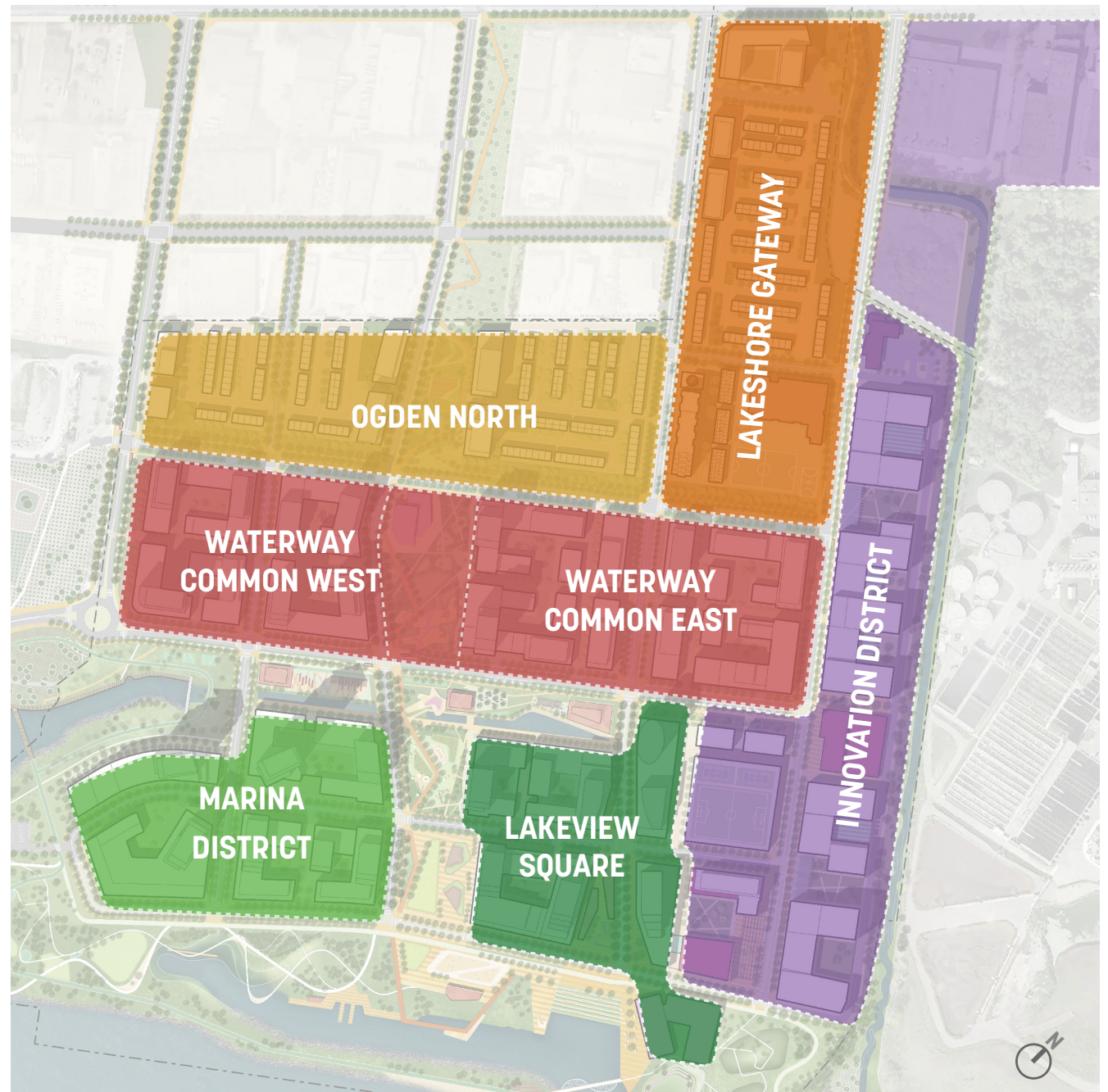


Figure 1: Lakeview Village Sub-Precincts

## 2.3 Block Typologies

The types of blocks within Lakeview Village can be divided into five typologies.

### Mid-Rise and Taller Elements

Most of the residential floor area in Lakeview Village is accommodated in podiums (or, “wings”) that define the street-facing edges of blocks, so as to create a strong urban form at ground level and in the low-rise elements of each block. On blocks where there are higher elements, these are strategically situated to support and enhance the overall structure of the block. Several of the urban blocks will have townhouse elements at their bases, providing a maximum number of “front doors to the street.”

### Mid-Rise and Townhouses

Neighbourhoods of mixed townhomes and 4-6 storey mid-rise buildings will add to the diversity of housing types at Lakeview Village, and will create a lower-in-height zone in the portion of Lakeview Village most closely sited towards Rangeview Estates and Lakeshore Road.

### Waterfront

Blocks fronting onto Lakefront Park will predominantly be apartments, and in some cases, retail in their lower stories (retail predominantly at first level). Taller elements will be set back from the front, Lakefront Park-facing edge of these blocks, and will be terraced to remove any sense of the Lakefront Park being negatively impacted by modestly higher elements.

### Marina

The Marina blocks are distinct, being a combination of terraced mid-rise and higher buildings. The three buildings are also intended to operate in-concert with one another, at the lower and higher levels. Extreme care has been taken to set the higher elements back from the Lakefront Park, and to orient the buildings so that views into Lakeview Village as well as outwards are maximized. The “fan-shaped” orientation of the higher elements supports this outcome.

### Office/Institutional

The Innovation District blocks are of a specific type, in order to maximize the utility of that compact corridor while maintaining an elegant street frontage onto New Haig Boulevard.

The office/institutional buildings of the Innovation District focus occupied space onto New Haig Boulevard, with parking visually concealed at the center of the blocks. The building configurations create open spaces that open up to the trail and landscape along Serson Creek. Screens and plantings along the trail will provide a green corridor experience for users of Serson Creek.

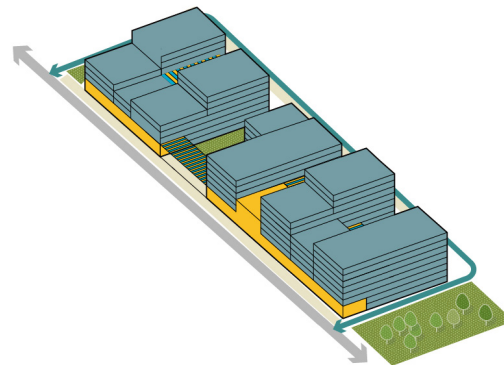


Figure 2: Office/Institutional Block Typology

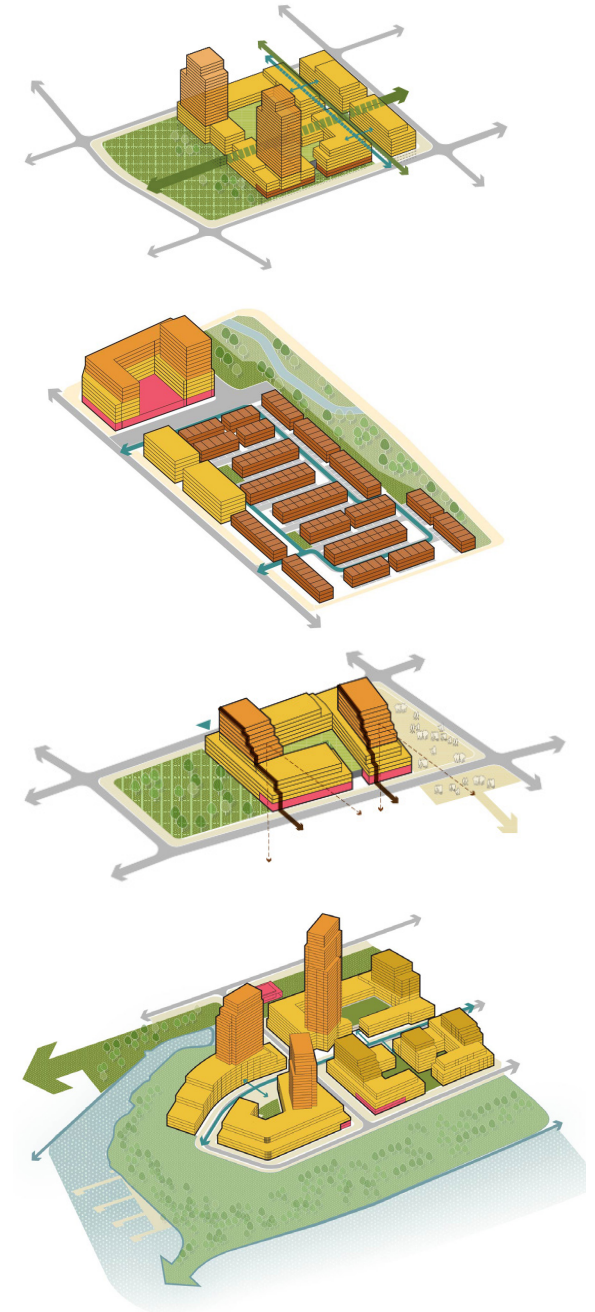


Figure 3: Residential Block Typologies

## 2.4 Physical Form of Blocks

This section describes alternative approaches to the overall block, in order to provide guidance on the balancing of differentiation and cohesion.

### Approach 1: Entire-Block Design

In certain situations, this approach can yield exciting results that add to cohesion within Lakeview Village. With this approach, mitigation of on-the-ground perception of scale and size is important, in order to maintain a human-friendly scale of architectural elements at Lakeview Village. This approach specifically suggests attainment of block-level planning and architectural design excellence, sophistication of overall composition, and sophistication of façade design and articulation.

### Approach 2: Horizontal Differentiation Between Mid-Rise and High-Rise

This approach treats elements below the mid-rise datum level differently than those above the datum. Differences may be architectural (amount and type of fenestration, balconies, façade articulation), and materially (masonry vs. glass, warm materials such as wood in proximity to the ground, etc.). This is a useful and valid approach to attaining diversity in design and materials within Lakeview Village. A negative outcome with this approach, however, may occur if all elements below the mid-rise datum are treated uniformly across the entire block (as with the risks inherent in Approach 1 overall). This approach can be combined with Approach 3.

### Approach 3: Vertical Differentiation of Block Quadrants (2 Primary Divisions)

The differentiation of mid-rise and high-rise elements

into distinct buildings is desirable (notwithstanding the fact that basements and podia may be shared). Opportunities to create these divisions at the mid-rise levels can be generated through the location of pedestrian ways to/from the interior of the blocks; utilizing these break-points to change architectural articulation. This approach can be combined with Approach 2 above.

### Approach 4: Vertical Differentiation of Block Quadrants (3 or More Primary Divisions)

Same as Approach 3, but with a greater number of vertical divisions. The precise number of these divisions may result from particularities of the specific frontages and/or adjacencies of the block in question. This approach can be combined with Approach 2 above.

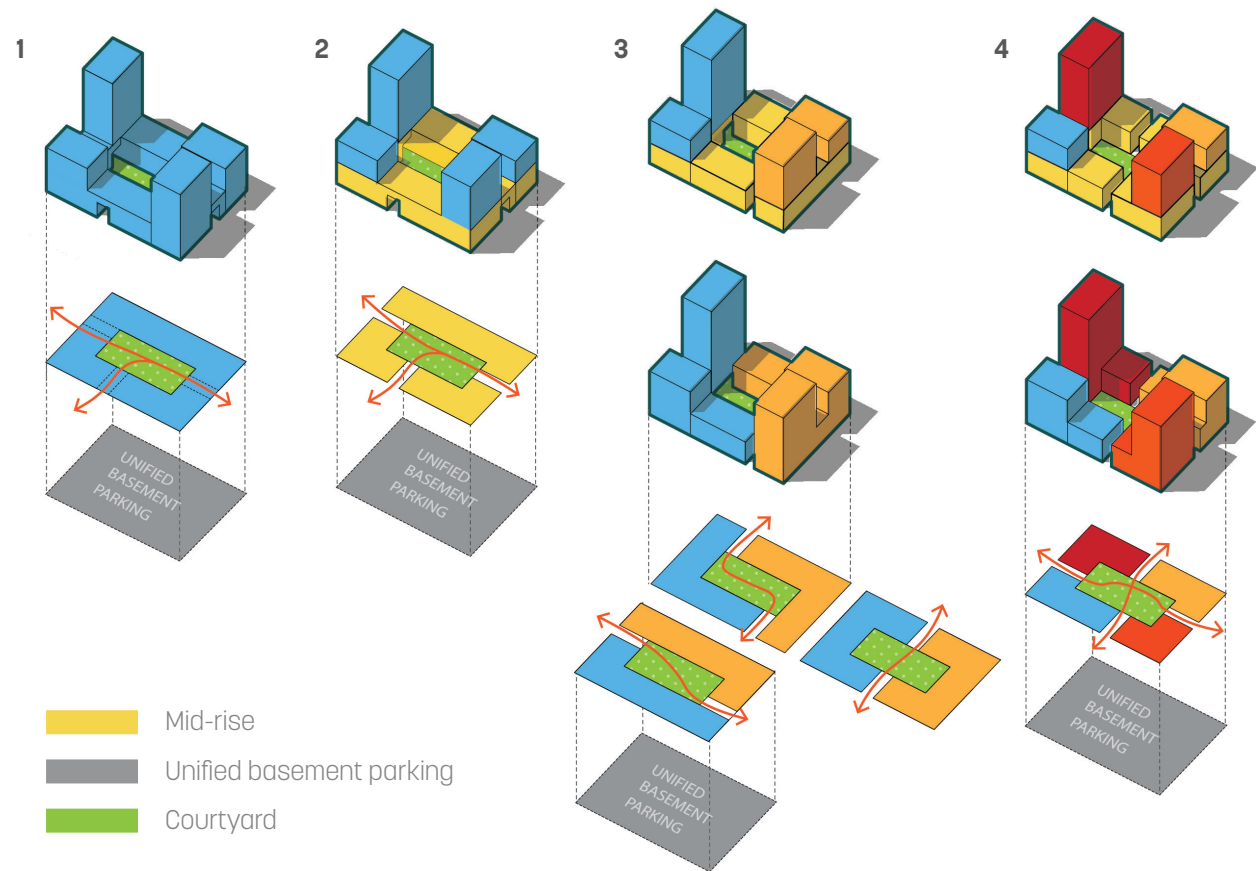


Figure 4: Alternative Approaches to the Physical Form of the Block



# Street Frontage and Ground Floor Design Guidelines

- 3.1** Section Overview
- 3.2** Master Plan Corridor Conditions
- 3.3** Street Frontages in Lakeview Village
- 3.4** General Guidance for Buildings on "A" and "B" Street Frontages
- 3.5** Guidance for Buildings on "A" Street Frontages
- 3.6** Guidance for Buildings on "B" Street Frontages
- 3.7** Buildings Facing Parks and Open Space
- 3.8** Design for Access, Parking, Service and Loading
- 3.0** Above Grade Parking Facilities on 'A' and 'B' Street Frontages
- 3.10** Ground Floor Conditions for Residential Buildings
- 3.11** Ground Floor Conditions for Other Use
- 3.12** Pedestrian Permeability Conditions
- 3.13** Utilities General Guidelines

### 3.1 Section Overview

This section provides guidance on the conditions experienced at ground level throughout Lakeview Village. First, this section explains design conditions organized by types of street frontages ('A' and 'B' Streets). It also provides guidelines for ground floor conditions, organized by building use type.

### 3.2 Master Plan Corridor Conditions

There are numerous locations within Lakeview Village in which it is going to be essential for mid-rise built elements in particular to form a part of a larger ensemble. The master plan has been prepared with this foremost in mind, so as to create a strong urban edge to development parcels.

These “corridor” conditions will, if developed synergistically, convey tremendous elegance on Lakeview Village. One of the primary purposes of these guidelines is to point out why these synergies between distinct parcels are important, and how they are to be achieved.

#### Ogden Park

Both the eastern and western sides of Ogden Park are strong opportunities for establishing an urban edge that helps to give structure to the park itself, while providing amenity to the residents of the abutting buildings. The western edge is of particular importance, because it is lined by a roadway, and because the alignment of facades will be perceived slightly obliquely due to the park’s shape. On the eastern edge there is an opportunity for buildings to read more as objects within the park itself.

#### Waterway Common

Waterway Common is the spatial and visual extension of the public parklands to the west of Lakeview Village. Both the northern and southern edges of Waterway Common will be important to define strongly at the mid-rise level to provide structure to this signature open space. Where Waterway Common intersects with Ogden Park, the corners of the abutting parcels will present immense opportunity for the further structuring of both open spaces.

#### Lakeview Square

Lakeview Square will have a concentrated feeling, with mid-rise elements providing a large amount of visual interest through facade articulation, balconies, etc. This is a see-and-be-seen space, where a greater than usual amount of articulation is called for, in the form of facade stepping, balconies, materials, etc. The establishment of moments for visual connectivity (e.g. balconies overlooking the Square) will create a sense of energy and use during day and nighttime.

#### Hydro Road

This is the main entry corridor leading all the way from Lakeshore Road into Lakeview Square. As such, its expression should be elegant, and as with many other locations in the plan, the mid-rise elements will be of fundamental importance to realizing the vision for this corridor. Setbacks have been defined to give this road a calm and stately feel. It is important to signify Hydro Road as the main character avenue into Lakeview Village where it will directly link Lakeshore Road East with Lakeview Square and the waterfront. The character will be achieved by providing an enhanced streetscape and upgraded architectural design and finishes.

#### Waterfront Edge



This is perhaps the most important frontage in all of Lakeview Village. The “bow” in the lakefront road means that the mid-rise facades in this location will be visible as a gently arcing continuity to anyone standing at the eastern or western end of the road. For this reason, it will be important for height datums and setbacks to be strongly respected. The combined result will be impressive.

#### New Haig Boulevard

The Innovation District will have its own distinct identity within Lakeview Village, given its distinct use. Both sides of New Haig Boulevard should convey the nature of this district as a place of high daytime interactivity.



Figure 5: Corridor Character Typologies

- |   |                 |   |                 |
|---|-----------------|---|-----------------|
|  | Ogden Park      |  | Hydro Road      |
|  | Waterway Common |  | Waterfront Edge |
|  | Lakeview Square |  | New Haig Blvd   |

### 3.3 Street Frontages in Lakeview Village

The endorsed Lakeview Master Plan proposes new public streets to augment the existing and planned public street pattern, thereby creating a comprehensive and interconnected urban grid.

Categories of frontages differentiate the various streets in the Lakeview according to their function, character and design. As such, all streets are categorized as 'A' and/or 'B' frontages. 'A' Street Frontages will require the highest attention to urban design, having a cohesive built form to achieve character and a vibrant pedestrian environment.

Similarly, 'B' Street Frontages are designed to ensure a quality pedestrian environment and high standard of built form, but provide defined locations for necessary access, delivery, service, loading and parking facilities serving development blocks. In general, new development should follow guidance for each street frontage type, which suggest how buildings (through their site design, streetwalls, built form treatments, etc.) individually contain and provide an appropriate sense of enclosure for the street or open space in order to collectively frame and animate the public realm. Given the urban character of Lakeview Village, there are instances in which blocks are only served by 'A' Streets, by necessity. Loading access, servicing and parking of these blocks, and select other 'A' Street conditions, should be carefully designed and managed.

The following sections set out the Guidelines for buildings along the frontage types in accordance with the Lakeview 'A' & 'B' Street Frontage Plan.



Figure 6: Lakeview 'A' and 'B' Street Frontage Plan

- 'A' Street Frontages
- 'B' Street Frontages

### 3.4 General Guidance for Buildings on 'A' and 'B' Street Frontages

Buildings are the most pronounced element of the urban fabric and create the sense of place. Buildings in the Lakeview Village will shape and articulate the streets and open spaces by forming edges and streetwalls to establish definition and enclosure. Collectively, they create the pedestrian environment, frame the public realm and establish the urban setting. Common guidance for 'A' and 'B' Street Frontages sets up the urban framework for all development blocks in Lakeview Village. The following guidance applies:

3.4.1 Locate build-to line guidance (minimum building setback along street frontage) on development blocks to inform the orientation and placement of buildings and streetwalls;

3.4.2 Coordinate build-to lines with adjacent properties in order to create consistent edges and street walls along frontages;

3.4.3 Locate buildings parallel to the street, with streetwalls placed at the build-to line, to contain the street and provide an appropriate sense of enclosure;

3.4.4 Ensure variation in setbacks along the building frontages to articulate façade emphasis at the build-to line, in order to allow for visual interest, accommodate outdoor patios, recessed entries and landscaped areas;

3.4.5 Incorporate active uses at grade, where practical within constraints of viability, such as commercial and retail, to animate the public realm and pedestrian environment (Also, refer to 3.10 Ground Floor Conditions for Residential Buildings and 3.11 Ground Floor Conditions for Other Uses);

3.4.6 Ground floor uses should incorporate glazing;

3.4.7 Locate main entrances flush with the public sidewalk for accessibility;

3.4.8 Ensure site designs relate to and interface with existing, proposed and future transit stops and facilities;

3.4.9 Locate main building entrances so that they are clearly identifiable and prominent with direct access to the public sidewalk, cycling infrastructure and transit facilities;

3.4.10 Where appropriate and practical, provide corner entrances to buildings located at prominent intersections and/or gateways;

3.4.11 Balconies and bay windows may not protrude into the public realm, but may extend as far as the build-to line in some locations;

3.4.12 Below grade parking structures shall provide an appropriate clearance between the top of the parking structure and grade to allow for healthy tree growth (typically minimum 1.5m depth of soil, not including the drainage layer). Lesser depths may be considered where planting is limited to smaller stature vegetation (for example, small to medium shrubs) and does not include trees; and

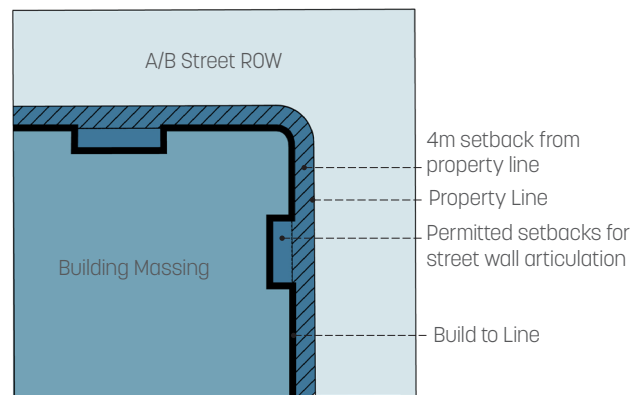


Figure 7: Prototypical Setback Conditions

3.4.13 Buildings in Lakeview Village should attain minimum heights commensurate with an urban district.

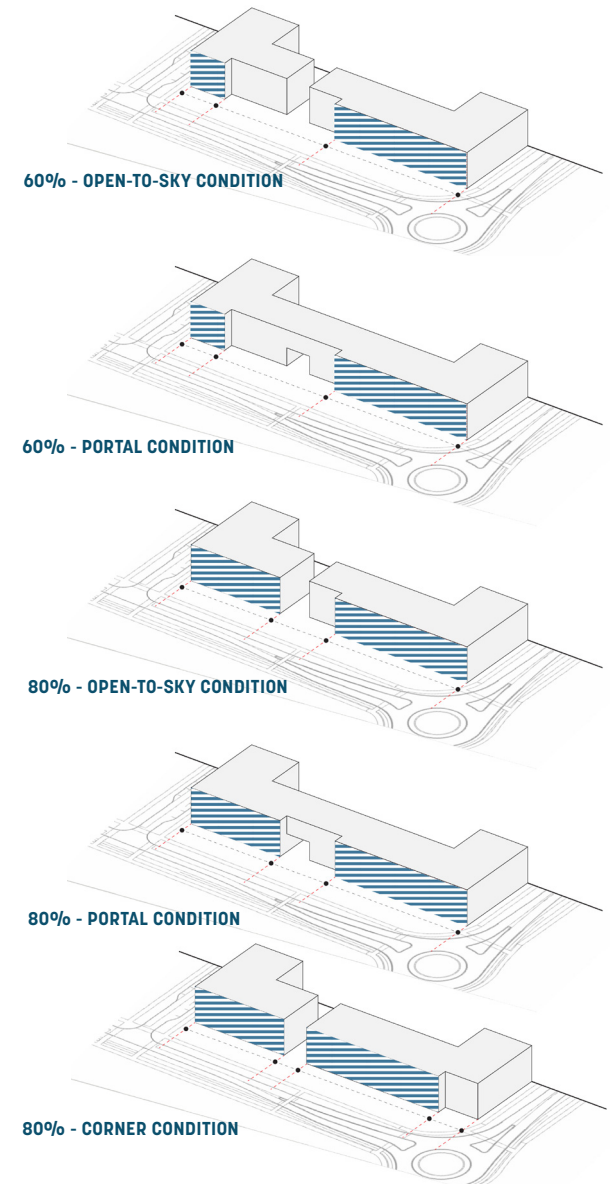


Figure 8: 60% and 80% Street Wall Conditions

### 3.5 Guidance for Buildings on 'A' Street Frontages

'A' Street frontages have a critical role and function in Lakeview Village. They are important for securing animation and character, a comfortable pedestrian environment with access to sunlight and sky views, street activity and vibrancy. Developments fronting onto 'A' Streets will require the greatest attention to urban design, ensuring a cohesive built form and streetscape treatment to achieve the highest standard in the execution of the public realm.

The following guidance applies:

3.5.1 Provide continuous buildings along development blocks fronting onto 'A' Streets to form a consistent streetwall;

3.5.2 Approximately 80% of a property's frontage is required to be occupied by the streetwall at the build-to line;

3.5.3 Development will be discouraged from locating curb cuts, driveways and laneways on 'A' Streets, except as provided for in 3.5.8 and 3.5.9;

3.5.4 Approximately 20% of the building frontage may be stepped-back to a maximum of 6m from the build-to line to allow for articulation of the streetwall, including provision for outdoor patios, recessed entries and landscaped areas;

3.5.5 Functioning main entrances to buildings should be provided on 'A' Streets;

3.5.6 Ground floor elevations along 'A' frontages should have approximately 60% vision glazing with views into the building;

3.5.7 Where residential uses are permitted at the ground level, special provisions should be applied for the design of unit entrances and setbacks; and

3.5.8 Indoor amenity spaces shall not be located on 'A' Streets, except where such amenity spaces are directly related to building entry and lobby functions. In these cases, frontages of lobby-associated amenity spaces should be primarily glazed, with strong visibility at all times into the interior space, and from the interior to the exterior.

#### Exceptions to Access on 'A' Streets

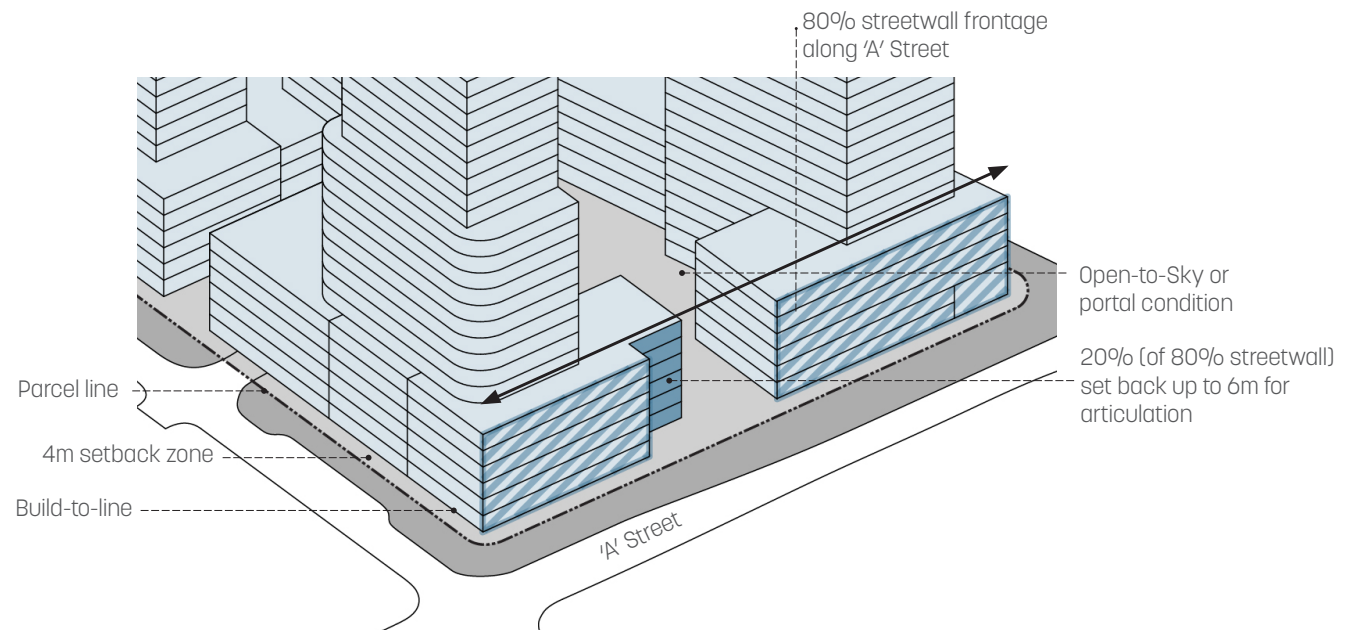
Curb cuts, driveways and access that would normally be provided on a 'B' Street may be provided on 'A' Street frontages:

3.5.9 When servicing from an 'A' Street is needed due to the phasing of buildings within a block; or

3.5.10 Provided that a site or block does not have access from a 'B' Street or other means of access, there is a requirement for emergency vehicle access; or

3.5.11 Provided that impacts on the pedestrian environment are minimized through a high standard of design treatment consistent with the quality of the public realm.

Figure 9: Wall Articulation for Buildings Facing a 'A' Street



## 3.6 Guidance for Buildings on “B” Street Frontages

‘B’ Streets generally connect ‘A’ Streets to each other. In contrast to ‘A’ Streets, they provide development blocks with access for deliveries, waste collection, service and loading, including vehicular access to structured and off-street parking within development sites. It should be noted that ‘B’ Streets are also intended to support a pedestrian environment, integrating a high standard of urban design to support street activity.

3.6.1 ‘B’ Streets will have buildings along development blocks, with provision for vehicular access to off-street parking, access for deliveries, waste collection, servicing and loading (refer to Section 3.8 Design for Access, Parking, Service and Loading);

3.6.2 Approximately 60% of a property’s frontage should be occupied by the streetwall at the build-to-line, with the exception of instances where townhomes with street entries occupy the ground-level;

3.6.3 Approximately 40% of the building frontage may be set back to a maximum of 6m from the build-to-line, to allow for articulation of the streetwall, including provision for outdoor patios, recessed entries and landscaped areas;

3.6.4 Provide functioning main front entrances to buildings on ‘B’ Street frontages when there is no ‘A’ Street frontage;

3.6.5 Provide functioning secondary entrance(s) for additional pedestrian access;

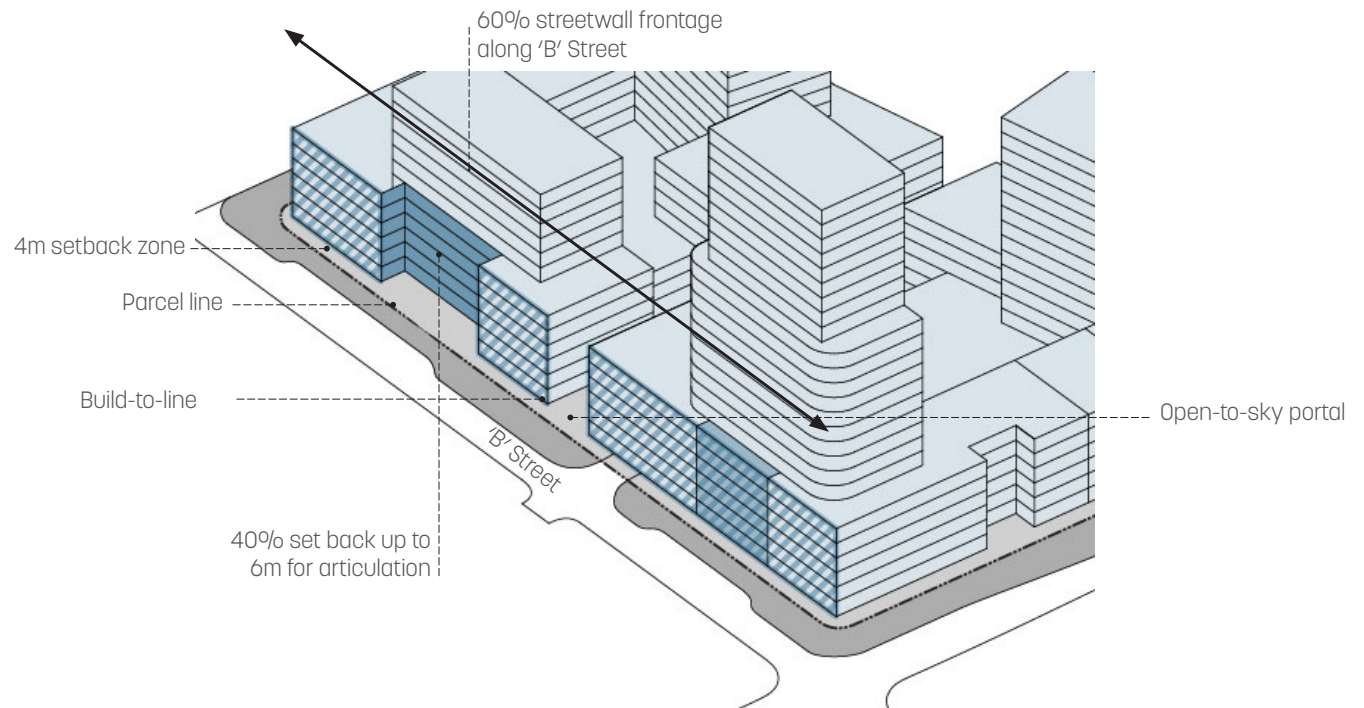
3.6.6 Ground floor elevations along ‘B’ frontages should have approximately 50% vision glazing with views into the building; and

3.6.7 Indoor amenity spaces can be considered on the ground floor of ‘B’ Streets, and shall be designed to ensure that their inclusion contributes to activation of the ground plane.

### Exceptions to Access on ‘B’ Streets

3.6.8 Where a development block has an ‘A’ and ‘B’ frontage, the most prominent building entrance shall be located on the ‘A’ Street frontage or with clear visual proximity from an ‘A’ Street, except where retail uses occupy the entirety of its ‘A’ Street frontage.

Figure 10: Wall Articulation for Buildings Facing a ‘B’ Street



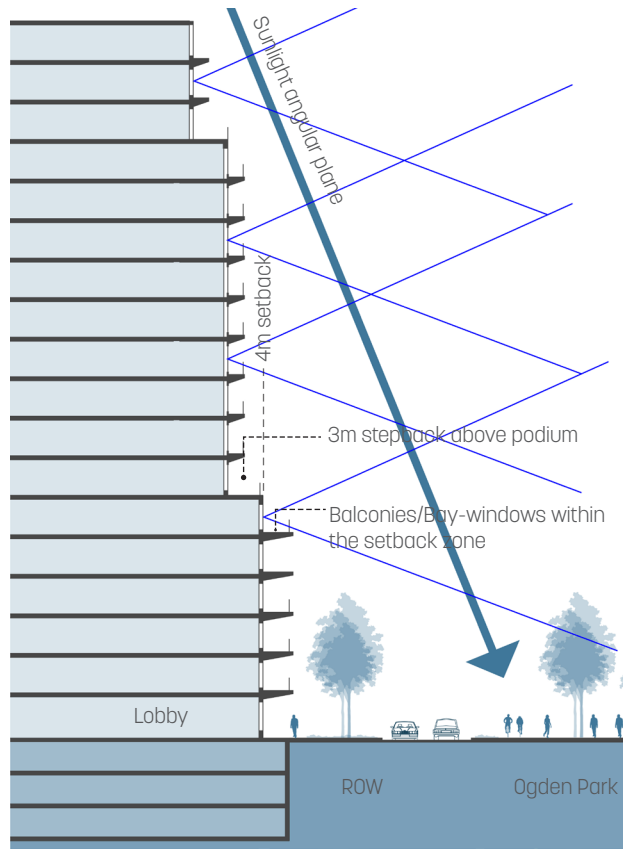
### 3.7 Buildings Facing Parks and Open Space

New buildings that front onto, or have proximity to parks and open space will require special attention to their frontage treatments and architectural design in order to achieve the kind of character, sense of place and pedestrian experience warranted for these important elements of the public realm.

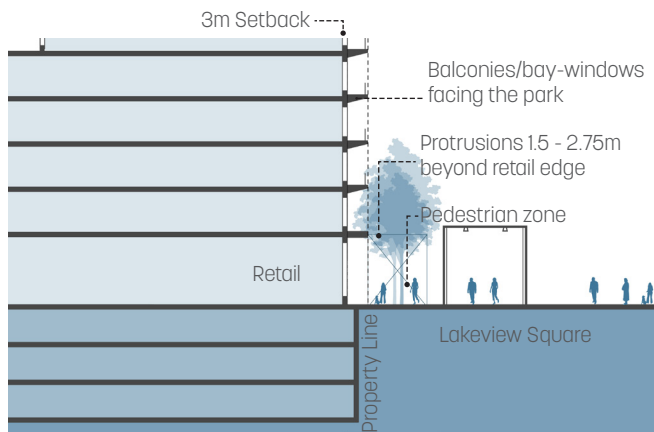
The following guidance applies:

3.7.1 If the build-to line of a development fronts onto the edge, or street adjoining a public park or open space, then 'A' frontage guidance will apply (refer to Section 3.5 Guidance for Buildings on 'A' Frontages);

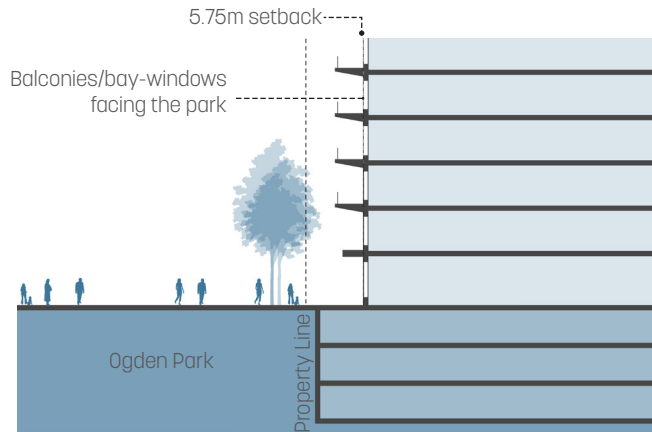
3.7.2 Consider having building frontages along parks and open spaces with uses on the ground floor that animate and/or activate the frontage such as retail, commercial and/or ground-related residential units (standalone townhouses and ground floor units within a mid-rise or podium);



SECTION FACING OGDEN PARK (A)

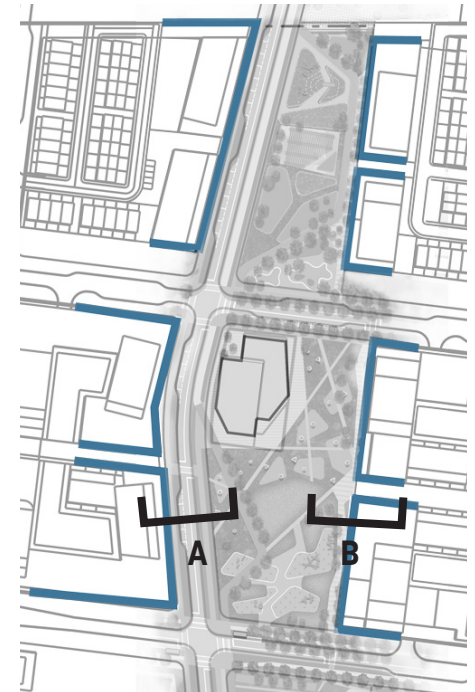


SECTION FACING LAKEVIEW SQUARE

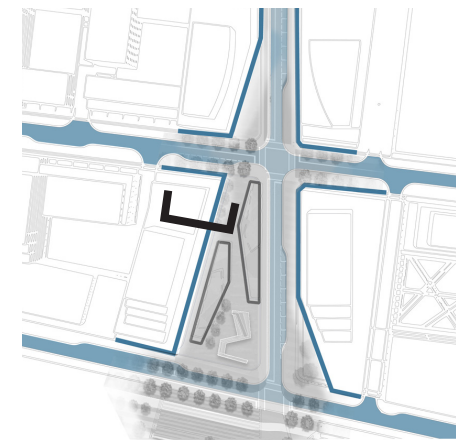


SECTION FACING OGDEN PARK (B)

◀ Figure 11: Section Conditions for Buildings Facing Ogden Park and Lakeview Square



PLAN OF OGDEN PARK



PLAN OF LAKEVIEW SQUARE

3.7.3 Buildings which surround, have proximity to, or front onto parks and open space shall have the highest level of architectural expression, articulation and use of materials that is cohesive to adjacent buildings and financially viable;

3.7.4 Consider incorporating at-grade level residential uses fronting onto parks and open space with individual unit entrances in order to animate the street environment (refer to Section 4.2 Mid-Rise Elements: All Categories);

3.7.5 Parking structures or surface parking lots shall not front onto or address parks and open spaces; and

3.7.6 The design of building massing shall protect for sun exposure onto parks and open space (refer to Section 5.4 Site Orientation).





## 3.8 Design for Access, Parking, Service and Loading

Access for loading, waste collection, parking and servicing to buildings should wherever possible be located on 'B' Streets (for exceptions, refer to Section 3.5, Guidance for Buildings on 'A' Frontages). In general, new development should consolidate the access for service, parking, loading and waste collection and will reduce vehicular interruptions along the public streets in order to improve the pedestrian experience and streetscape.

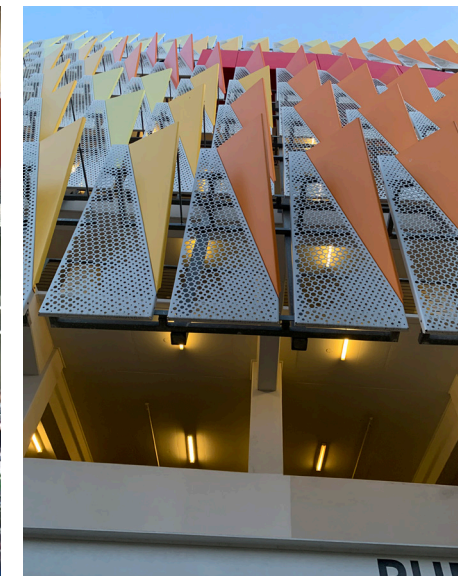
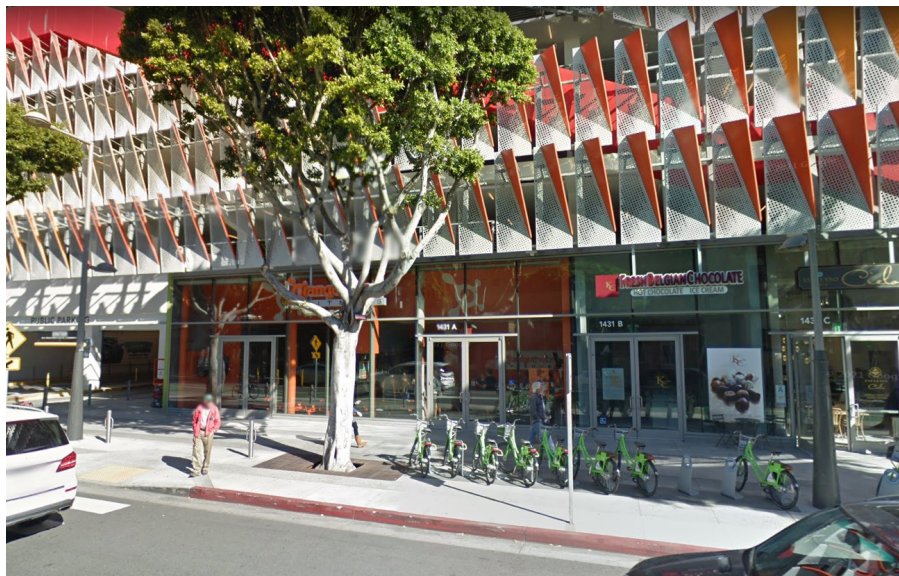
The following guidance applies:

3.8.1 Service, parking and loading shall be coordinated on sites by providing, for example, consolidated locations that can serve a number of buildings simultaneously from one area;

3.8.2 Loading, waste and service spaces shall be located internal to the building or site;

3.8.3 Loading, servicing, other vehicular related functions and utilities shall not detract from the use, safety or attractiveness of the pedestrian and public realm. These uses shall:

- incorporate special architectural treatment;
- use soft and hard landscape treatments to screen loading and servicing areas; and
- providing safe levels of illumination and lighting.



### 3.9 Above-Grade Parking Facilities on "A" and "B" Street Frontages

In select locations, above-grade parking may be necessary to augment capacity below-grade. This type of parking should be carefully integrated into the fabric of Lakeview Village, and be concealed to the greatest extent feasible within the blocks it serves. In the Innovation Corridor and serving Lakeview Square, structured parking may take the form of integrated above grade parking garage(s) with high-quality architectural screening and active ground floors.

#### Integrated Above Grade Parking Facilities

In these instances, above-grade parking should be lined on all sides by active program (residential, commercial, retail). Amenity decks situated above this parking should be designed to enable use by occupants and visitors, with sufficient depth over structure for plantings and high-quality paving. Public access should be maintained to these elevated courtyard conditions wherever feasible, with steps and ramps strategically located at edges to enable access.

3.9.1 Strive to conceal block-interior above-ground parking from view (with the exception of entries/ramps), through the use of edge liner program on all frontages;

3.9.2 Provide ventilation to embedded above ground parking in ways that are not detrimental to the amenity areas on the roof of that parking, and that are carefully integrated visually and acoustically.

#### Standalone Parking Structures

3.9.3 Standalone garages should be limited to the zone east of Hydro Road (and should not front onto Hydro Road), excluding the Innovation District. Any such garages should be for the purpose of providing public parking for public uses along the waterfront and in Lakeview Square;

3.9.4 Any free-standing garages shall have high-quality architectural screening of upper levels, and activated ground floor program;

3.9.5 Vehicle entries/exits shall be carefully integrated with adequate sightlines to allow for pedestrian safety when crossing these areas.

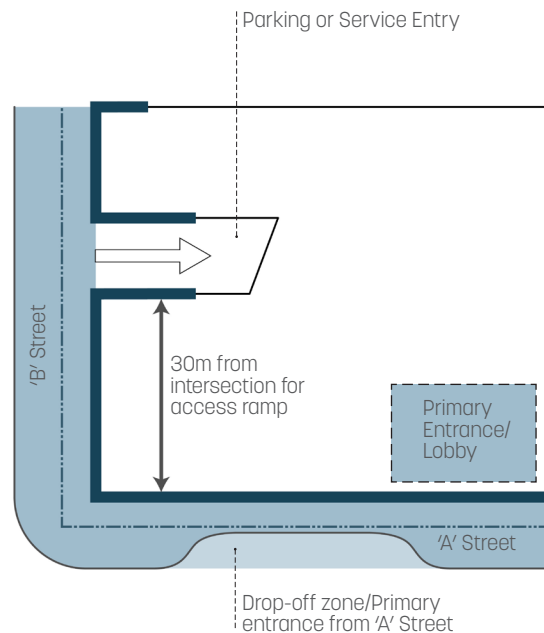
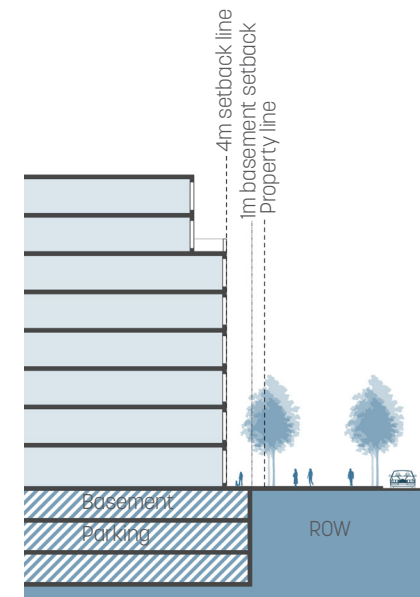
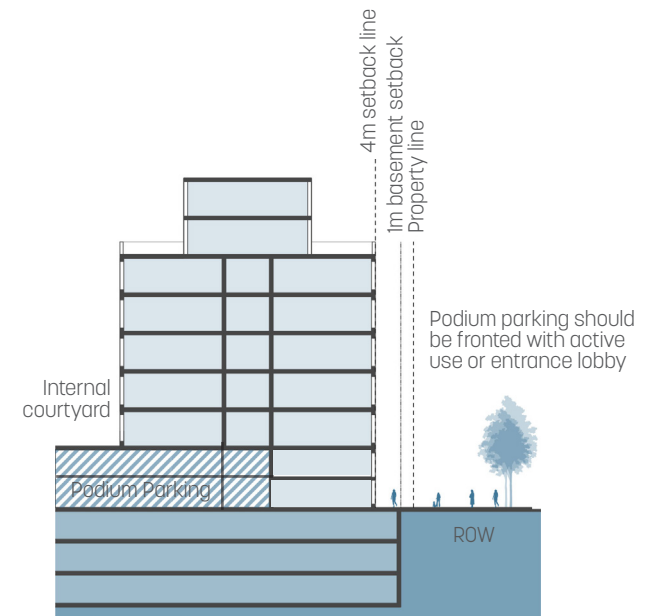


Figure 12: Prototypical Parking Plan



TYPICAL BASEMENT PARKING



TYPICAL PODIUM PARKING

Figure 13: Basement and Podium Parking Sections

## 3.10 Ground Floor Conditions for Residential Buildings

### Residential Lobbies and Amenities

Residential entries and lobbies should be designed in such a way as to enable users to find their way to them in ways that feel natural and easy, whether those users are residents, visitors, delivery persons, or others. To facilitate this:

3.10.1 Provide convenient and well-integrated drop-off zones in close proximity to entries and lobbies;

3.10.2 The interior of lobbies shall be visible to the street outside by utilizing, for example, clear glazing with large windows;

3.10.3 Utilize building signage in concert with other architectural and urban design elements to make finding the entry and lobby easy;

3.10.4 Ground floors of residential buildings shall contribute to an active public street through strategies such as locating internal shared program spaces and utilizing clear glazing with large windows.

### Townhomes

There are a variety of different possible townhome typologies within Lakeview Village, ranging from those embedded in the base of mid-rise elements, to freestanding typologies that include back-to-back and traditional.

The following guidance applies:

3.10.5 Provide a clear identity for the townhome's front door, as well as a defined path to the door. This should be combined with a "threshold" zone that provides a degree of separation for the townhome's entry from the public way it faces;

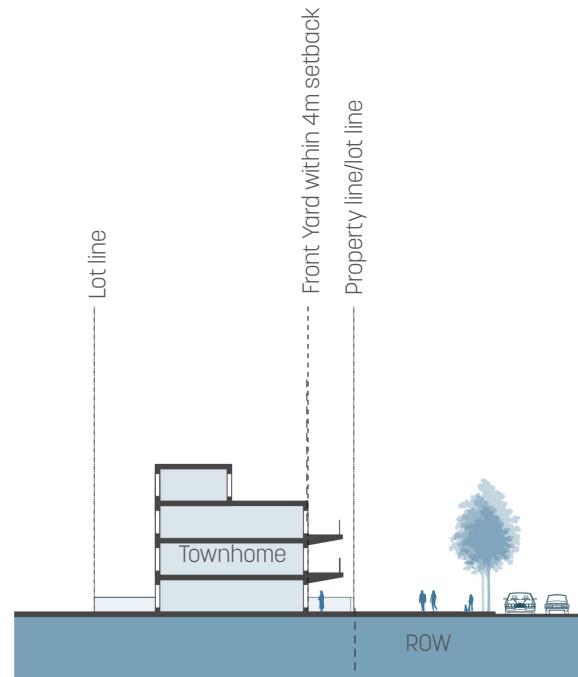


Figure 14: Standalone Townhomes Facing the Street



3.10.6 Strive to create architectural expression that facilitates recognition of each townhome as a distinct entity within a larger composition (particularly important for embedded townhomes at the base of mid-rise podia);

3.10.7 Create a distinct identity vertically for embedded townhomes (e.g., vertical reveals and/or material changes between units to establish the distinct identity of each townhome within a row).

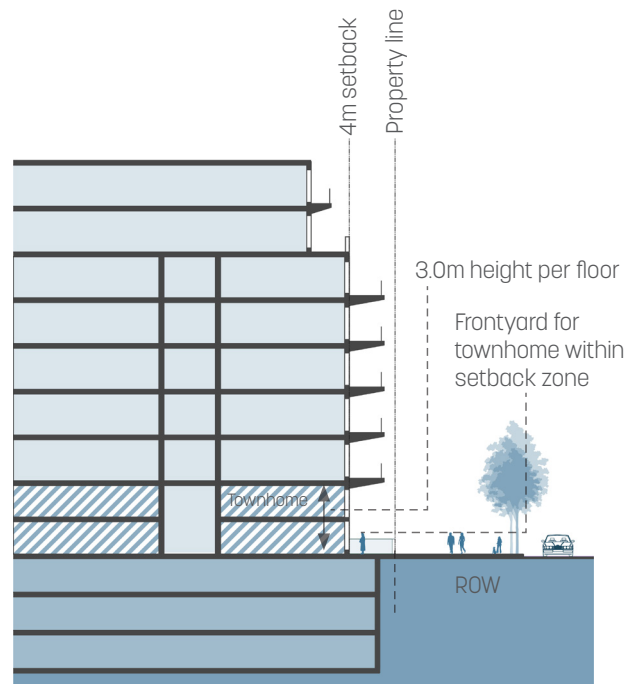
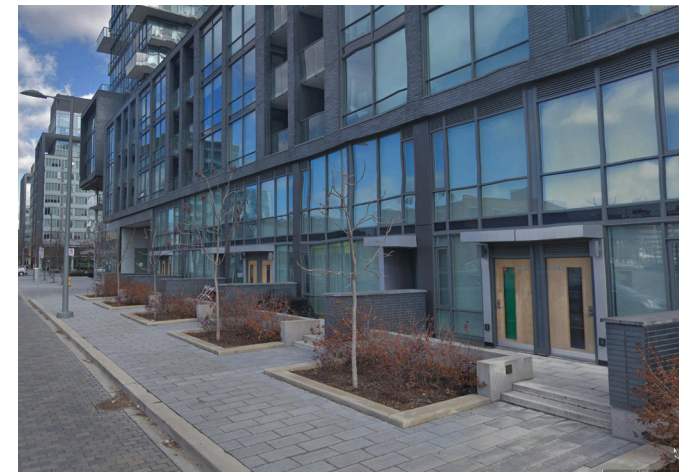
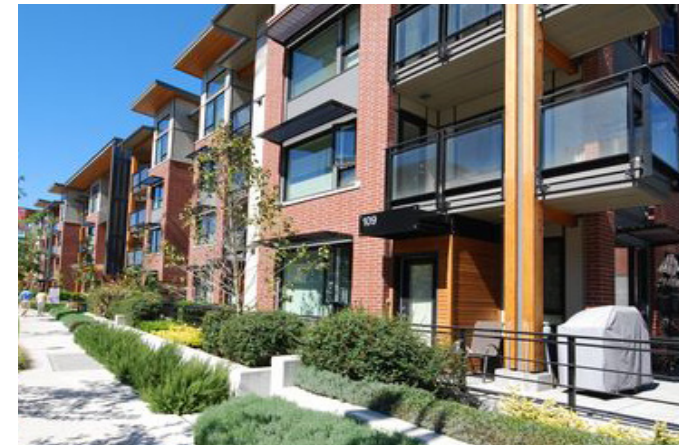


Figure 15: Integrated Townhomes Facing the Street



## 3.11 Ground Floor Conditions for Other Uses

### Parking Garage Conditions

#### 3.11.1 Structured Parking Facilities on 'A' and 'B' Street Frontages:

Parking facilities will have an important role to play in supporting key uses, attractions and urban amenities in Lakeview Village. Development shall locate structured parking and vehicular access to reduce impacts on the property and on surrounding properties and to improve the safety and attractiveness of adjacent streets, parks and open spaces.

3.11.2 Parking structures should not directly front onto 'A' Streets, and should be entirely screened by 'liner' buildings incorporating a mix of uses between the parking structure and street space in accordance with Section 2.1 - 2.3 for 'A' Street Frontages;

3.11.3 Liner uses should have approximately 10 metres depth;

3.11.4 On the ground floor, parking structures shall support activity on the streets, including those fronting onto parks and/or open spaces, through strategies such as having active uses, appropriate scale, and architectural expression (refer to Chapter 3.0 Street Frontage and Ground Floor Design Guidelines);

3.11.5 Parking structures should limit the number

of direct frontages to only one 'B' Street where more than one 'B' street frontage exists;

3.11.6 When fronting onto 'B' Streets, parking structure façade elevations shall be designed to the highest level of architectural treatment and animation to mask the parking and screen views of the interior;

3.11.7 Entrances, lobbies and passageways that provide a convenient means of access to parking facilities should be fully enclosed, appropriately signed and integrated into the façade design;

3.11.8 At grade exhaust vents serving structured parking facilities should be integrated into the design of buildings, expressed as part of the architectural character;

3.11.9 Consult the Crime Prevention Through Environmental Design (CPTED) document.

### General Standards for Retail Uses at Grade

To ensure well designed ground floors, at-grade conditions incorporating retail uses should be carefully executed in order to animate the street edge and support the pedestrian experience.

The following guidance applies to retail uses on all street frontages where retail is indicated:

3.11.10 Ground floor heights for retail uses at grade should be approximately 4.5 - 6.0 metres (floor-to-floor, measured from established grade) to accommodate retail and commercial uses with windows that correspond to the height of ground floors to the underside of ceiling and exterior canopy height;

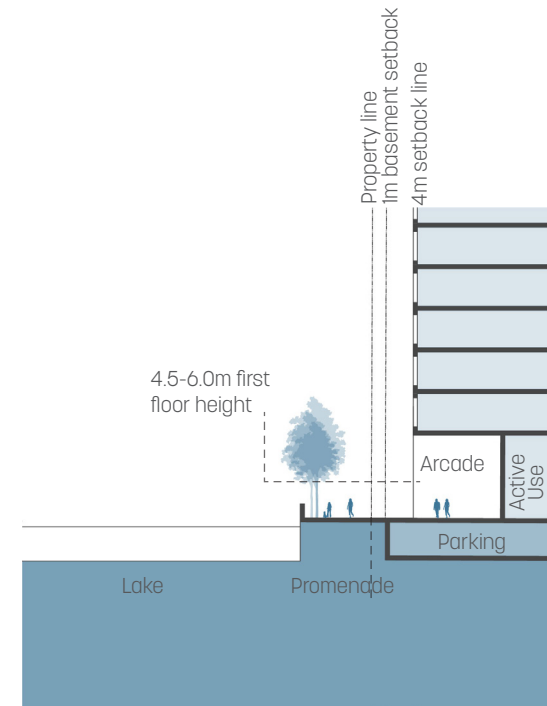


Figure 16: Section of Cultural Centre Building, located within Lakefront Park



3.11.11 The primary entrance to each street level tenant space that has its frontage along a public street or public square should be provided from that street;

3.11.12 Where retail units occupy prominent corner locations, design units to:

- address the corner with well designed, animated storefronts; and
- incorporate corner entrances;

3.11.13 Entrances to retail and commercial tenant spaces shall be operational and directly related to the grade of the public sidewalk;

3.11.14 Entrance doors to retail units shall minimize conflicts between door swings and pedestrians. Design of entries shall also accommodate handicap accessibility, such as pull- and push-side clearance for doors;

3.11.15 Articulate ground floors containing storefronts in ways that support the pedestrian experience at the street level, including creation of:

- a strong sense of rhythm and pattern collectively across frontages;
- subtle recesses and reveals to create depth and visual interest; and
- assistance with spatial perception, orientation and accessibility along retail frontages;

3.11.16 At grade retail and commercial uses shall incorporate the highest standard of storefront design, such as:

- Durable, high quality materials such as metal,

steel, glass, natural stone and brick;

- Vision glazing and doors that allow for views into and out of storefronts;
- Elements such as mullions, glazing bars and transoms to help frame, divide and define storefront window sections and apertures;
- Movable/stackable/tip storefronts where feasible based on internal use, to enhance visual connectivity between the sidewalk and retail interior in the appropriate seasons;
- Signage and patterning integrated into storefront glazing;

3.11.17 When part of a larger single development or individual tenancy, provide coordinated and consistent signage and lighting that integrates with the storefront design, and which complements or acts as an extension of the buildings architectural character;

3.11.18 Retail development shall achieve pedestrian scale, and contribute to comfortable and weather-sheltered pedestrian routes, through design strategies for architectural cantilevers, fixed canopies, awnings and similar features such as:

- complementing the architectural character of the building
- being made of highly durable materials
- extending 1.5 metres to 2.75 metres beyond the retail edge and/or into the public sidewalk area; however, such installations should not conflict or interfere with any streetscape elements or public realm treatments;

3.11.19 Patios and outdoor amenity space shall promote activation of the street and vibrancy and work in tandem with movable/ stackable storefronts that create strong indoor-outdoor connectivity in the appropriate seasons;



3.11.20 Patios shall have a depth that maintains a clear sidewalk path outside of the building lot line to avoid disrupting pedestrian traffic flow, and may in certain instances be recessed into the building as part of the permitted setback from the build-to line.

### Retail Uses at Key Frontages

The key retail frontage of Lakeview Village is Lakeview Square and immediately adjacent frontages. In this zone, ground-level retail has been determined to be a strategic priority.

3.11.21 The design of units fronting onto Lakeview Square shall create a rhythm of storefronts that creates an urban square experience, through strategies such as providing narrow storefronts (approximately 10.0 metres in width) and limiting distances between entrances of successive storefronts (approximately 10.0 metres, based on actual retail uses);

3.11.22 Main front entrances and lobbies that serve residential uses above the ground storey shall maintain the integrity of retail activation streets, through strategies such as:

- locating entrances to establish and reinforce patterns of access between blocks of development;
- positioning entrances so that there is minimal interruption of retail units along the block; and
- limiting the width of entrances at the building face (approximately 6.5 metres for a typical condition, these could become wider behind the retail units);

3.11.23 Special attention should be paid to tenancing

of corner units in order to reinforce activation of corner retail. Retail uses such as restaurants, bars and coffee shops will potentially bring more activity to corners (compared with standard service uses).

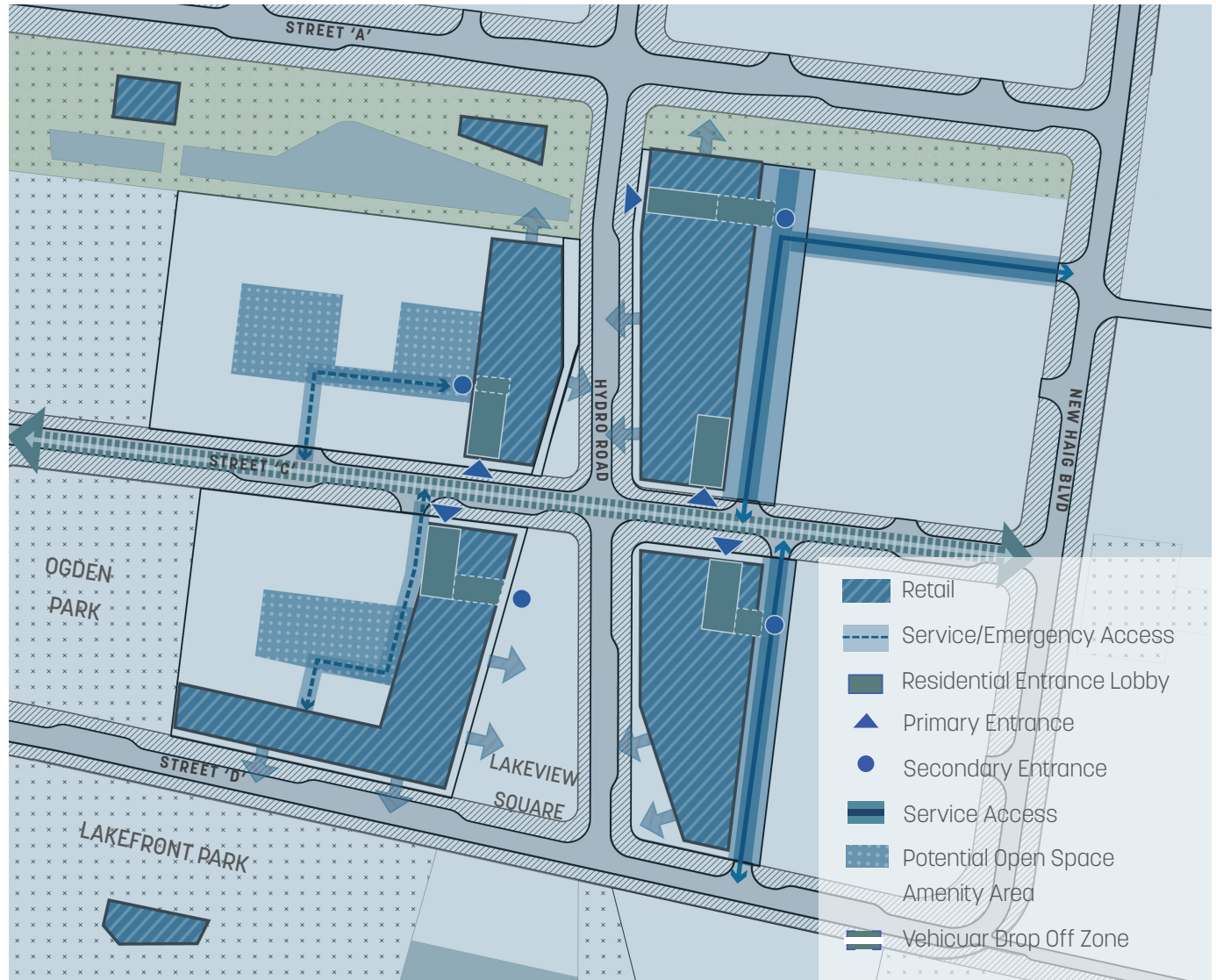


Figure 17: Lakeview Square Ground Floor Retail Plan

## Commercial/Office and Institutional Use Conditions at Grade

Commercial office and institutional uses will occupy the ground floors of certain blocks within Lakeview Village overall, and in particular in the Innovation District zone focused along New Haig Boulevard. Commercial office and institutional uses at ground-level should contribute to a vibrant streetscape and public realm;

3.11.24 Buildings incorporating at grade commercial and/or institutional uses should be designed to ensure that such uses will be directly related to the public sidewalk and will incorporate operating entrances, doorways and windows oriented to the street; along with signage and lighting to provide animation, interest and variety in the streetscape;

3.11.25 Retail uses, communal and shared spaces such as cafeteria, atrium or hallway that service the commercial and institution uses should occupy as much as possible (approximate minimum of 75%) of the frontage along the Street 'I' (the extension of Haig Blvd);

3.11.26 Buildings shall soften the edge where vertical elevations meet the ground plane, through strategies such as canopy trees, seating areas, and planting between the property line and building face. Buildings should be set back approximately 4.0 metres from the property line. Basements should be set back a minimum of 1.0 metres from the property line.

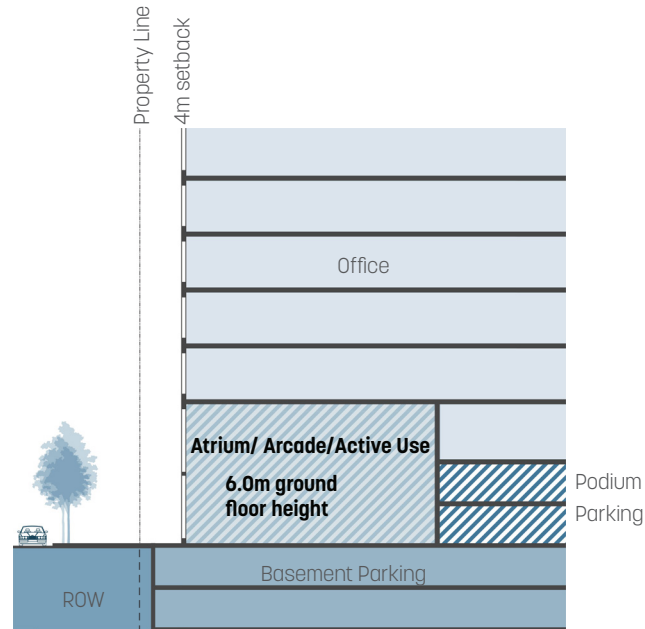


Figure 18: Institutional Use Section





## 3.12 Pedestrian Permeability Conditions

Pedestrian connections shall be included based on the Lakeview Master Plan in certain locations to:

- Provide through-connectivity in certain parts of Lakeview Village;
- Create pedestrian access to block interior open spaces (on blocks where mid-rise perimeters frame an internal-to-the-block open space);
- Prevent unrelieved lengths of building facade;
- Provide access between multiple development blocks and open space, enabling multiple narratives-of-use for pedestrians making their way through Lakeview Village.

Pedestrian connectors within Lakeview are intended to have strong civic quality and robust urban character that is inviting, comfortable, accessible and easy to use for pedestrians.

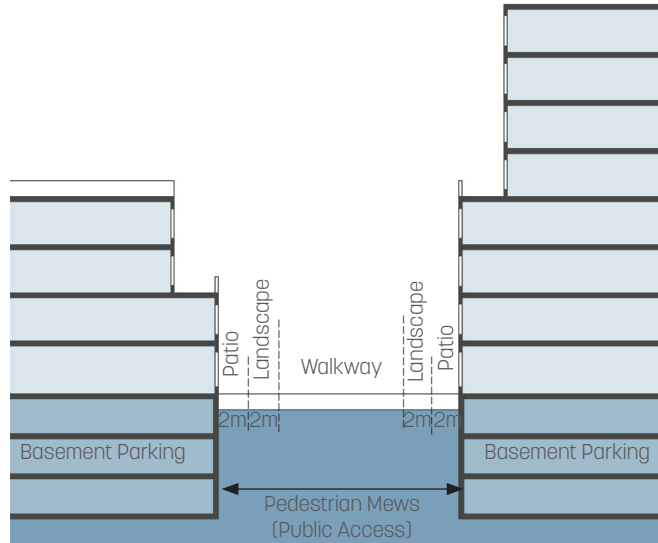


Figure 19: Mid-Block Pedestrian Connection Section

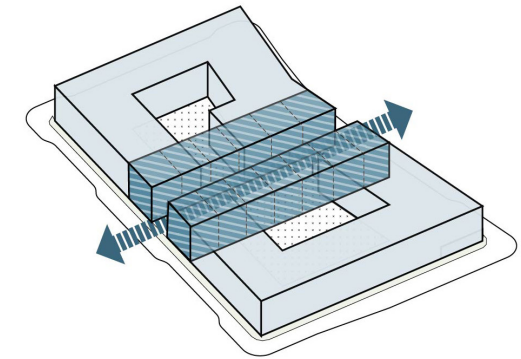


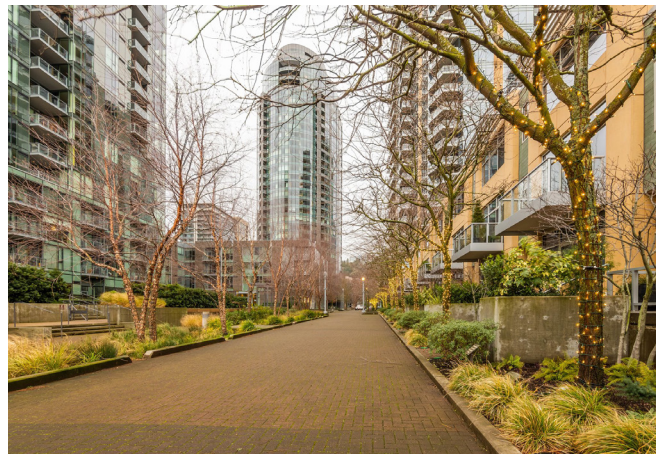
Figure 20: Mid-Block Pedestrian Connection with Integrated Townhomes

### Location and Placement

The design of pedestrian connections should reflect the following:

3.12.1 Pedestrian connections shall be located to provide permeability and ease of navigation, and provide intuitive connectivity between adjacent development blocks, streets, and open space;

3.12.2 Incorporation of a strong urban expression and civic presence through design and choice of materials and avoid the appearance of back alley or service lane;



3.12.3 Provide a comfortable year-round microclimate for pedestrians;

3.12.4 Follow line-of-desire pathways to the public sidewalk, and provide clear sightlines from one end to the other;

3.12.5 Each block with four frontages shall provide a publicly accessible pedestrian connection through to the open space in the block's interior, with a minimum of two walk-through openings, and one open to air opening at one of the street frontages;

3.12.6 Open-to-Sky connections should have an overall width of approximately 12.0 metres, between building faces, incorporating a walkway zone having approximate width of 4.0 metres. This width should increase to approximately 18.0 metres in the case of the mid-block connector north of Waterway Common (Blocks 6 through 9);

3.12.7 Portal connections shall be sized to be a welcoming public opening (approximately a clear width of 12.0 metres, height of 9.0 metres);

3.12.8 Design of portal connections shall prevent the creation of a tunnel by limiting their depth (approximately 12.0 metres depth perpendicular to roadway).

### Pedestrian Connections Relationship to Built Form

Development that incorporates a pedestrian connection should provide an appropriate sense of enclosure along the length of the walkway by:

- creating a comfortable scale for pedestrians (both horizontally and vertically);
- striving to ensure that the design of built form

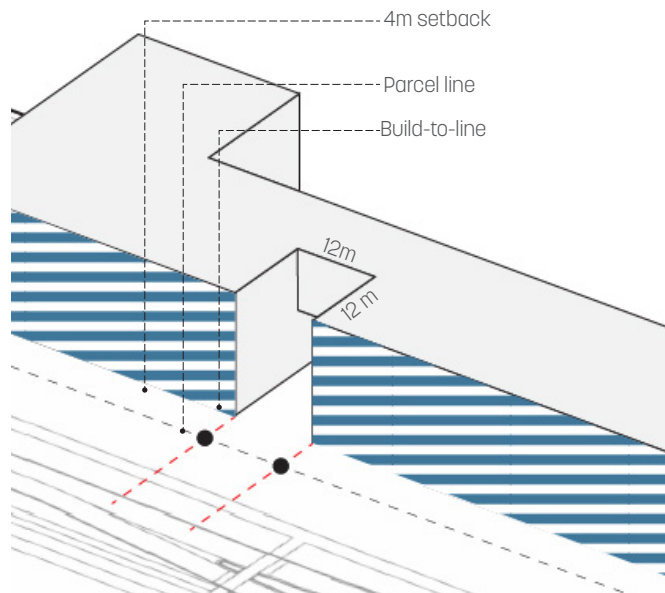


Figure 21: Portal Condition

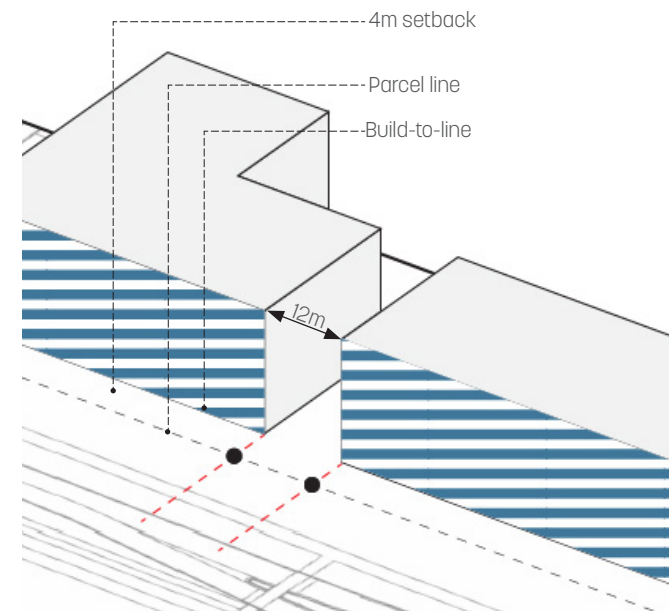
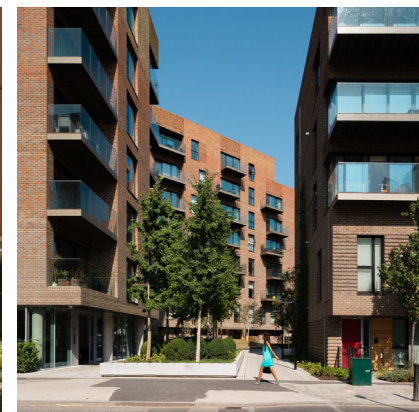
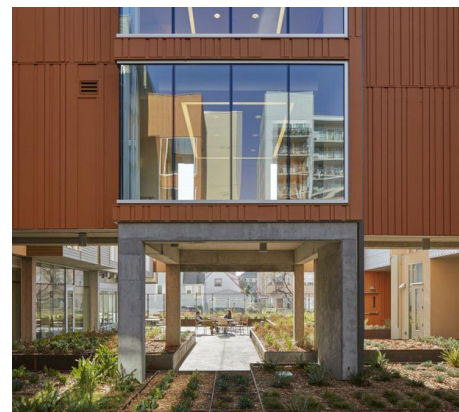


Figure 22: Open-To-Sky Condition



achieves a height that is proportionally equal to the width of the connection similar to a streetwall; and

- incorporating wherever feasible a step-back at the fourth storey facing the connection, to mitigate tunnel-like effects;

3.12.9 Building edges fronting onto the walkway shall provide visual interest through strategies such as providing variation in setbacks, breaking up the massing, or expanding landscaped areas/amenity space;

3.12.10 Where feasible, incorporate continuous active uses including ground related residential units adjacent to walkways along the ground floor;

3.12.11 Provide the maximum amount of vision glazing feasible based on use along the at grade condition to ensure informal surveillance along the entirety of the walkway (i.e. "eyes on the walkway");

3.12.12 Maximise access to sunlight and sky views on the walkway;

3.12.13 Mitigate the effects of wind;

3.12.14 Development should provide pedestrian comfort through the incorporation of:

- building massing and articulation;
- architectural cantilevers, fixed canopies or awnings;
- pedestrian scale lighting;
- landscape treatments; and
- public art.

### Access to Walkways

Pedestrian connections should be identifiable from the public sidewalk by:

3.12.15 Articulating building façades that signify the access through treatments such as emphasized corners, modulation, step-backs, use of colour, materials, patterns and/or textures;

3.12.16 Utilization of elements that signify "threshold" such as low gateposts (compatible with the building design), and lighting standards;

3.12.17 Signage that is visually integrated with the built form or design of the connection;

3.12.18 Enhanced landscaping;

3.12.19 Paving that is generally consistent with the public sidewalk;

3.12.20 Alignment with adjacent bump-outs in the roadway to facilitate pedestrian crossing where connectors on either side of the roadway meet the roadway.

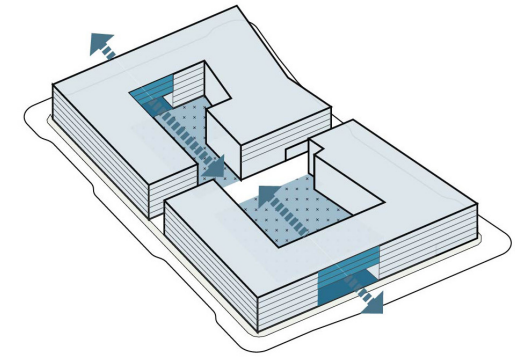


Figure 23: Portal Condition for Mid-Block N-S Pedestrian Connectivity



## Landscape and Amenities

To promote the pedestrian experience, the following landscape treatments should be provided within pedestrian connections where such connectors traverse multiple blocks (Consider also providing the elements below as part of the landscape treatment of the interior of individual blocks):

3.12.21 A combination of soft and hard landscape treatments to define the walkway edges and amenity areas such as trees, water features, public art, pavers and planters;

3.12.22 Plant species that create visual and seasonal interest;

3.12.23 A cadence of aligned trees along the walkway, to provide shade and visual interest;

3.12.24 Sufficient uncompacted soil volume per tree (approximately 30 cubic metres for large trees) to ensure that trees flourish (note: structural soil will not be permitted);

3.12.25 Provide an appropriate clearance between the finished grade of the mid-block connection and the top deck of any below-grade structure to allow for healthy tree growth (typically minimum 1.5m depth of soil, not including the drainage layer) or possibly lesser depths for smaller stature vegetation;

3.12.26 Sod will generally be discouraged;

3.12.27 Benches and/or built-in seating spaced at regular intervals along the length of the walkway;

3.12.28 Pedestrian scale lighting that enhances

facial recognition for a sense of safety;

3.12.29 Bike racks where appropriate;

3.12.30 Waste and recycling receptacles;

3.12.31 Where desirable, patios internal to the walkway may be incorporated to support active uses, provided they do not obstruct the walking path;

3.12.32 High-quality paving materials that have a strong civic presence and which satisfy AODA standards.

## 3.13 Utilities General Guidelines

3.13.1 Above ground utilities, such as transformer, gas regulatory station, and meters should not be prominent along street frontage, and they should be appropriately screened from public view to ensure that such building services do not take away from the scenic quality of public realm in Lakeview Village;

3.13.2 Above ground utilities and ventilation shafts should not be located within any required landscape buffer;

3.13.3 Ventilation shafts shall be set back to avoid negative impacts on pedestrian experience on public sidewalks (approximate minimum of 2.0 metres from any street lot line);

3.13.4 Above ground utilities, meters, gas pipes, and other building services should be incorporated into the building footprint to reduce their visual, odorous

and acoustic impacts on the public realm;

3.13.5 If district energy is implemented, required mitigation of on-site mechanical equipment will in large part not be necessary, except for localized equipment such as emergency generators. Screening requirements should remain for these elements;

3.13.6 Approval criteria from third party utilities providers shall take priority in the design of service features to ensure functionality, while seeking to uphold the intent of the design guidelines listed above.

# Mid-Rise Elements Design Guidelines

- 4.1 Section Overview
- 4.2 Mid-Rise Elements: All Categories
- 4.3 Mid-Rise Perimeter-Block Elements
- 4.4 Non-Perimeter-Block (Freestanding) Mid-Rise Elements

## 4.1 Section Overview

The vision for Lakeview Village is to create a compact, mixed-use urban fabric that puts people in close proximity to jobs, transit, parks, and a broad range of uses and urban amenities. To achieve this, the master plan calls for street edges that are generally defined at street-level by mid-rise elements (and occasionally by townhomes and/or taller elements that come directly down to ground level).

## 4.2 Mid-Rise Elements: All Categories

4.2.1 The minimum basement setback from the property line should generally be 1.0 metres;

4.2.2 The setback from the property line for building faces should be approximately 4.0 metres, with approximately 80% adherence to the build-to line achieved on 'A' Street frontages, and approximately 60% adherence to the build-to line achieved on 'B' Street frontages;

4.2.3 Minimum setback from the property line should be reduced to 3.0 metres in Lakeview Square;

4.2.4 Consider providing a change in materials, textures, patterns, colours, placement of bay windows, windows and balconies, and architectural details on building frontages to provide a higher level of details and create a sense of smaller scale buildings;

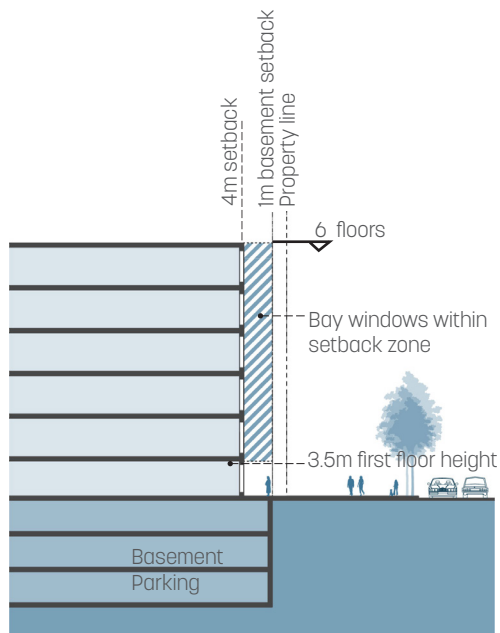


Figure 24: Typical Mid-Rise Condition

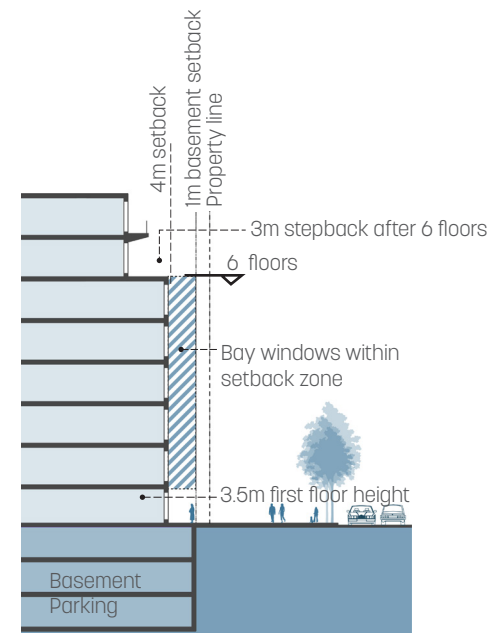


Figure 25: Typical Mid-Rise Condition with Step Back



#### 4.2.5 Design of street walls in mid-rise buildings should consider:

- Base: to define the at-grade level;
- Middle: to define the upper storeys; and
- Top: to express the termination of the streetwall at the step-back or roof line, parapet, related floor juncture or storey, in a way that articulates the expression and character of the building;
- Elements that may accentuate floor and/or ceiling junctures and transitions between subsequent storeys. These may include horizontal expression lines, such as belt-courses, cornices, banding(s) or architectural treatments integrated into the streetwall expression;

#### 4.2.6 Establishment of a rhythm, with frequency and articulation, across the streetwall by deploying elements that are vertically oriented and scaled to the pedestrian to include:

- Fenestration, including vision glazing, apertures, bow, bay, storefronts, dormer and monitor-style windows;
- Recesses, reveals, expression lines or other architectural treatments to create interest for the pedestrian and which serve to break up elongated facades;

#### 4.2.7 Consider the proportion of glazing to solid wall materials in the design of facades in order to achieve a higher level of visual interest;

#### 4.2.8 Consider designing corner lot buildings with special massing and architectural treatments on both streets to give prominence along the frontages and visually distinguish these sites and their corner

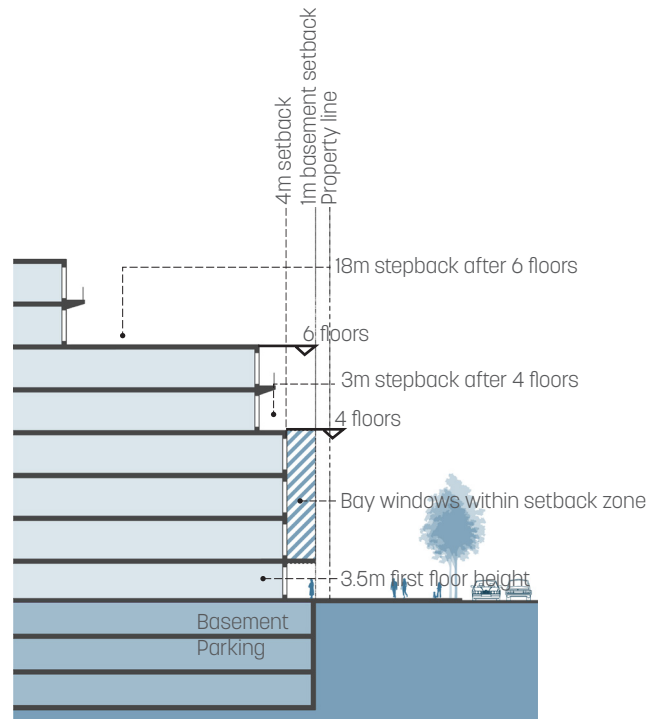
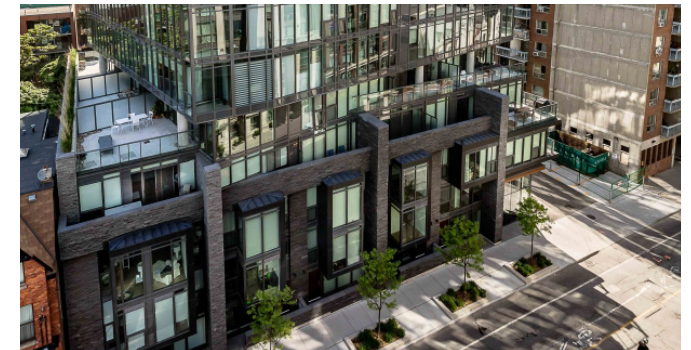


Figure 26: Mid-Rise Condition facing Street 'D'



conditions;

4.2.9 Express principal building entrances with a height, proportion and level of articulation that is compatible with the ground floor storey height, so that entrances are visible and serve as focal points for orientation and access from the street;

4.2.10 Incorporate upgraded doors for entrances, including egress and fire exit doors fronting onto streets;

4.2.11 Design balconies to be architecturally integrated into the design of the building façade incorporating high quality materials, balustrades and railings;

4.2.12 Consider partially screening balconies to provide some privacy for users, and so that materials that are stored on them are not excessively visible from the public realm;

4.2.13 Integrate, conceal and screen roof top equipment into the complete building design;

4.2.14 Consider constructing building exteriors from enduring, materials such as clay brick, stone, metal, glass and wood in order to create durable buildings;

4.2.15 Consider using building materials that are energy efficient and/or those that have been reused or recycled (Please consult the City of Mississauga's Green Development Standards for sustainable approaches and further direction);

4.2.16 Integrate building signage with the

development as sign-bands or fascia signs, and as an extension of the building's architectural expression (Except for wayfinding signage and commemorative plaques, and/or by special consideration, pylon and stand-alone signs are not permitted in Lakeview);

4.2.17 Establish a maximum height of 8 storeys for mid-rise elements;

### Mid-Rise Height and Massing

The design of mid-rise elements, whether perimeter-block or free-standing buildings, requires careful consideration in terms of their role as the primary defining elements in containment of the street, and the achievement of pedestrian scale qualities. Mid-rise elements will need to consider appropriate heights in relation to the street right-of-way, streetwalls, and step-backs at the upper storeys, including ways in which sunlight can reach sidewalks, mid-block walkways, and block interiors to support a light-filled, vibrant public realm.

The following guidance applies:

4.2.18 Establish a 3 storey minimum height for mid-rise elements;

4.2.19 To support a pedestrian scale and contain the street, mid-rise elements should incorporate a street wall placed at the build-to line, having 3 to 6 storeys prior to step-back in height. For buildings that have an edge facing Lakefront Park, the step back should occur along that edge, at a suggested maximum of 4 storeys prior to step-back;

4.2.20 Mid-rise elements shall mitigate the

perception of height and create a comfortable scale for pedestrians, through strategies such as incorporating step-backs at the upper storeys. Step-backs at the sixth storey or lower shall generally be a minimum of 3.0 metres; Balconies shall not project more than 1.5m into the step-back zone, and shall be of light materials so as to not dilute the visual intent of the step-back;

4.2.21 Mid-rise elements shall maximize sunlight on the public realm in accordance with the parameters set out in the City of Mississauga's Standards for Shadow Studies; and mitigate the effects of wind on the pedestrian environment in accordance with the parameters set out in the Pedestrian Wind Comfort and Safety Studies;

4.2.22 Taller (mid-high-rise or high-rise) elements shall be stepped back from the mid-rise perimeter-block element below, with stepping back occurring above the top tier of the perimeter-block edge, in order to establish a clear definition between the mid-rise perimeter-block and taller elements (Note: There may be instances where it is architecturally advantageous to have the taller elements be coplanar or even project in front of the mid-rise perimeter-block elements) architectural merit and sun penetration; these should be studied and presented for review on a case-by-case basis);

4.2.23 Mid-rise buildings abutting or facing onto parks and open spaces, with or without mid-high-rise and high-rise elements above, should maximize opportunities for sunlight exposure in accordance with the City of Mississauga's Standards for Shadow Studies;

4.2.24 The streetwall facing on to the Lakefront Park should be 4 storeys before step-back per the



endorsed Development Master Plan. A 45-degree angular plane set at the property line at the height of the top of the 4th floor parapet, for the first half of the block back from the build-to-line facing the Lakefront Park.

4.2.25 When tall residential building proposals are phased, ensure that party walls of mid-rise elements below these taller elements are architecturally treated and enhanced to address interim conditions.

### Double or Triple floor Dwelling Units At Grade

4.2.26 Ground floor residential units with entries from the sidewalk, at grade, should be considered for street frontages and along mid-block connections, including along 'A' Streets, to create an active residential frontage and vibrant urban condition, which can be a part of the essential character of 'A' Streets in Lakeview Village. If implemented, these "front doors" should have the look and feel of inset or projected townhome entries with associated entry steps, sidelight windows, front door, plantings, etc.

4.2.27 Street frontage in front of residential units at-grade shall protect the privacy of residents and create a clear definition between public and private realm, through strategies such as providing a sufficient setback (approximately 4.0 metres) to accommodate a buffer. A landscape buffer should provide sufficient space (approximately 2.0 metres) to sustain the growth of small trees, shrubs and other plant materials with substantial height and foliage volume, so the plants collectively can function as a proper landscape screen, contribute to a greener image of Lakeview Village, and a better pedestrian experience;

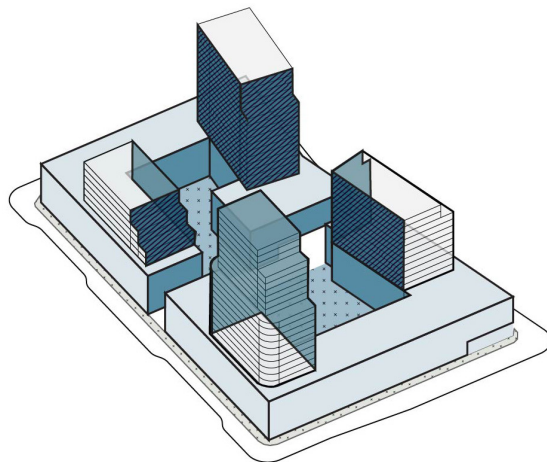


Figure 28: Dwelling Units at Grade in Mid-Rise Elements, Facing Interior Courtyard



4.2.28 Raised planters may be used to provide sufficient soil volume, and may be built and/or clad with similar materials as the building. Raised planters may be utilized as part of the overall architecture, running along the front of a building. If utilized, such planters should not interfere with public passage or overall barrier-free access to front doors of buildings;

4.2.29 Patios should ideally be provided only when associated with an entry area for a front door to the street of an embedded townhome or duplex unit. If provided, these should be at the same level with unit entrance for ease of use, but should be elevated moderately above the grade at public sidewalks to clearly define the public and private realm. Patios should ideally only be incorporated as part of units that have front-door entries from the street, as part of an entry-patio ensemble. Entries and patios may be recessed back from the street wall to create more space and accentuate sense of entry to units.

4.2.30 The residential units should be setback minimum of approximately 4.0 metres from street lot lines to accommodate the Landscape Buffer, with additional indentation at first floor level to accommodate townhome/duplex entries and their associated patios;

4.2.31 If patios are implemented, privacy screens can be provided between units, but should not be more than 1.8 metres in height and should be built with materials that echo the building's exterior materials (pressure-treated wood should not be utilized);

4.2.32 The ground floor should be raised above the

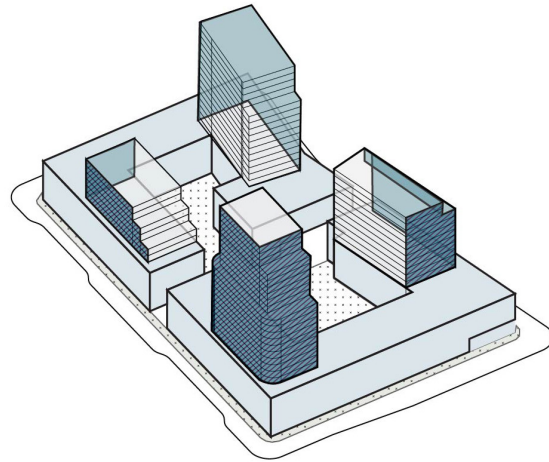


Figure 29: Dwelling Units at Grade in Mid-Rise Elements, Facing the Street



sidewalk level, subject to requirements for building entry, average grade, etc.;

### Mid-Rise Single Floor Dwelling Units At Grade

4.2.33 Single floor residential units should generally not be located along 'A' Streets, and when present shall be designed to provide resident privacy and contribute to an active street frontage;

4.2.34 To protect the privacy of residential units at grade, especially their bedrooms, consider providing a landscape buffer along the street frontage with a mix of plant materials to screen the views from public sidewalks and clearly define the public and private realm;

4.2.35 Single floor units at grade may incorporate a street-facing patio on 'B' Streets, and should ideally not incorporate a full townhome/duplex-type entry;

4.2.36 Raised planters can be used to provide sufficient soil volume to sustain more substantial shrubs and other plants, rather than merely cover the ground;

4.2.37 Consider making the open spaces within the setbacks accessible for active uses. A no-man's dead space along street frontage shall be avoided.

### Mid-Rise Dwelling Units At Grade Along A Public Park

4.2.38 A landscape buffer should be provided along the public park to create a landscaped zone, clear definition for public and private realm, and accommodate drainage and grading;

4.2.39 Walkways that provide access to the ground floor units should be within privately-owned lands, and should not encroach onto public lands;

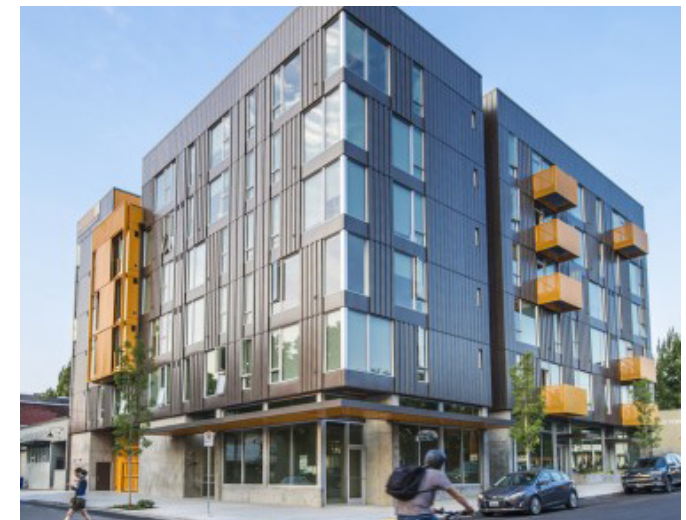
4.2.40 Patios facing onto the public park may be permitted, to provide more 'eyes on the park' and create a more engaging edge condition, subject to safety and privacy considerations of residential units located in these locations;

4.2.41 Residential units along a public park shall provide a sufficient setback to accommodate features such as a landscape buffer (approximately 5.75 metres) if there is a walkway;

4.2.42 Walkways along the lot lines with doorway connections to ground floor units should be setback from the units to protect their privacy (approximately 3.0 metres);

## 4.3 Mid-Rise Perimeter-Block Elements

The predominant building typology within Lakeview Village is mid-rise perimeter-block elements that frame the street on all sides of a block, while creating an interior garden or courtyard condition (accessible at ground-level from the exterior of the block through openings). This typology may function entirely as a semi-continuous mid-rise element, or may have mid-high-rise and high-rise elements situated atop of it at various locations. Townhomes may be embedded in the ground-level



of these blocks, and smaller free-standing mid-rise or townhome buildings may be located within the interior garden/courtyard spaces.

The design of perimeter-block mid-rise elements requires careful consideration due to the fact that they will in many cases form the full exterior expression of a particular block, as well as provide containment of the inner-block open space. As such, careful consideration must be paid to walk-through openings in perimeter-blocks, and to articulation of these facades (insets, bump-outs, bay windows, balconies, etc.)

4.3.1 The maximum continuous length of mid-rise perimeter-block building unbroken by an open-to-sky or portal opening should generally be 70.0 metres or less, where feasible.

## **4.4 Non-Perimeter-Block (Freestanding) Mid-Rise Elements Massing and Articulation**

Guidance for massing and articulation for freestanding mid-rise buildings is similar to that of Mid-Rise Perimeter-Block Elements. Given, however, the tendency for free-standing buildings to function as visual objects in the landscape to a greater extent than perimeter-block elements, care should be taken with consideration of the relationship (proportion, materiality, fenestration, etc.) of long walls to shorter end-walls.

# Taller Elements Design Guidelines

- 5.1 Section Overview
- 5.2 Taller Elements: General
- 5.3 Taller Elements Massing, Articulation and Building Facade Treatments
- 5.4 Site Orientation
- 5.5 Key Landmark Locations and View Corridor Anchor Elements
- 5.6 Overall Block Strategies and Synthesis
- 5.7 Non-Traditional/Non-Conventional Building Types

## 5.1 Section Overview

Mid-rise buildings provide the greatest amount of definition of the character of Lakeview Village to a person at street-level, in keeping with the community's intended character as a "predominantly mid-rise community." Taller building elements provide variety, increase the number of dwelling units (and hence enhance the sense of an urban community and the number of people located close to transit), and function as "punctuation marks" at the terminus points of key view corridors.

Given their visual prominence and potential impacts (sun, wind, views, etc.), tall residential buildings are accompanied by civic obligations and responsibilities. Spatially, they articulate the City pattern and urban structure, while visually reinforcing the importance of Lakeview Village, and defining the skyline of the City along Lake Ontario.

Tall residential buildings' location, orientation, relationship to the street, height ranges, built qualities, architectural treatment, and their ability to enhance the pedestrian environment by minimizing shadowing and unfavorable wind conditions of the public realm, should be carefully considered. These buildings should be designed in ways that protect important views, minimize impacts on natural resources, and that accommodate a range of uses, in addition to being a support for transit viability.

Tall residential buildings should also ensure that fundamental quality of life aspects are supported, such as access to sunlight, sky views and privacy for those that live, work and visit Lakeview Village.

## 5.2 Taller Elements - General

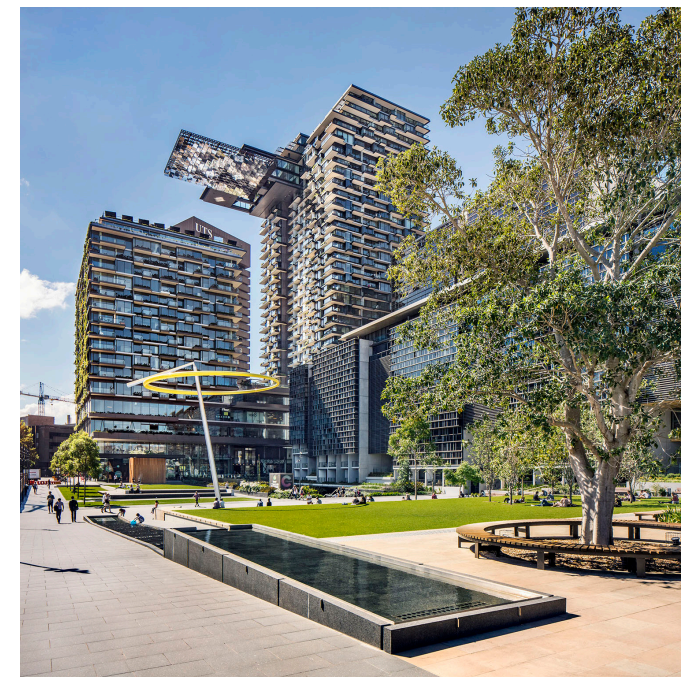
5.2.1 Residential Tower Floor Plates shall be designed to achieve proportionately slender tower profiles based on their heights. "Tower Floor Plate" refers to the average floor area of all storeys within that portion of a building or structure or part thereof located above the podium, measured to the exterior walls of each storey of a building or structure. Tower Floor Plates should generally follow the following maximum areas, with exceptions that can be made based on architectural form:

- 0 - 30 Storeys = Up to 750 m<sup>2</sup> (Tower Floor Plate)
- 31 - 49 Storeys = Up to 800 m<sup>2</sup> (Tower Floor Plate)
- 50 Storeys and over = Up to 850 m<sup>2</sup> (Tower Floor Plate)

5.2.2 Taller elements shall be separated to provide sky views, natural daylighting, adequate privacy, and to minimize wind conditions and cumulative shadow on the streets, parks, and open spaces (approximately 30.0 metres between portions of towers above the 6th storey).

## 5.3 Taller Elements Massing, Articulation and Building Facade Treatments

Taller (mid-high-rise and high-rise) residential buildings in Lakeview should generally be designed to consist of three constituent parts (though these Guidelines recognize that certain innovative buildings defy easy categorization, and the Guidelines therefore are intended to accommodate



variety and innovation):

## Base

In general, taller residential buildings within Lakeview Village will have a relationship with a “podium” base that is formed by a mid-rise perimeter-block building. In these cases, the base of the taller element is described by the mid-rise building Guidelines. In some instances, however, it may be beneficial to accommodate either free-standing taller buildings, or taller buildings that partially overlap their mid-rise bases, and are hence expressed partially at ground-level. In these instances, taller residential buildings (mid-high-rise and high-rise) that express themselves at ground level should consider:

5.3.1 Having a ground-level that is distinct from the middle section (outset or inset), with a greater amount of glazing. Approximate ground-level height (floor-to-floor) at these conditions should be 4.5 metres. Consideration should be given to creating a double height ground floor condition containing duplex units where appropriate and feasible;

5.3.2 Having no direct-to-exterior unit front doors in the faces of the building. That is, the building’s front door (and required services and emergency entries/exits) should ideally be the only entries to the building;

5.3.3 Having no semi-private or private patios for ground-level units at the base of the building;

5.3.4 Having overhangs and/or canopies over entries to protect users from wind downdrafts. The sizing, structural design, and other considerations of

Figure 30: Typical Building Elements

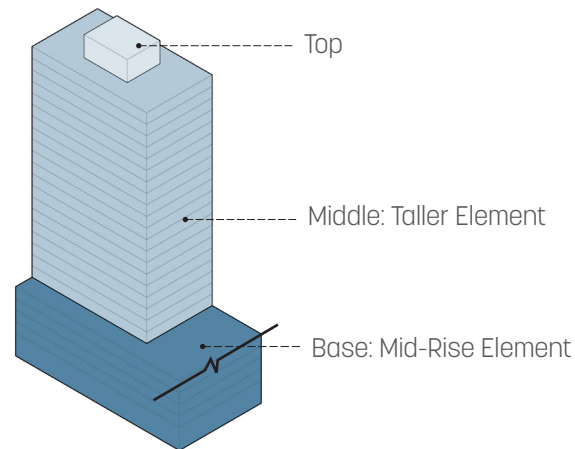
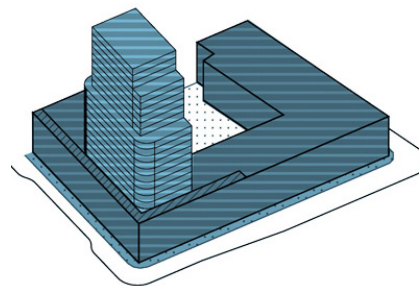
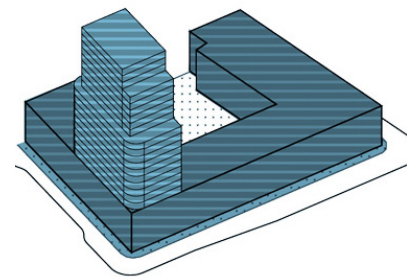


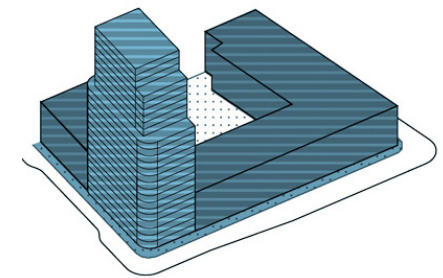
Figure 31: Possible Relationships Between the Taller Elements Middle and Base



ON MID-RISE PERIMETER BLOCK ELEMENT WITH STEP BACK



ON MID-RISE PERIMETER BLOCK ELEMENT WITHOUT STEP BACK



EXPRESSED FROM GROUND LEVEL UPWARDS



these elements should result from quantifiable wind studies conducted using the as-designed elements;

5.3.5 Having the exposed at-grade portion of the taller building element, when deployed at corner locations, create a highly animated and active condition, potentially with entrances in these locations, in order to positively reinforce and celebrate the corner;

### Middle

The floor plate size and shape of the middle portions of conventional taller elements should be designed with appropriate dimensions for the site; the middle shaft should be located and oriented on the site and in relationship to the “podium” and adjacent buildings.

Compact slim towers and small floor plates minimize shadowing; maximize separation, and views between buildings. They also improve privacy and reduce overlook impacts on adjacent streets, parks, open spaces and properties. Consider designing and articulating of the floor plates to break down the mass of the building and to create ‘street interest’ and enhance skyline character;

### Top

Taller elements contribute to the image of Lakeview Village. Consider designing the tops of the tallest elements as distinctive elements against the skyline, when seen from the street, from a distance, or from Lake Ontario. Moreover, a well-designed roofline creates opportunities for views to distinctive landmarks, including orientation and wayfinding.

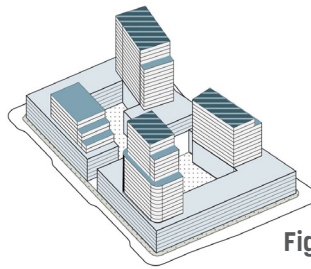


Figure 32: Taller Elements - Top

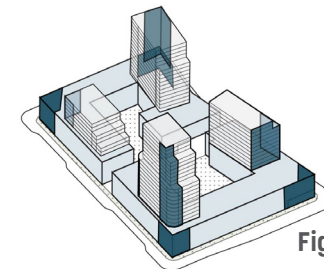


Figure 33: Taller Elements - Corners





5.3.6 Consider designing the uppermost floors of the tops of tall buildings to achieve a sculptural or distinctive profile;

5.3.7 Design the tops of tall buildings to integrate and screen the mechanical penthouse function and other roof top units into the complete building design;

5.3.8 Use materials, finishes and patterns that are consistent with the overall building design and architectural expression;

5.3.9 Potentially incorporate ways in which the roof top expression can be subtly illuminated and enhanced with architectural lighting effects during evening hours.

#### Taller Elements without Mediating Base

5.3.10 Other than guidance for differentiation of ground-level articulation of taller elements, it shall not be a specific requirement for taller elements that are expressed at ground-level to have a defined, conventional “base” or “podium” that is distinguished in material, character or form from the middle and upper portions of the building, other than that the pedestrian scale shall be strongly considered.

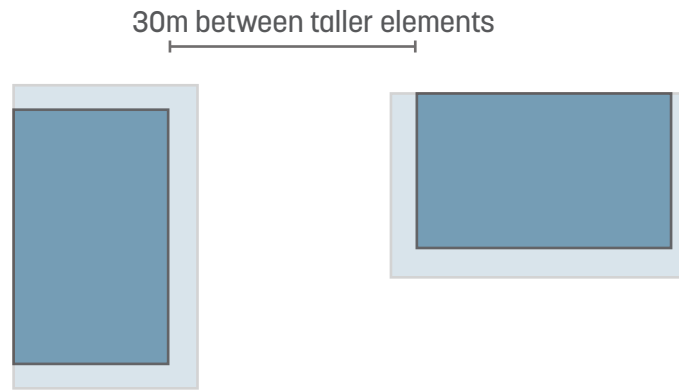


Figure 34: Taller Elements General Guidelines

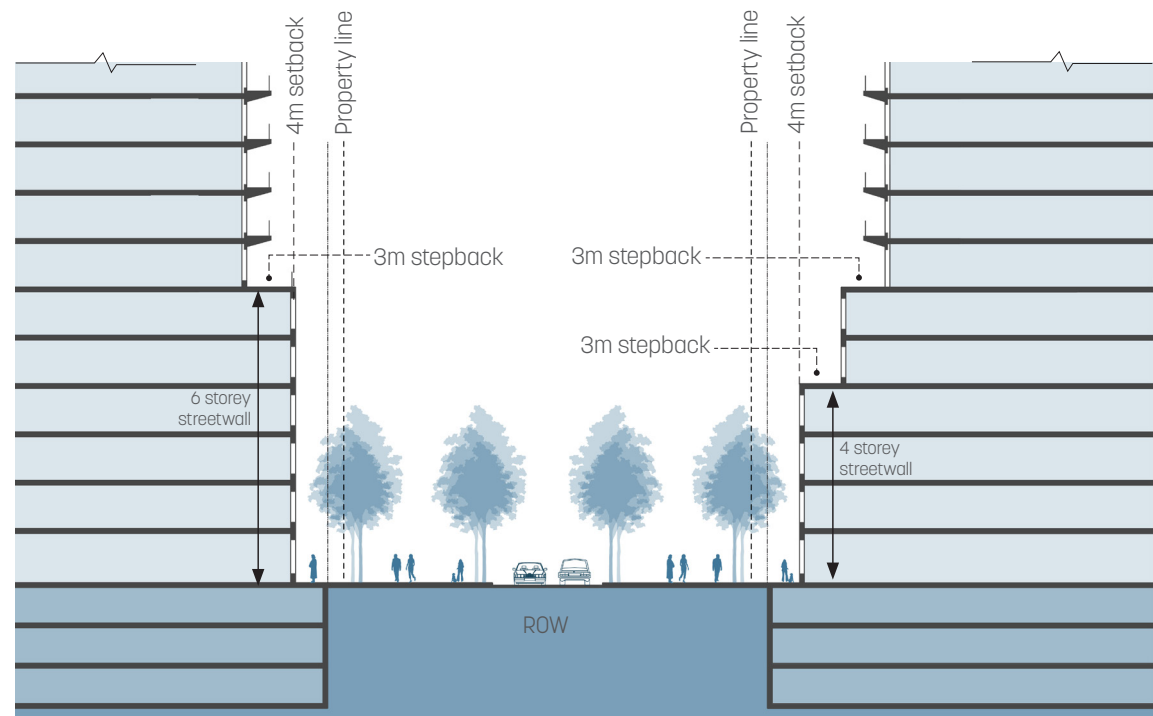
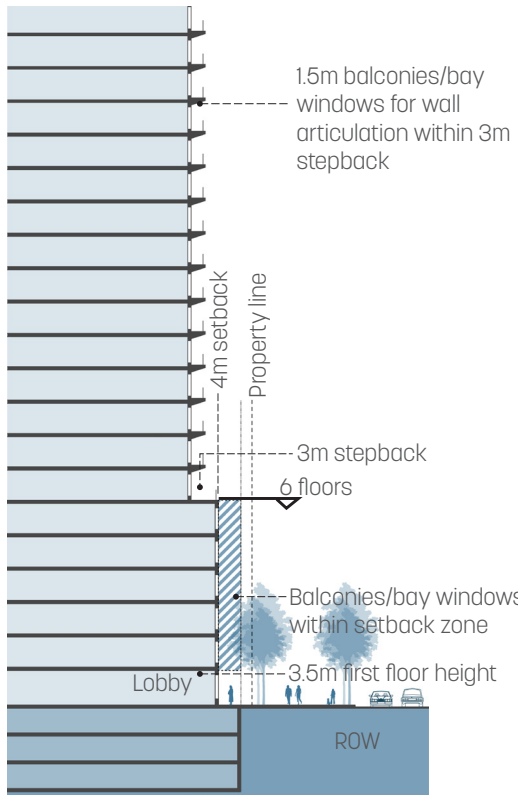
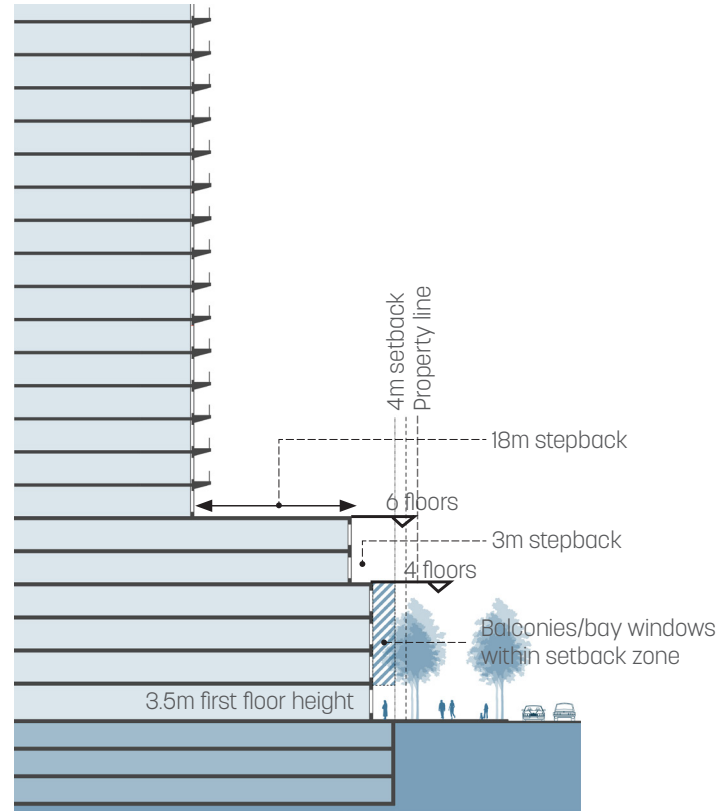


Figure 35: Prototypical Section of Two Taller Elements Across a ROW

**Figure 36: Varying Taller Element Conditions by Block Type**



**TALLER ELEMENT CONDITION FOR TYPICAL BLOCK**



**TALLER ELEMENT CONDITION FACING STREET 'D' (WATERFRONT STREET)**



## 5.4 Site Orientation

Taller built elements should be located on their sites to minimize shadow impacts on adjacent buildings, streets, and open spaces.

### Sun and Shadow

5.4.1 Taller elements should be located on the north-west and south-west sides of a block or development site so that shadows fall primarily within the block itself rather than on the street;

5.4.2 Elongated floor plates should be oriented in a manner which minimizes shadow impacts; this should be demonstrated in accordance with any sun/shadow studies that are required by the City;

5.4.3 Adjacent to a park, taller elements should be located to minimize negative impacts on the park;

### Lake Views

Lake views are extremely important within Lakeview Village, being the name-giving element of the project. There is a broad opportunity to afford such views to a wide spectrum of dwelling units. Taller elements should be positioned on each parcel with the objective of enabling neighboring blocks to equally capitalize on views.

### Open Space Views

Lakeview Village equally has a tremendous amount of open space, with attendant opportunities for open space views throughout the community. These are beneficial in themselves, for the residents of dwelling

units, but are equally important for the safety of users within the open spaces (“Eyes on the park”). Buildings should take advantage of the opportunities afforded by open space views without being arrayed in ways that diminish the user experience within the open spaces.

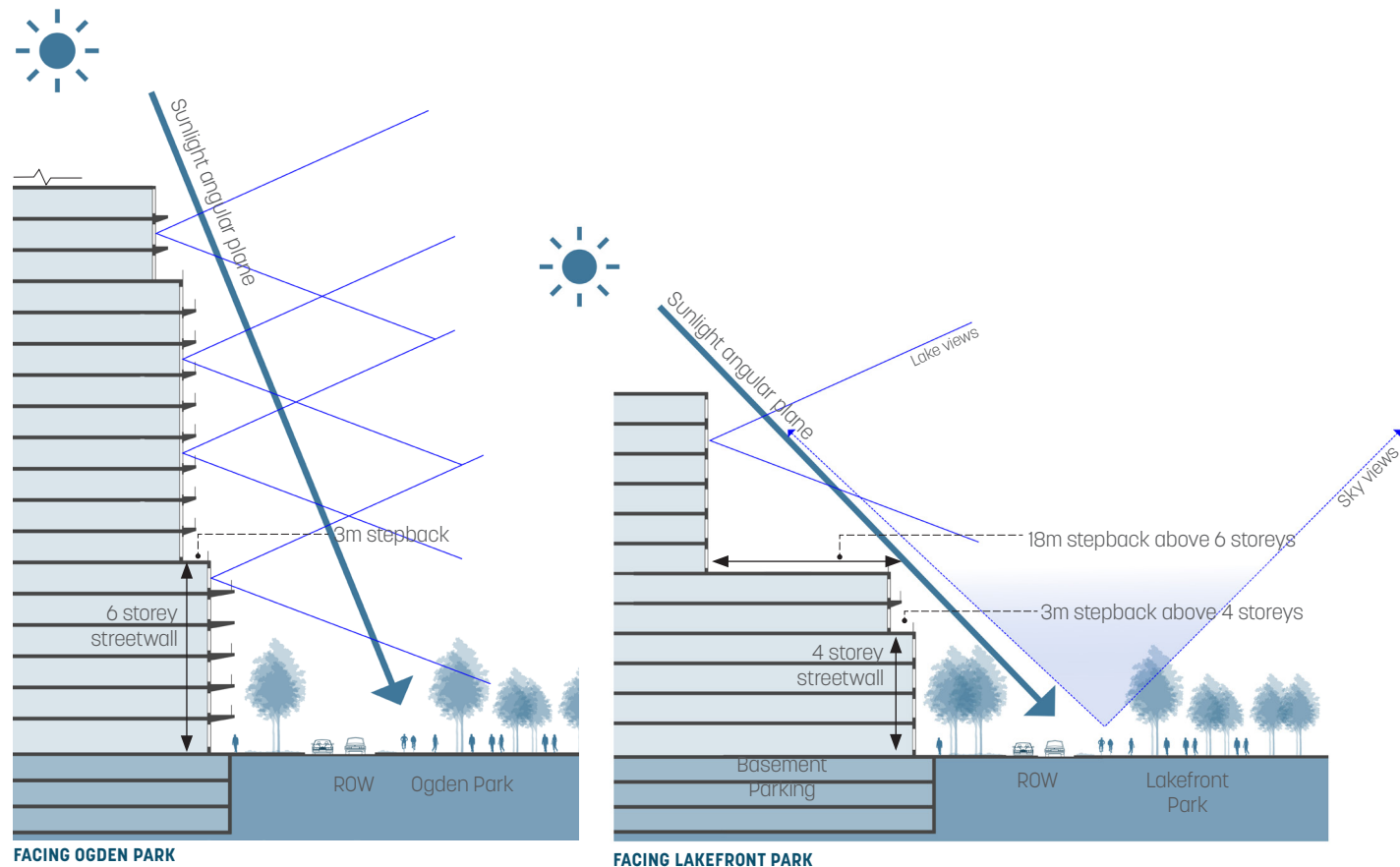


Figure 37: Site Orientation Factors Facing Key Open Spaces

## 5.5 Key Landmark Locations and View Corridor Anchor Elements

There are several important opportunities within Lakeview Village for landmark building elements to provide signature moments, and to enhance the presence of Lakeview within Mississauga and the surrounding community. Exceptional architecture should be implemented at such locations.

The Marina District (Blocks 10-13) and the eastern terminus of Waterway Common (Block 16) are subject to architectural competition. Details about the process, rules, and jury will be determined at a later date.

### Western Gateway at Lakefront Promenade

The building occupying Block 6 should be of high architectural merit, given its prominence at the western roundabout entry to Lakeview Village. This building is an opportunity to establish the community's identity at this western edge, and additionally has access to sweeping views of Lake Ontario and the Lakefront Promenade Park.

### Marina District

The Marina District (Blocks 10 through 12) has, from the earliest days of the conception of the project, been seen as the "punctuation mark" on Lakeview Village. As a result of this, it is the zone of greatest height. This height calls for a responsible approach

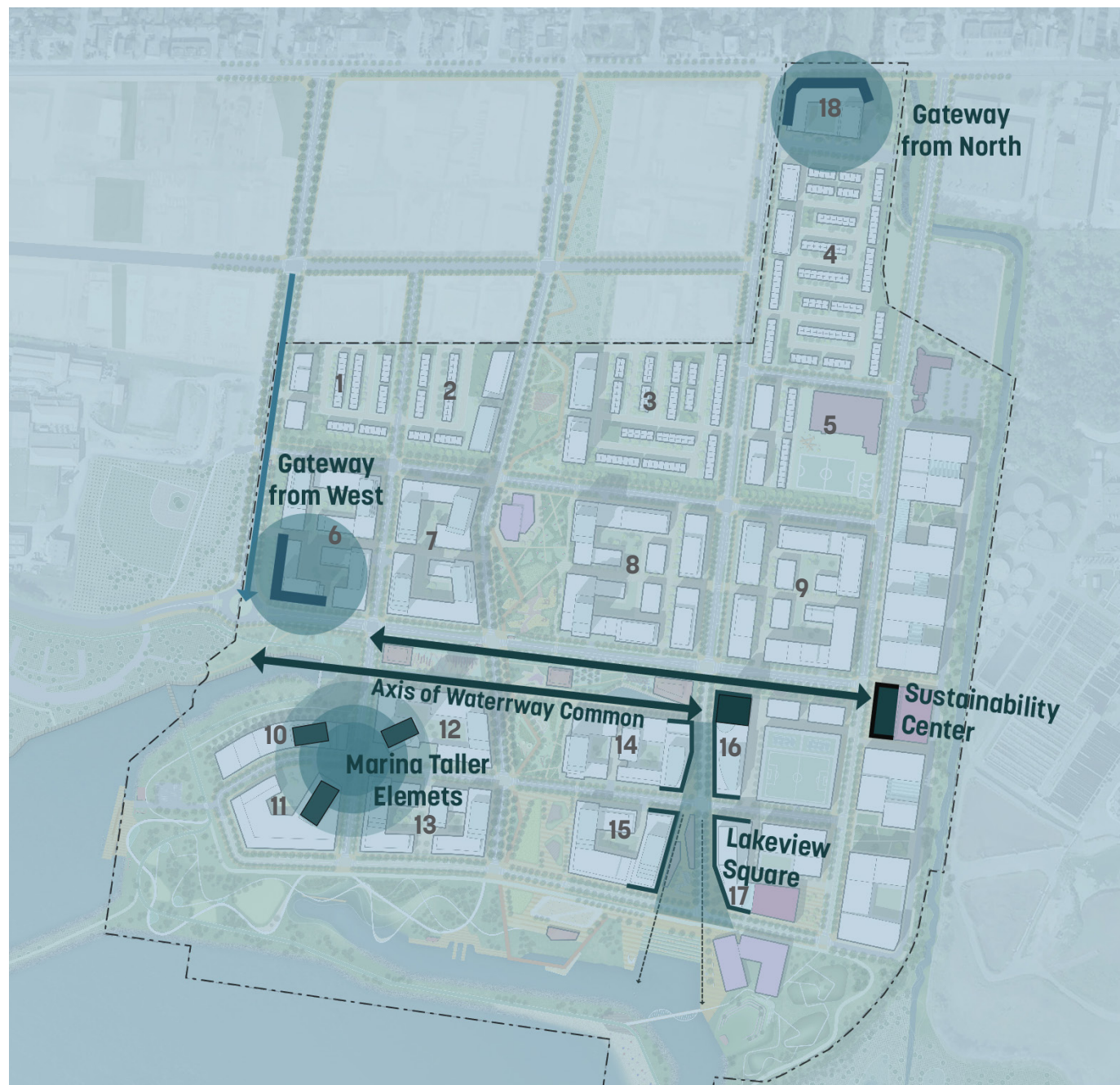


Figure 38: Key Landmark Locations

to building placement, ensuring that views for blocks “behind” the Marina District will have view access, and also ensuring a high-degree of architectural excellence for the Marina District buildings.

### Eastern Terminus of Waterway Common

Due to the fact that the western end of Waterway Common is open to the public lands beyond, the eastern terminus takes on great significance as the anchor of this extended arrangement of open spaces. There are two ways in which the eastern end of Waterway Common functions:

Block 16, on the eastern side of Hydro Road (Street “H”), is a strong opportunity for a taller building element of architectural excellence to function as the visual anchor of the immediately-adjacent open space of Waterway Common;

The proposed Sustainability Centre on the eastern site of New Haig Boulevard (Street “I”) is another terminus, albeit further east, for the view corridor of Waterway Common. This building, while likely to not be tall, is another opportunity for anchoring the viewshed with architectural excellence.

### Northern Gateway at Hydro Road and Lakeshore Road

Block 18, at the intersection of Lakeshore Road East and Hydro Road (Street “H”) is perhaps the primary “front door” to Lakeview Village, given that the lands of Lakeview Village extend up to Lakeshore Road East in this location, and that the intersection is signaled. As such, the mid-rise and taller elements on this block should function as a “beacon” announcing one’s arrival at the community of Lakeview Village.



## 5.6 Overall Block Strategies and Synthesis

5.6.1 For blocks with a mix of mid-rise and mid-high-rise/high-rise components, the following guidelines should be applied to create more a more diverse built form and human-friendly scale of architectural elements at Lakeview Village;

5.6.2 The mid-rise and high-rise elements may potentially be differentiated architecturally into distinct buildings (notwithstanding the fact that basements and mid-rise “podia” may be shared);

5.6.3 Opportunities to create these divisions at the mid-rise levels can be generated through the location of pedestrian ways to/from the interior of the blocks; utilizing these break-points to change architectural articulation;

5.6.4 For blocks with mix of mid-rise and low-rise buildings, strategies are needed to ensure that these differing elements are able to occupy the same parcel. In particular, townhomes should be protected from a sense of excessive overlook by mid-rise and taller elements, and from excessive shadowing at certain times of day.

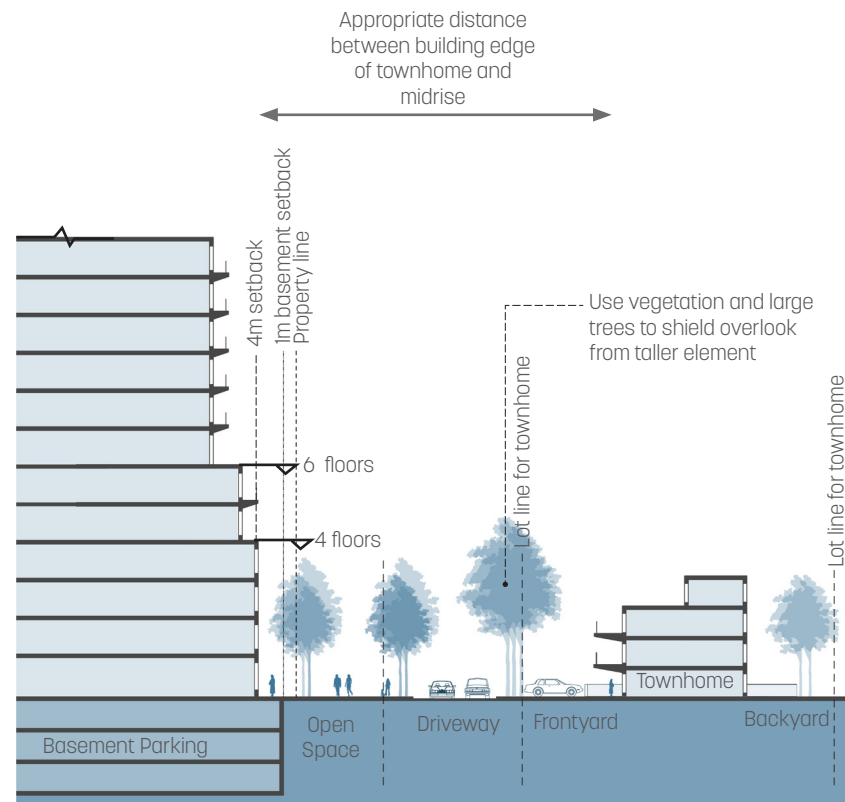


Figure 39: Relationship Between Townhomes and Mid-Rise/Taller Elements

## 5.7 Non-Traditional/Non-Conventional Building Elements

The Guidelines for Lakeview Village have generally been written to guide conventional design and construction within the community - blocks employing traditional perimeter mid-rise buildings (sometimes functioning as a “podium” for more vertical mid-high-rise and high-rise elements).

These Guidelines, however, recognize changing market forces, market preferences, and innovation and creativity in design. As such, these Guidelines are not construed to deter innovation on individual parcels. These strategies should generally be deemed to meet the following criteria:

5.7.1 There is no reduction of on-site open space;

5.7.2 Guidance for the number of walk-through openings per block leading from the block exterior to the interior-block open space is followed wherever feasible;

5.7.3 The guidance for breaking up of continuous lengths of street facade may be relaxed, subject to specific measures to mitigate perceptions of overall block length (indents, reveals, outward-projection, etc.);

5.7.4 Street-walls of excessive height are should not be created;

5.7.5 Sun penetration to interior open space should be demonstrated to be generally similar to that of conventional mid-rise “podium” with “tower” types;

5.7.6 Views from adjacent parcels are not excessively impacted;

5.7.7 The public realm is not overwhelmed by massing and/or height that is inconsistent with the overall objectives for the look and feel of Lakeview Village.

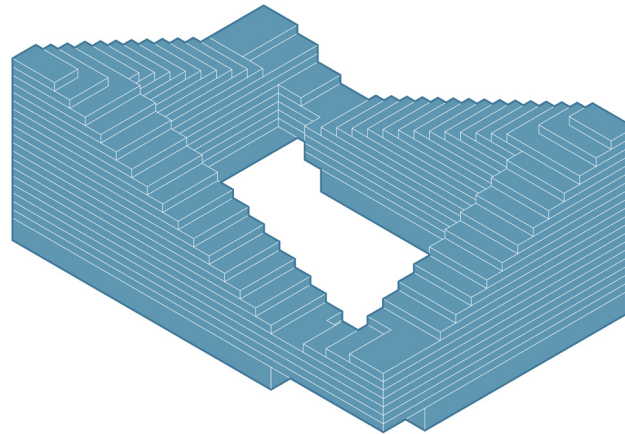
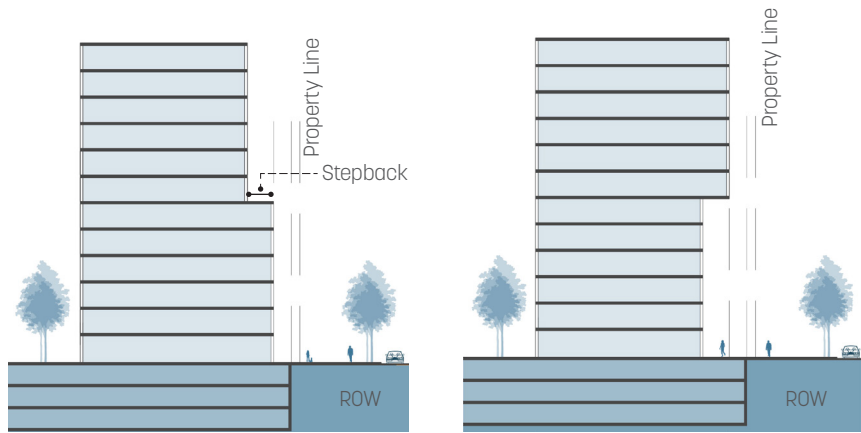


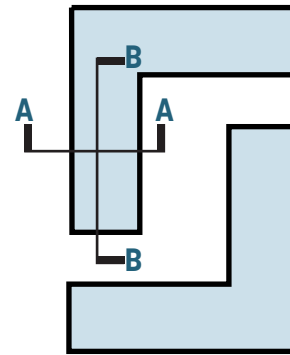
Figure 41: Sample Non-Traditional Massing Approach

**Figure 42: Sections Illustrating Non-Conventional Massing Scenarios**

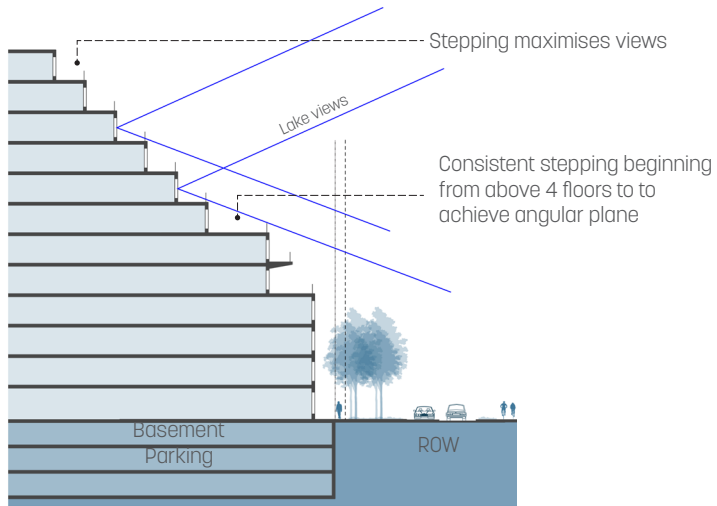


**A: STEP-BACK SCENARIO**

**A: OVERHANG SCENARIO**



**KEY PLAN**



**B: STEPPED PROFILE SCENARIO**

