#### REPORT



# 3403-3445 FIELDGATE DRIVE

MISSISSAUGA, ONTARIO

NOISE IMPACT STUDY RWDI #2406207 August 9, 2024

#### **SUBMITTED TO**

**Forest Glenn Shopping Center** 

Attention: Morgan Dundas Senior Planner, Sajecki Planning Inc. Morgan@sajeckiplanning.com

#### **SUBMITTED BY**

Kathryn Kim, P.Eng. Senior Project Manager Kathryn.Kim@rwdi.com

**Gillian Redman, M.Sc., P.Eng.**Noise and Vibration Engineer
Gillian.Redman@rwdi.com

**RWDI - Toronto Office** 625 Queen Street West Toronto, ON M5V 2B7 T: 647.475.1048 x2031

RWDI - Head Office 600 Southgate Drive Guelph, ON N1G 4P6 T: 519.823.1311



# NOISE IMPACT STUDY 3403-3445 FIELDGATE DRIVE

RWDI#2406207 August 9, 2024



# **VERSION HISTORY**

Index	Date	Description	Prepared by	Reviewed by
1	June 27, 2024	Draft	Junchao Ma	Gillian Redman
2	August 9, 2024	Final	Junchao Ma	Gillian Redman



#### **EXECUTIVE SUMMARY**

RWDI was retained to prepare a Noise Impact Study for the proposed mixed-use development located in Mississauga, Ontario. The proposed development will consist of three (3) residential buildings with commercial spaces on the first floor. The residential buildings are 22-, 18- and 13-storey plus mechanical penthouse. This assessment was completed to support the Zoning By-Law Amendment (ZBA) submission as required by the City of Mississauga.

The following noise control measures are recommended for the proposed development:

- 1. Installation of central air-conditioning so that all suites' windows can remain closed.
- 2. The inclusion of noise warning clauses related to:
  - a. Proximity to commercial land-use

The potential noise levels from stationary sources of sound were evaluated. The most significant sources of sound are idling cars and rooftop HVAC equipment associated with the adjacent Tim Hortons drive-thru and Shell gas station. With the implementation of mitigation measures, including central air-conditioning and a warning clause, the proposed development is expected to be compatible with the adjacent commercial use.

At this stage in design the noise levels produced by the development on itself and its surroundings could not be quantitatively assessed. However, the effect on both the building itself and its surroundings is expected to be feasible to meet the applicable criteria. We recommend that the building design is evaluated prior to building permit to ensure that the acoustical design is adequately implemented in order to meet the applicable criteria.

Based on the results of the analysis including implementation of the recommendations included with this assessment, the proposed development is compatible with respect to noise.



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#### 1 INTRODUCTION

RWDI was retained to prepare a Noise Impact Study for the proposed mixed-use development located in Mississauga, Ontario. The proposed development site is on the northeast corner of the intersection of Ponytrail Drive and Fieldgate Drive.

The proposed development will consist of three (3) interconnected buildings (Building A: 22-storeys plus mechanical penthouse, Building B: 18-storeys plus mechanical penthouse, Building C: 13-storeys plus mechanical penthouse); commercial spaces on the first floor; and 2 levels of below grade parking. The context site plan is shown in **Figure 1**.

The site is exposed to noise from road traffic from: Ponytrail Drive to the west; Fieldgate Drive to the south; and Bloor Street to the east.

No stationary sources associated with environmental noise permits (ECA or EASR) were identified. Nearby stationary sources exempt from environmental permitting requirements are assessed.

This assessment was completed to support the Zoning By-Law Amendment (ZBA) submission as required by the City of Mississauga. This assessment was based on design drawings dated August 8, 2024. A copy of the drawings is included in **Appendix A**.

#### 2 APPLICABLE CRITERIA

Applicable criteria for transportation noise sources and stationary noise sources are adopted from the Ontario Ministry of the Environment, Conservation and Parks (MECP) NPC-300 Environmental Noise Guideline (MOE, 2013), with a summary of the applicable criteria included with **Appendix B**.

The proposed development site would be characterized as a "Class 1 Area", which is defined according to NPC-300 as an area with an acoustical environment typical of a major population centre, where the background sound level is dominated by the activities of people, usually road traffic, often referred to as "urban hum."

# 3 THE EFFECTS OF THE ENVIRONMENT ON THE PROPOSED DEVELOPMENT

#### 3.1 Transportation Source Assessment

#### 3.1.1 Road Traffic Volume Data

Turning Movement Counts (TMCs) at the intersection of Ponytrail Drive and Fieldgate Drive provided detailed traffic volumes for the two peak hours: 08:00 to 09:00 and 16:45 to 17:45. The TMCs were used to determine the traffic volume and types of vehicles on each link during the AM and PM peaks which were assumed to be 9% and 10% of the Annual Average Daily Traffic (AADT), respectively. The maximum AADTs obtained from the approximation of each of these two time periods was used for the AADT for Ponytrail Drive and Fieldgate Drive. A 90%/10%



daytime/nighttime split and a 38%/62% medium/heavy truck split was applied. The growth rates were assumed to be 0%, which was provided by the traffic consultant for this proposal.

The ultimate traffic volume of Bloor Street was obtained from the City of Mississauga.

A summary of the traffic data used is included in **Table 1** below with more detailed information included in **Appendix C**.

**Table 1: Road Traffic Volumes** 

Roadway	Segment	2034 Future Traffic (AADT)	% Day/Night	Speed Limit (km/hr)	% Medium Trucks	% Heavy Trucks
Ponytrail Drive	North of Fieldgate Drive	3,111	90% /10%	30	2.3	3.7
Fieldgate Drive	East of Ponytrail Drive	5,878	90% /10%	40	1.4	2.2
Fieldgate Drive	West of Ponytrail Drive	4,967	90% /10%	30	0.9	1.4
Bloor Street	Fieldgate Drive to Etobicoke Creek	20,600	90% /10%	50	1.1	0.9

#### 3.1.2 Representative Receptors

The selection of receptors affected by transportation noise sources was based on the drawings reviewed for this assessment. Using the "building evaluation" feature of Cadna/A, each façade of the residential buildings was assessed.

Outdoor Living Areas (OLAs) would include outdoor areas intended and designed for the quiet enjoyment of the outdoor environment and which are readily accessible from the building. OLAs may include any common outdoor amenity spaces associated with a multi-unit residential development (e.g. courtyards, roof-top terraces), and/or private backyards and terraces with a minimum depth of 4 m provided they are the only outdoor living area for the occupant. Daytime sound levels were assessed at the following identified OLAs:

OLA\_01a: Level 2 west part, outdoor amenity between Building A and Building B,

• OLA\_01b: Level 2 middle part, outdoor amenity between Building A and Building B,

• OLA\_01c: Level 2 east part, outdoor amenity between Building A and Building B,

• OLA\_02a: Level 6 west part, outdoor amenity at Building B, and

• OLA\_02b: Level 6 east part, outdoor amenity at Building B.

The OLAs are indicated in Figure 2.



#### 3.1.3 Analysis and Results

Sound levels due to the adjacent transportation sources were predicted using Cadna/A software package. Roads were modelled as line sources with reference sound power levels calculated with MECP's ORNAMENT algorithm (MOE 1989). The predictions were equivalent to those predicted with STAMSON v5.03 road traffic noise model. STAMSON validation files are included in **Appendix C** and **Figure C-1**.

To assess the effect of transportation noise on suites, the maximum sound level on each façade was determined with the results summarized in **Table 2**.

Table 2: Predicted Ground Transportation Source Sound Levels - Plane of Window

		Ro	ad	
Building	Façade	Day L <sub>EQ</sub> , 16hr	Night L <sub>EQ</sub> , 8hr	Notes
	North	54	47	-
Duilding A	East	58	51	1
Building A	South	59	52	1
	West	54	48	-
	North	48	42	-
Duilding D	East	56	49	1
Building B	South	55	48	-
	West	50	43	-
	North	53	47	-
Puilding C	East	55	48	-
Building C	South	58	51	1
	West	56	50	1

Note(s):

To assess the effect of transportation noise on the qualifying OLAs for the development, predicted sound level results are summarized in **Table 3**.

**Table 3: Transportation Sound Levels in Outdoor Living Areas (OLAs)** 

Receptor	Description	Daytime L <sub>EQ</sub> , 16hr	Notes
OLA_01a	Level 2 west part, outdoor amenity between Building A and Building B	43 dBA	1
OLA_01b	Level 2 middle part, outdoor amenity between Building A and Building B	45 dBA	1
OLA_01c	Level 2 east part, outdoor amenity between Building A and Building B	55 dBA	1
OLA_02a	Level 6 west part, outdoor amenity at Building B	44 dBA	1
OLA_02b	Level 6 east part, outdoor amenity at Building B	48 dBA	1

Note(s):

<sup>1.</sup> Applicable for high density developments: Installation of air-conditioning to allow for windows and doors to remain closed, warning clause "Type D". Refer to **Appendix D** for guidance regarding air-conditioning as a noise mitigation measure.

<sup>1.</sup> The predicted sound level meets the NPC-300 criterion for OLAs. Noise control measures are not required.



#### **3.2 Stationary Source Assessment**

Stationary sources could be grouped into two categories: Those that have a permit with the Ontario Ministry of the Environment, Conservation and Parks (MECP) through an Environmental Compliance Approval (ECA) or Environmental Activity and Sector Registry (EASR); and those that are exempt from ECA or EASR permit requirements.

In the case where a stationary source has an Environmental Compliance Approval (ECA) or Environmental Activity and Sector Registry (EASR) permit with the MECP, and would be put in a position where it is no longer in compliance with the applicable sound level criteria due to the encroachment of the proposed new development, source specific mitigation and/or formal classification of the proposed development lands as a "Class 4 Area" (refer to C.4.4.2 "Class 4 Area" in NPC-300) would be required. In this case, coordination, and agreements between the stationary source owner, proposed new development owner, the land-use planning authority and potentially the MECP would be needed.

In the case where a stationary source is exempt from ECA or EASR permit requirements with the MECP, the noise provisions of the applicable Municipal Code and guidance from NPC-300 would be applicable. In this case, mitigation of sound levels due to stationary sources would be from a due diligence perspective to avoid nuisance complaints from future occupants of the proposed new development. Mitigation could be in the form of mitigation at the source (with agreement from the stationary source owner) and/or mitigation at the receptor through site and building element design (building orientation, acoustical barriers, façade sound insulation design).

#### 3.2.1 Stationary Source Modeling

Stationary sources of noise surrounding the proposed development were identified using publicly available aerial and street-level imagery.

#### 3.2.1.1 Representative Receptors

Stationary source noise modelling was carried out using the Cadna/A software package, a commercially available implementation of the ISO 9613 (ISO, 1994 and ISO, 1996) algorithms. Using the "building evaluation" feature of Cadna/A, each façade of the residential buildings was assessed to evaluate the potential stationary source noise impact.

#### 3.2.1.2 Assumed Sources and Sound Power Levels

RWDI proxy data were used for the sound power levels of the HVAC units, exhausts, boilers and drive-thru speaker and idling cars included in the model. The assumed sound power levels included in the screening level stationary source assessment are presented in **Table 6**. The locations of the sources summarized in **Table 4** included in the stationary source assessment are illustrated in **Appendix E**, **Figure E-1**.



**Table 4: Stationary Source Sound Power Level Assumptions** 

	Brown Data /	Sound Power	Duty Cycle				
Source	Proxy Data / Calculation	Level (dBA)	Daytime and Evening (07:00h - 23:00h)	Nighttime (23:00h - 07:00h)			
Gas Station HVAC_1Fan	Proxy Data	82	Continuous	30 min/hour			
Gas Station HVAC_2Fan	Proxy Data	85	Continuous	30 min/hour			
Drive-thru Speaker	Proxy Data	82 <sup>1</sup>	30 min/hour	30 min/hour			
Idling Car	Proxy Data	78	Continuous	Continuous			
Exhaust	Proxy Data	78	Continuous	Continuous			
Boiler	Proxy Data	65	Continuous	Continuous			

Note(s):

The assumed sound power level values and duty-cycles for the stationary sources are based on reasonable assumptions for the source type. Continuous daytime operation and half-duty nighttime operation represents the worst-case hour for the Shell gas station store. Continuous car idling and half-duty speaker represents the worst-case hour for the Tim Hortons drive-thru. Continuous operation of the exhausts and boilers represent the worst-case hour for the surrounding residential buildings.

#### 3.2.1.3 Analysis and Results

The predicted sound levels are assessed against the Class 1 Area limits (refer to **Appendix A**). The sound level limits were found to be elevated on east, south, and west façades from roadway traffic during daytime and evening. The background roadway sound level was calculated using the line sources with reference sound power levels calculated with MECP's ORNAMENT algorithm (MOE 1989) and the lowest hour of traffic during daytime and evening. The lowest hour of traffic was determined based on the ITE distribution (ITE, 2010).

As the ambient levels and stationary source impact vary, the "building evaluation" arithmetic feature in Cadna/A was used to determine the potential for exceedances of the limits along the entirety of each façade of the building. The predicted sound levels during the worst-case 1-hour from existing stationary sources are presented in **Table 5**.

Table 5: Predicted Sound Levels at Worst-case Receptor Locations - Continuous Stationary Sources

Façade/OLA	Time Period	Sound Level (dBA)	Class 1 Limit (dBA)	Meets Criteria
Building B Level 2-5 East Façade	Day/Evening	52	52	Yes
Building B Level 2-3 East Façade	Night	51	45	No
OPOR_1 [1]	Day/Evening	53	53	Yes
OPOR_2 [1]	Day/Evening	47	50	Yes

Note(s):

<sup>1.</sup> Drive thru speaker sound power levels include a 5 dB penalty for tonality.

<sup>1.</sup> Outdoor areas are not assessed during the nighttime period.



As shown in **Table 5**, the daytime-evening continuous sound levels at the façade and in the outdoor point of reception (OPOR) due to existing stationary sources are predicted to meet the applicable Class 1 sound level criteria based on screening level noise modelling analysis. The nighttime continuous sound levels at the façade are predicted to be 6 dB above the limit due to the short setback distance to the Tim Hortons drive-thru and conservative modelling of 7 idling cars in the lineup.

#### 3.3 Recommendations

Based on the noise assessment results, the following recommendations were determined for the project. Recommendations are provided for both transportation sources and stationary sources.

#### 3.3.1 Transportation Sources

The following recommendations are provided to address transportation sources.

#### 3.3.1.1 Building Façade Components

The noise insulation design is not required to be specified. Building envelope assemblies meeting the minimum Ontario Building Code (OBC) requirements will exhibit sufficient noise reduction to meet the interior sound level criteria.

#### 3.3.1.2 Ventilation Recommendations

Due to the transportation sound levels at the plane of the façade, central air conditioning is recommended for the proposed development to allow for windows and doors to remain closed as a noise mitigation measure. Further, prospective purchasers or tenants should be informed by a warning clause "Type D".

#### 3.3.1.3 Outdoor Living Areas

The predicted sound level meets the NPC-300 criterion for OLAs. Noise control measures are not required.

#### 3.3.2 Stationary Sources

Sound from the adjacent Tim Hortons drive-thru and Shell gas station are predicted to exceed the Class 1 limits at the façade of the proposed development during nighttime. To address the exceedance of 6 dB, central air conditioning will be provided, allowing occupants to close windows to achieve an acceptable indoor sound environment. A warning clause will be included to inform occupants of the mitigation measures included.

#### 3.3.3 Warning Clauses

The following warning clauses are recommended for the proposed development:

1. NPC-300 Type D to address transportation sound levels at the plane of window



2. NPC-300 Type E to address proximity to commercial facilities, adjusted to account for inclusion of airconditioning

Warning clauses are recommended to be included on all development agreements, offers of purchase and agreements of purchase and sale or lease. The wording of the recommended warning clauses is included with **Appendix F**.

# 4 THE EFFECTS OF THE PROPOSED DEVELOPMENT ON ITS SURROUNDINGS AND ON ITSELF

On-site stationary sources for the development are expected to consist of HVAC related equipment in the roof-top mechanical penthouse as well as various exhaust fans. Further, consideration should be given to control airborne and structure-borne noise generated within the proposed development.

Within the development itself the main sources of noise that are likely to affect the uses of the building are the mechanical systems. The potential noise effect of the commercial component of the development is recommended to be reviewed during detailed design, to ensure the applicable criteria will be met.

Provided that best practices for the acoustical design of the building are followed, noise from building services equipment associated with the development are expected to be feasible to meet the applicable sound level criteria due to the nature (residential/mixed-use) of the proposed development.

We recommend that the potential noise effect of the proposed development is reviewed during detailed design to ensure the applicable sound level criteria will be achieved.

#### 5 CONCLUSIONS

RWDI was retained to prepare a Noise Impact Study for the proposed mixed-use development located in Mississauga, Ontario.

The following noise control measures are recommended for the proposed development:

- 1. Installation of central air-conditioning so that all suites' windows can remain closed.
- 2. The inclusion of noise warning clauses related to:
  - a. Proximity to commercial land-use

The potential noise levels from stationary sources of sound were evaluated. The most significant sources of sound are idling cars and rooftop HVAC equipment associated with the adjacent Tim Hortons drive-thru and Shell gas station. With the implementation of mitigation measures, including central air-conditioning and a warning clause, the proposed development is expected to be compatible with the adjacent commercial use.

### NOISE IMPACT STUDY 3403-3445 FIELDGATE DRIVE

RWDI#2406207 August 9, 2024



At this stage in design the noise levels produced by the development on itself and its surroundings could not be quantitatively assessed. However, the effect on both the building itself and its surroundings is expected to be feasible to meet the applicable criteria. We recommend that the building design is evaluated prior to building permit to ensure that the acoustical design is adequately implemented in order to meet the applicable criteria.

Based on the results of the analysis including implementation of the recommendations included with this assessment, the proposed development is compatible with respect to noise.

#### 6 REFERENCES

- 1. Ontario Ministry of the Environment (MOE), August 2013, Publication NPC-300, Environmental Noise Guideline Stationary and Transportation Sources Approval and Planning (MOE, 2013).
- 2. Ontario Ministry of the Environment (MOE), 1989, ORNAMENT Ontario Road Noise Analysis Method for Environment and Transportation, Technical Publication (MOE, 1989)
- 3. Ontario Ministry of the Environment (MOE) Publication Guideline D-6, "Compatibility Between Industrial Facilities and Sensitive Land Uses", July 1995 (MOE, 1995).
- 4. Institute of Transportation Engineers (ITE), 2010, Traffic Engineering Handbook, 6th Edition (ITE, 2010)
- 5. International Organization for Standardization (ISO), 1994b, International Standard ISO 9613-1:1994, Acoustics Attenuation of Sound during propagation outdoors. Part 1: Calculation of the absorption of sound by the atmosphere. (ISO, 1994)
- 6. International Organization for Standardization (ISO), 1996, International Standard ISO 9613-2:1996, Acoustics Attenuation of sound during propagation outdoors Part 2: General method of calculation (ISO, 1996)



#### STATEMENT OF LIMITATIONS

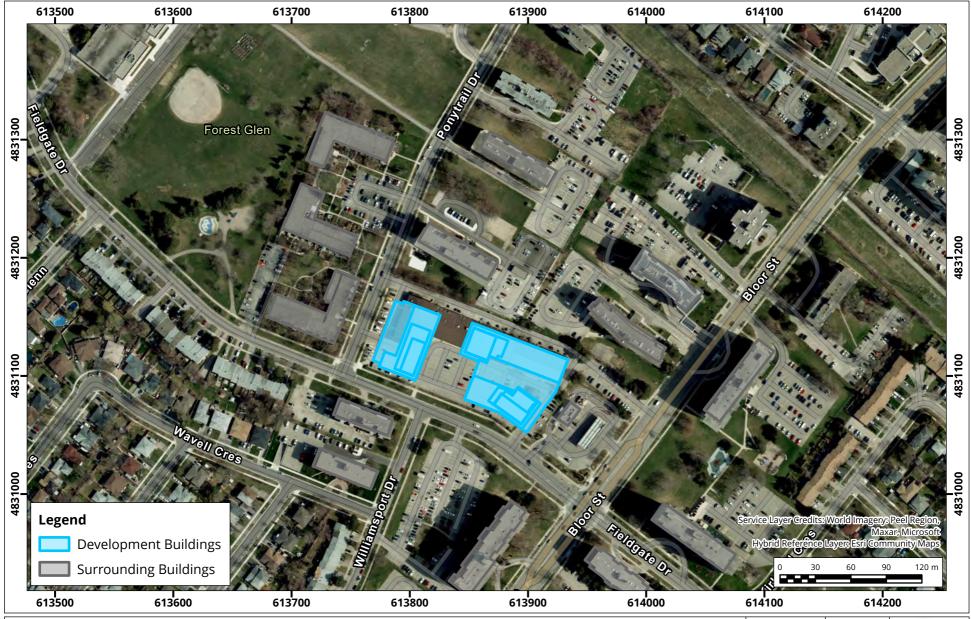
This report entitled 3403-3445 Fieldgate Drive was prepared by Rowan Williams Davies & Irwin Inc. ("RWDI") for Forest Glenn Shopping Center ("Client"). The findings and conclusions presented in this report have been prepared for the Client and are specific to the project described herein ("Project"). The conclusions and recommendations contained in this report are based on the information available to RWDI when this report was prepared. Because the contents of this report may not reflect the final design of the Project or subsequent changes made after the date of this report, RWDI recommends that it be retained by Client during the final stages of the project to verify that the results and recommendations provided in this report have been correctly interpreted in the final design of the Project.

The conclusions and recommendations contained in this report have also been made for the specific purpose(s) set out herein. Should the Client or any other third party utilize the report and/or implement the conclusions and recommendations contained therein for any other purpose or project without the involvement of RWDI, the Client or such third party assumes any and all risk of any and all consequences arising from such use and RWDI accepts no responsibility for any liability, loss, or damage of any kind suffered by Client or any other third party arising therefrom.

Finally, it is imperative that the Client and/or any party relying on the conclusions and recommendations in this report carefully review the stated assumptions contained herein and to understand the different factors which may impact the conclusions and recommendations provided.



# **FIGURES**



Site Context Plan

Map Projection: NAD 1983 UTM Zone 17N 3403-3445 Fieldgate Drive - Mississauga, ON

Project #: 2406207

True North Drawn by: JM Figure:

Approx. Scale: 1:3,200

Date Revised: Jun 27, 2024





**Outdoor Living Areas (OLAs) Location of Common Outdoor Amenity Areas** 

Map Projection: NAD 1983 UTM Zone 17N

3403-3445 Fieldgate Drive - Mississauga, ON

True North | Drawn by: JM | Figure:

Approx. Scale: 1:800

Date Revised: Jun 27, 2024 Project #: 2406207





# **APPENDIX A**



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# 3403-3445 FIELDGATE DRIVE DEVELOPMENT

3403-3445 FIELDGATE DRIVE, MISSISSAUGA, ONTARIO

 002
 07 FEB 2024
 RE-ISSUED FOR DARC MEETING

 001
 25 JAN 2024
 ISSUED FOR DARC MEETING

 No.
 DATE
 ISSUED

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ALL CONSTRUCTION TO BE ACCORDING TO BEST COMMON PRACTICE AND CONFORM TO THE ONTARIO BUILDING CODE IN FEFECT AT THE TIME

Forest Glen Shopping Ce Ltd. 3427 Fieldgate Dr.,

ADDRESS

3403-3445 Fieldgate Drive

DRAWING

Cover Sheet

PROJECT NUMBER

23063

07 FEB 2024

SCALE

PLOTTED DATE

08/08/2024 12:22:02

PM

# 3403-3445 Fieldgate Drive, Development 3403-3445 Fieldgate Drive, Mississauga, Ontario

Apartment Tower A

3 Residential

Maximum Building Height

(From Established Grade of 134.85)

Commercial Loading Space

\* Loading Space Size: 3.5m x 9m

Site Description Registered Plan 719 City of Mississauga Municipality of Peel

Subject To Zoning By-Law 0225-2007 Ground Floor FFE 134.85

ESTABLISHED GRADE: means, with reference to a building, structure or part thereof, the average elevation of the finished grade of the ground immediately surrounding such building or structure, and when used with reference to a street, means the elevation of the street, established by the 134.85 Full Property Without Parkland Dedication
15,836 m<sup>2</sup> 14,559 m<sup>2</sup> Municipality or other designated authority. (0308-2011)

Units

<u> </u>																		
Floor No.	Storey	Total Units	STUDIO	1B	1B + D	2B	2B+D	3B	Total GCA	Total GCA	Garage Area	Amenity Area	Residential Area	Commercial Area	BOH Area	Deductions are per by-law	zoning GFA	zoning GFA
	·								(m²)	(sq.ft.)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(sq.ft.)
1	Parking Level 2	0							12,765.95 m <sup>2</sup>	137,412.69 sq.ft.		0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	12,765.95 m <sup>2</sup>		0.00 sq.f
2	Parking Level 1	0							12,765.95 m²	137,412.69 sq.ft.	12,765.95 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	12,765.95 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 sq.f
	Totals	0	0	0	0	0	0	0	25,531.90 m²	274,825.37 sq.ft.	25,531.90 m²	0.00 m	2 0.00 m <sup>2</sup>	0.00 m²	0.00 m²	25,531.90 m²	0.00 m²	0.00 sq.f
Ара	artment Tower A				Units						Gross Co	onstruction Ar	ea				Gross Floor Ar	rea
Floor No.	Storey	Total Units	STUDIO	1B	1B + D	2B	2B+D	3B	Total GCA	Total GCA	Garage Area	Amenity Area	Residential Area	Commercial Area	BOH Area	Deductions are per by-law	zoning GFA	zoning GFA
									(m²)	(sq.ft.)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(sq.ft.)
1	Lobby/Amenity/Residential	0	0	0	0	0	0	0	4,588.70 m <sup>2</sup>	49,392.75 sq.ft.	136.46 m²	342.98 m <sup>2</sup>	<sup>2</sup> 1,311.31 m <sup>2</sup>	1,820.00 m <sup>2</sup>	977.95 m <sup>2</sup>	1,218.71 m <sup>2</sup>	3,369.99 m²	36,274.57 sq.ff
2	Residential	13	0	2	1	5	0	5	1,324.45 m²	14,256.35 sq.ft.	0.00 m <sup>2</sup>	199.74 m <sup>2</sup>	1,124.71 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	49.93 m²	1,274.52 m <sup>2</sup>	13,718.90 sq.f
3	Residential	16	0	3	4	3	0	6	1,304.62 m <sup>2</sup>	14,042.93 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	1,304.62 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	49.93 m²	1,254.69 m <sup>2</sup>	13,505.48 sq.f
4	Residential	16	0	3	4	3	0	6	1,304.62 m <sup>2</sup>	14,042.93 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	1,304.62 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	49.93 m²	1,254.69 m <sup>2</sup>	13,505.48 sq.f
5	Residential	17	0	4	6	3	2	2	1,263.05 m <sup>2</sup>	13,595.47 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	1,263.05 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	49.93 m <sup>2</sup>	1,213.12 m <sup>2</sup>	13,058.02 sq.fr
6	Residential	11	0	7	2	2	0	0	714.42 m <sup>2</sup>	7,690.02 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	<sup>2</sup> 714.42 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m <sup>2</sup>	678.29 m <sup>2</sup>	7,301.11 sq.f
7	Residential	11	0	7	2	2	0	0	714.42 m <sup>2</sup>	7,690.02 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	714.42 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m <sup>2</sup>	678.29 m <sup>2</sup>	7,301.11 sq.ft
8	Residential	8	0	4	1	2	1	0	583.77 m <sup>2</sup>	6,283.70 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	583.77 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m²	547.64 m <sup>2</sup>	5,894.80 sq.f
9	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m <sup>2</sup>	537.39 m <sup>2</sup>	5,784.47 sq.f
10	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m²	537.39 m <sup>2</sup>	5,784.47 sq.f
11	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	<sup>2</sup> 573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m <sup>2</sup>	537.39 m <sup>2</sup>	5,784.47 sq.f
12	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	<sup>2</sup> 573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m²	537.39 m²	5,784.47 sq.f
13	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m <sup>2</sup>	537.39 m <sup>2</sup>	5,784.47 sq.f
14	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m <sup>2</sup>	537.39 m <sup>2</sup>	5,784.47 sq.f
15	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m <sup>2</sup>	537.39 m <sup>2</sup>	5,784.47 sq.f
16	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m²	537.39 m <sup>2</sup>	5,784.47 sq.f
17	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m <sup>2</sup>	537.39 m <sup>2</sup>	5,784.47 sq.f
18	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m²	537.39 m <sup>2</sup>	5,784.47 sq.f
19	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m <sup>2</sup>	537.39 m <sup>2</sup>	5,784.47 sq.f
20	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m²	537.39 m <sup>2</sup>	5,784.47 sq.f
21	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	<sup>2</sup> 573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m <sup>2</sup>	537.39 m <sup>2</sup>	5,784.47 sq.f
22	Residential	8	1	3	0	3	1	0	573.52 m <sup>2</sup>	6,173.37 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	573.52 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.13 m²	537.39 m <sup>2</sup>	5,784.47 sq.f
	Mechanical	0	0	0	0	0	0	0	335.86 m²	3,615.20 sq.ft.	0.00 m²	0.00 m <sup>2</sup>	<sup>2</sup> 335.86 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	335.86 m²		0.00 sq.ff
	Totals	204	14	72	20	62	17	19	20,163.19 m²	217,036.52 sq.ft.	136.46 m²	542.72 m	<sup>2</sup> 16,686.06 m <sup>2</sup>	1,820.00 m²	977.95 m²	2,368.50 m²	17,794.69 m²	191,542.01 sq.f
Ара	artment Tower B				Units						Gross Co	onstruction Ar	ea				Gross Floor Ar	rea
Floor No.	Storey	Total Units	STUDIO	1B	1B + D	2B	2B+D	3B	Total GCA	Total GCA	Garage Area		Residential Area	Commercial Area	BOH Area	Deductions are per by-law	zoning GFA	zoning GFA
									(m²)	(sq.ft.)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(sq.ft.)
1	Lobby/Amenity/Residential	0	0	0	0	0	0	0	0.00 m <sup>2</sup>	0.00 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>		0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>		0.00 sq.f
2	Residential	11	0	1	2	7	1	0	1,947.43 m²	20,962.14 sq.ft.	0.00 m <sup>2</sup>		,	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	49.62 m²	1,897.81 m <sup>2</sup>	20,428.03 sq.f
0	D	0.4	•	•	-	40	•	_	4 == 0 00 0	10 000 10 6	0.00 2	0.00	4 750 00 0	0.00 2	0.00	10.00	4 200 00 0	40.004.00

**Gross Construction Area** 

	Minim	um Apartment	Setbacks I	Provided
	Front Yard (m)	Side Yard (m)	Rear Yard (m)	Side Yard (n
	(South)	(West)	(North)	(East)
ft.	4.01 m	N/A	N/A	7.50 m
ft. ft.	R	equired Apartr	ment Setba	cks
ft.	Front Yard	Side Yard	Rear Yard	Side Yard
ft.	(m)	(m)	(m)	(m)
ft.	(South)	(West)	(North)	(East)
ft.	4.00 m	7.50 m	16.50 m	4.50 m
ft.				
ft.				
ft.				
t.				
ft. ft.				

Minimum Apartment Setbacks Provided

(South) (West) (North) (East) N/A N/A 16.50 m 10.43 m

Required Apartment Setbacks

49.62 m<sup>2</sup> 1,703.30 m<sup>2</sup> 18,334.32 sq.ft. Front Yard Side Yard Rear Yard Side Yard

**Gross Floor Area** 

4	Residential	24	0	2	7 12 9 10		0	1,752.92 m²	18,868.43 sq.ft.	0.00 m²	0.00 m <sup>2</sup>	1,752.92 m²	0.00 m <sup>2</sup>	0.00 m²	49.62 m²	1,703.30 m²	18,334.32 sq.ft.	Front Yard	Side Yard	Rear Yard	Side Yard
5	Residential Residential	24	1	5	2 2		0	1,691.10 m <sup>2</sup> 620.99 m <sup>2</sup>	18,203.00 sq.ft. 6,684.34 sq.ft.	0.00 m <sup>2</sup> 0.00 m <sup>2</sup>	0.00 m <sup>2</sup> 29.57 m <sup>2</sup>	1,691.10 m <sup>2</sup> 591.42 m <sup>2</sup>	0.00 m <sup>2</sup> 0.00 m <sup>2</sup>	0.00 m <sup>2</sup> 0.00 m <sup>2</sup>	49.62 m <sup>2</sup> 49.62 m <sup>2</sup>	1,641.48 m <sup>2</sup> 571.37 m <sup>2</sup>	17,668.89 sq.ft.	(m)	(m) (\Most)	(m)	(m)
7	Residential	10	1	6	1 2	0	0	620.99 m²	6,684.34 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m²	0.00 m <sup>2</sup>	36.29 m <sup>2</sup>	584.70 m <sup>2</sup>	6,150.23 sq.ft. 6,293.71 sq.ft.	(South) 4.00 m	(West) 7.50 m	(North) 16.50 m	(East) 4.50 m
,	Residential	10	1	6	1 2	0	0	620.99 m²	6,684.34 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m²	0.00 m <sup>2</sup>	36.29 m²	584.70 m²	6,293.71 sq.ft.	4.00 111	7.50 111	10.50 111	4.50 111
0		10	1	6	1 2	0	0	620.99 m²		0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m <sup>2</sup>			584.70 m²		1			
10	Residential Residential	10	1	6	1 2	_	0	620.99 m²	6,684.34 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m²	0.00 m <sup>2</sup> 0.00 m <sup>2</sup>	36.29 m <sup>2</sup> 36.29 m <sup>2</sup>	584.70 m²	6,293.71 sq.ft. 6,293.71 sq.ft.	l .			
11	Residential	10	1	6	1 2	0	0	620.99 m²	6,684.34 sq.ft. 6,684.34 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m²	0.00 m <sup>2</sup>	36.29 m <sup>2</sup>	584.70 m²	6,293.71 sq.ft.	l .			
12	Residential	10	1	6	1 2	0	0	620.99 m²	6,684.34 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m²	0.00 m <sup>2</sup>	36.29 m²	584.70 m²	6,293.71 sq.ft.	l .			
13	Residential	10	1	6	1 2	0	0	620.99 m²	6,684.34 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m²	0.00 m <sup>2</sup>	36.29 m²	584.70 m²	6,293.71 sq.ft.	l .			
14	Residential	10	1	6	1 2	0	0	620.99 m <sup>2</sup>	6,684.34 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.29 m <sup>2</sup>	584.70 m <sup>2</sup>	6,293.71 sq.ft.	l .			
15	Residential	10	1	6	1 2	0	0	620.99 m²	6,684.34 sq.ft.	0.00 m²	0.00 m <sup>2</sup>	620.99 m²	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.29 m²	584.70 m²	6,293.71 sq.ft.	l .			
16	Residential	10	1	6	1 2	0	0	620.99 m²	6,684.34 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	36.29 m²	584.70 m²	6,293.71 sq.ft.	l .			
17	Residential	10	1	6	1 2	0	0	620.99 m²	6,684.34 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m²	0.00 m <sup>2</sup>	36.29 m <sup>2</sup>	584.70 m <sup>2</sup>	6,293.71 sq.ft.	l .			
16	Residential	10	1	6	1 2	0	0	620.99 m²	6,684.34 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	620.99 m²	0.00 m²	0.00 m <sup>2</sup>	36.29 m <sup>2</sup>	584.70 m²	6,293.71 sq.ft.	l .			
10	Mechanical	0	0	0	0 0		0	466.33 m²	5,019.58 sq.ft.	0.00 m²	0.00 m <sup>2</sup>	466.33 m²	0.00 m²	0.00 m <sup>2</sup>	466.33 m²	0.00 m <sup>2</sup>	0.00 sq.ft.	l .			
	modifical					•			0,010.00 04.11.	0.00 111	0.00 111	400.00 III	0.00 111	0.00 111	-100.00 III	0.00 111	0.00 04.11.	I			
	Totals	212	13	86	39 6	7 7	0	15,683.57 m²	168,817.95 sq.ft.	0.00 m²	899.43 m²	14,784.14 m²	0.00 m²	0.00 m <sup>2</sup>	1,149.91 m²	14,533.66 m²	156,440.32 sq.ft.				
Apa	rtment Tower C			Ur	nits					Gross Co	nstruction Are	a				Gross Floor A	ea	Minimu	ım Apartmer	nt Setbacks I	Provided
Floor No.	Storey	Total Units	STUDIO	1B	1B + D 28	3 2B+D	3B	Total GCA	Total GCA	Garage Area	Amenity Area	Residential Area	Commercial Area	BOH Area	Deductions are per by-law	zoning GFA	zoning GFA	Front Yard (m)	Side Yard (m)	Rear Yard (m)	Side Yard (m)
	,							(m²)	(sq.ft.)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(sq.ft.)	(South)	(West)	(North)	(East)
1	Lobby/Amenity/Residential	0	0	0	0 0	0	0	1,713.96 m <sup>2</sup>	18,449.07 sq.ft.	172.16 m <sup>2</sup>	0.00 m <sup>2</sup>	417.21 m <sup>2</sup>	1,083.22 m <sup>2</sup>	41.37 m <sup>2</sup>	291.24 m²	1,422.72 m <sup>2</sup>	15,314.16 sq.ft.	4.01 m	7.50 m	20.00 m	N/A
2	Residential	18	0	2	2 1	1 0	3	1,474.04 m²	15,866.57 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	1,474.04 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	44.07 m²	1,429.97 m <sup>2</sup>	15,392.20 sq.ft.				-t
3	Residential	19	1	2	2 13	3 0	1	1,417.87 m <sup>2</sup>	15,261.95 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	1,417.87 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	44.07 m <sup>2</sup>	1,373.80 m <sup>2</sup>	14,787.58 sq.ft.	,   Re	equired Apar	tment Setba	CKS
4	Residential	19	1	2	2 13	3 0	1	1,417.87 m <sup>2</sup>	15,261.95 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	1,417.87 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	44.07 m²	1,373.80 m <sup>2</sup>	14,787.58 sq.ft.	Front Yard	Side Yard	Rear Yard	Side Yard
5	Residential	19	1	6	4 8	0	0	1,286.85 m <sup>2</sup>	13,851.65 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	1,286.85 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	44.07 m <sup>2</sup>	1,242.78 m <sup>2</sup>	13,377.28 sq.ft.	(m)	(m)	(m)	(m)
6	Residential	11	0	9	1 1	0	0	704.85 m <sup>2</sup>	7,587.01 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	704.85 m²	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	45.16 m²	659.69 m²	7,100.90 sq.ft.	(South)	(West)	(North)	(East)
7	Residential	11	0	9	1 1	0	0	704.85 m <sup>2</sup>	7,587.01 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	704.85 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	45.16 m <sup>2</sup>	659.69 m²	7,100.90 sq.ft.	4.00 m	7.50 m	20.00 m	4.50 m
8	Residential	11	0	9	1 1	0	0	704.85 m²	7,587.01 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	704.85 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	45.16 m²	659.69 m²	7,100.90 sq.ft.	ı			
9	Residential	11	0	9	1 1	0	0	704.85 m <sup>2</sup>	7,587.01 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	704.85 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	45.16 m <sup>2</sup>	659.69 m <sup>2</sup>	7,100.90 sq.ft.	l .			
10	Residential	11	0	9	1 1	0	0	704.85 m <sup>2</sup>	7,587.01 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	704.85 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	45.16 m <sup>2</sup>	659.69 m <sup>2</sup>	7,100.90 sq.ft.	l .			
11	Residential	11	0	9	1 1	0	0	704.85 m <sup>2</sup>	7,587.01 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	704.85 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	45.16 m <sup>2</sup>	659.69 m²	7,100.90 sq.ft.	l .			
12	Residential	11	0	9	1 1	0	0	704.85 m <sup>2</sup>	7,587.01 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	704.85 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	45.16 m <sup>2</sup>	659.69 m <sup>2</sup>	7,100.90 sq.ft.	l .			
13	Residential	11	0	9	1 1	0	0	704.85 m <sup>2</sup>	7,587.01 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	704.85 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	45.16 m <sup>2</sup>	659.69 m <sup>2</sup>	7,100.90 sq.ft.	I			
	Mechanical	0	0	0	0 0	0	0	375.19 m²	4,038.55 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	375.19 m²	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	375.19 m²	0.00 m²	0.00 sq.ft.				
	Totals	163	3	84	18 5	3 0	5	13,324.58 m²	143,425.78 sq.ft.	172.16 m²	0.00 m²	12,027.83 m²	1,083.22 m²	41.37 m²	1,203.99 m²	12,120.59 m²	130,466.03 sq.ft.				
Tov	wnhouse Units			Ur	nits					Gross Co	nstruction Are	a				Gross Floor A	ea	Minimu	m Townhous	se Setbacks	Provided
																		l			
Floor No.	Storey	Total Units	STUDIO	1B	1B + D 28	3 2B+D	3B	Total GCA	Total GCA	Garage Area	Amenity Area	Residential Area	Commercial Area	BOH Area	Deductions are per by-law	zoning GFA	zoning GFA	Front Yard (m)	Side Yard (m)	Rear Yard (m)	Side Yard (m)
								(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(m²)	(sq.ft.)	(South)	(West)	(North)	(East)
1	Townhouses	13	0	0	0 0	5	8	598.21 m²	6,439.13 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	598.21 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	598.21 m <sup>2</sup>	6,439.13 sq.ft.	N/A	7.50 m	20.00 m	N/A
2	Townhouses	0	0	0	0 0	0	0	605.26 m <sup>2</sup>	6,515.02 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	605.26 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	605.26 m <sup>2</sup>	6,515.02 sq.ft.	D <sub>0</sub>	quired Town	house Setha	cks
3	Townhouses	0	0	0	0 0	0	0	368.57 m <sup>2</sup>	3,967.29 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	368.57 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	368.57 m <sup>2</sup>	3,967.29 sq.ft.		quired 10WII	e Getba	
	Roof	0	0	0	0 0	0	0	76.86 m²	827.32 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	76.86 m²	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	76.86 m²	827.32 sq.ft.	Front Yard	Side Yard	Rear Yard	Side Yard
		0																(South)	(West)	(North)	(East)
	Totals	13	0	0	0 0	5	8	1,648.90 m²	17,748.76 sq.ft.	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	1,648.90 m²	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	0.00 m <sup>2</sup>	1,648.90 m²	17,748.76 sq.ft.	4.00 m	7.50 m	20.00 m	4.50 m
	Total Site			Ur	nits					Gross Co	nstruction Are	a				Gross Floor A	ea				

592 30 242 77 182 29 32 76,352.14 m<sup>2</sup> 821,854.38 sq.ft. 25,840.52 m<sup>2</sup> 1,442.15 m<sup>2</sup> 45,146.93 m<sup>2</sup> 2,903.22 m<sup>2</sup> 1,019.32 m<sup>2</sup> 30,254.30 m<sup>2</sup> 46,097.84 m<sup>2</sup> 496,197.12 sq.ft.

coverage shall be deemed to apply to only that portion of context grade and:

ridge of a sloped roof.

maximum height of the top of such components is no higher than 6.0 m above the height limit otherwise applicable. (0158-2023)

**24** 0 2 7 12 3 0 **1,752.92 m² 18,868.43 sq.ft.** 0.00 m² 0.00 m² 1,752.92 m² 0.00 m² 0.00 m² 49.62 m² **1,703.30 m² 18,334.32 sq.ft.** 

		Minimum Apartment Setbacks Provided									
		Front Yard (m)	Side Yard (m)	Rear Yard (m)	Side Yard (m)						
		(South)	(West)	(North)	(East)						
q.ft.		4.01 m	7.50 m	20.00 m	N/A						
ı.ft. ı.ft.		R	equired Apartı	ment Setba	cks						
ı.ft.		Front Yard	Side Yard	Rear Yard	Side Yard						
q.ft.		(m)	(m)	(m)	(m)						
ą.ft.		(South)	(West)	(North)	(East)						
ą.ft.		4.00 m	7.50 m	20.00 m	4.50 m						
ı.ft.											

Minimu	ım Townhous	e Setbacks	Provided
Front Yard (m)	Side Yard (m)	Rear Yard (m)	Side Yard (m)
(South)	(West)	(North)	(East)
N/A 7.50 m		20.00 m	N/A
Re	equired Townh	ouse Setba	icks
Front Yard	Side Yard	Rear Yard	Side Yard
(South)	(West)	(North)	(East)
4.00 m	7.50 m	20.00 m	4.50 m

		N/A	N/A		0.04	
F.S.I (Total GFA / S	Site Area)	Maximum as Per Zoning By- (RA4)	law 0225-2007		Provided	
		N/A			2.91	
Maximum	Lot Coverage	Maximum as Per Zoning By- (RA4)	law 0225-2007		Provided	COVERAGE: Lot Coverage means the percentage of the lot area covered by all buildings, structures or parts thereof, at or above average grade or established grade,
Maximum	Lot Goverage	N/A			44%	exclusive of overhanging eaves of 0.45 m or less, and
					٦	outdoor swimming pools, but inclusive of pergolas and decks greater than 10 m2 and higher than 0.61 m above
Parking	0 0 5 0					established grade. For lots having two or more zones, lot coverage shall be deemed to apply to only that portion of
Parking Stall Size Barrier Free Type	e: 2.6m x 5.2m e A Stall Size: 3.4m x 5.2m	Barrier Free Type B Stall Size: 2.4	4m x 5.2m			the lot that is located within each specified zone. (0325-
Minimum Drive	Aisle	Required as Per Zoning By-law 0225-200	7	Provided		2008), (0208-2022)
		7.00 m		7.00 m	$\dashv$	
Parkir	ng Requirements	Required as Per Zoning By-la	w 0225-	Provided	-	
	Apartments	637			1	
Resident	1.1 Spaces/ Unit Townhouses	6	63	452		
	2.0 Spaces/ Unit	26			4	
Visitor	Apartments 0.2 Spaces/ Unit	116				
Visitoi	Townohouses 0.25 Spaces/ Unit	4 1	46	146	*11 Lay-by Visitors	s Parking located on Fieldgate Drive
Commercial	5 Spaces/ 100 m² of	146				
Total	Commercial GFA	809		598	-	
Includes Barrier	r Free Parking Spaces	009				
Barrier Fr	ee Parking	Required as Per Zoning By-law 0225-200	07	Provided	7	
Barrier Free Re	esident Parking	8		8	* BF Parking Loca	ited on P1 Level
Barrier Free Vis		5				
	total required visitor parking) mmercial Parking	6		6	* BF Visitor/Comm	nercial Parking Located on P1 Level
(1 space +3% of a	total required commercial parking)	19		14	-	
Total Barrier I	Tee Farking	10		14	_	
Bike Lock	cers					
length clearance	,	Required as Per Zoning By-law 0225-200	07	Provided	+ Desidential Land	Town Pinush Badian Landad at D4 Land
Residential Lon 0.6 stalls / unit		356		356		-Term Bicycle Parking Located at P1 Level
Residential Sho 0.05 stalls / unit		30		32	* Residential Shor	t-Term Bicycle Parking Located at Grade
Commercial Lor 0.15 stalls / 100		5		5	* Commercial Lon	g-Term Bicycle Parking Located P1 Level
Commercial Sho	ort-Term	6		8		ort-Term Bicycle Parking Located at Grade
Total	"	397		401		At 15th Disyster arking Essential at Grade
		Required as Per	·		_ ¬	
Lockers		Zoning By-law 0225-200	07	Provided	4	
Resident Total		0 <b>0</b>		57 <b>57</b>	$\dashv$	
					_	
Amenity A		Required as Per Zoning By-law 0225-2007	(m²) P	Provided (m²)		
Indoor & Outdoo 5.6m² per unit c	or combined for indoor and outdoor	0.045.00 2		1,442.15 m²	* Provided at Leve	el 1, 2 & 6
amenities		3,315.20 m²		1,926.99 m²	* Provided on Lev	el 2 & 6 Rooftop Terrace
Total		3,315.20 m²	3	3,369.14 m²		
	n² of amenity to be provided outside at gra				_ ¬	
Garbage F	Requirements	Required as Per Zoning By-law 0225-200	07	Provided		
Residential Lo	•	1		1		
Loading Space	e Size: 6m x 18m and 7.5m Overhea	d Clearance				

Required as Per

Maximum as Per Zoning By-(Total # law 0225-2007 (RA4) Per Zoning By-law 0225-2007

Maximum as Per Zoning By- Per Zoning By- Provided (Apartment Provided

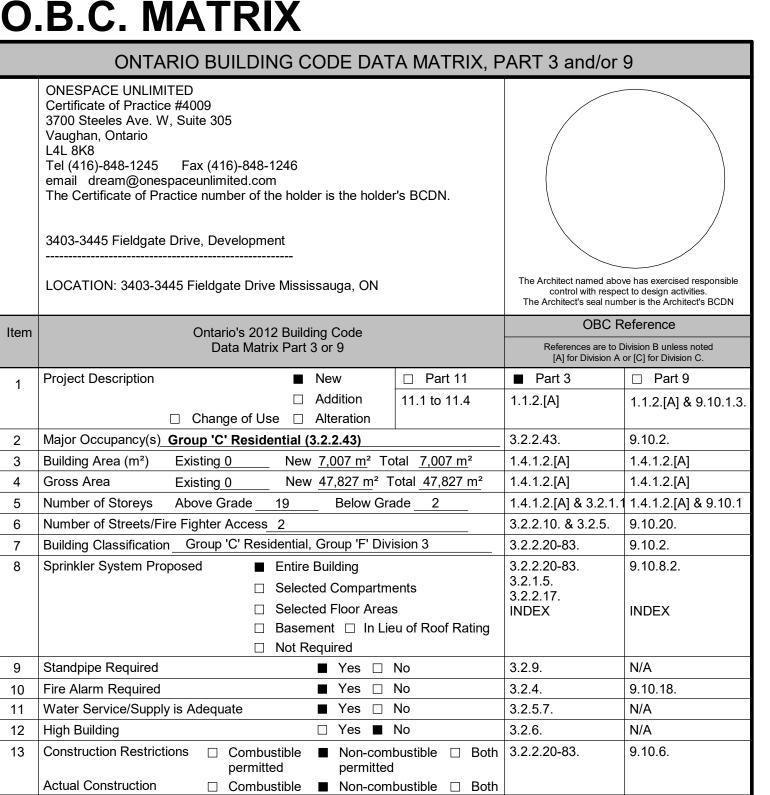
law 0225-2007 (RA4) law 0225-2007 A) (Apartment B) (Apartment C) (Townhouses) (Townhouses) Height and Height - Highest Ridge Gross Floor Area (GFA) - Non-Residential means the sum of the areas of each storey above or 71.40m 58.90m 43.4 m 9.0 m 6.0 m building, structure or part thereof, except a detached dwelling, semi-detached, duplex, triplex, below established grade, measured from the exterior of outside walls, or from the midpoint of common walls, including the area of any floor system or assembly fourplex, townhouse, back to back townhouse or located within a storey which is designed or used for stacked townhouse, the vertical distance between access and passage by persons and including all parts the established grade and: of the building or structure or part thereof below (1.1) the highest point of the roof surface of a flat established grade used for retail, office, industrial or warehouse uses, but excluding the following: (1.2) the mean height level between the eaves and (1) any part of the building, structure or part thereof ridge of a sloped roof. used for mechanical floor area; (1.3) the mean height level between the eaves and (2) areas of stainwells, washrooms or elevators; highest point of the flat roof where there is a flat (3) any enclosed area used for the collection or storage roof on top of a sloped roof; or of disposable or recyclable waste generated within the (1.4) the highest point of a structure without a roof. building or structure or part thereof; COVERAGE: Lot Coverage means the percentage of the (0325-2008), (0174-2017), (0181-2018/LPAT (4) any part of the building or structure or part thereof lot area covered by all buildings, structures or parts Order 2019 February 15), (0196-2023) above or below established grade used for motor vehicle thereof, at or above average grade or established grade, See Illustration No. 6 - Section 1.3 - Illustrations parking, bicycle parking, or the provision of loading spaces; (0118-2022) outdoor swimming pools, but inclusive of pergolas and (3) means, with reference to the height of a (5) any part of the building, structure or part thereof decks greater than 10 m2 and higher than 0.61 m above townhouse, back to back townhouse and stacked below established grade used for storage incidental to established grade. For lots having two or more zones, lot townhouse, the vertical distance between the other uses in the building, structure or part thereof or provided and reserved for the personal needs of the the lot that is located within each specified zone. (0325- (3.1) the highest point of the roof surface of a flat occupants of the building, structure or part thereof roof, including a parapet; or including lunch rooms, lounges or fitness rooms; (0118-(3.2) the mean height level between (6) accessory outdoor tank. (0379-2009) (3.3) the mean height level between the eaves and highest point of the flat roof where there is a flat Gross Floor Area (GFA) - Residential roof on top of a sloped roof; or means the sum of the areas of each storey of a building (3.4) the highest point of a structure without a roof. measured from the exterior of outside walls but shall not (0181-2018/LPAT Order 2019 February 15), include any part of the building used for motor vehicle parking or bicycle parking. (0118-2022) See Illustration No. 6 - Section 1.3 - Illustrations Notwithstanding any other provisions of this By-law, the calculation of height for a building, structure or part thereof, except in a Residential Zone, shall be exclusive of mechanical or architectural components such as mechanical equipment, mechanical penthouses, elevator machine rooms, stairwell enclosures, telecommunication equipment, parapets, turrets, cupolas, stairs and stair enclosures located on the roof, provided that the

A-02 I	Existing Survey
A-030	Zoning Envelope & Setback Plan
A-031	Future Development Plan
-035	Renderings
036	Perspectives
A-037	Perspectives
A-038	Perspectives
A-040	Site Plan
A-042	Existing and New Building Footprints
A-045	Site Plan Typical Details
A-081	Area Plans - GFA
A-082	Area Plans - GFA
A-083	Area Plans - GFA
A-085	Waste Management Plans
A-100	P2 Overall Floor Plans
A-101	P1 Overall Floor Plans
A-102	Level 1 Overall Floor Plans
A-103	Level 2 Overall Floor Plans
A-104	Level 3-4 Overall Floor Plans
A-105	Level 5 Overall Floor Plans
A-106 A-107	Level 6 Overall Floor Plans Level 7 Overall Floor Plans
A-107 A-108	Level 8 Overall Floor Plans
A-100 A-109	Level 9 Overall Floor Plans
A-110	Level 10-13 Typical Overall Floor Plans
A-111	Level 14-18 Typical Overall Floor Plans
A-112	Level 19-22 Typical Overall Floor Plans
A-113	MPH Overall Floor Plans
A-300	Building Elevations - South & West Elevations
A-301	Building Elevations - North & East Elevations
A-302	Building Elevations - Driveway Courtyard
A-303	Building Elevations - Amenity Courtyard
A-400	Building Sections
A-401	Building Sections

# OPC MATDIY

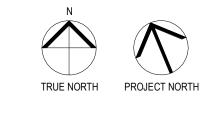
14 Mezzanine(s) Area m<sup>2</sup> N/A

Architectural Drawing Sheet List		B.C. MATRIX			
		ONTARIO BUILDING CO	DDE DATA MATRIX,	PART 3 and/or	9
Cover Sheet		DNESPACE UNLIMITED	,		
Drawing List, Site Statistics, OBC Matrix, Context Plan		Certificate of Practice #4009			
Existing Survey		3700 Steeles Ave. W, Suite 305			
Zoning Envelope & Setback Plan		/aughan, Ontario			
Future Development Plan		.4L 8K8			
Renderings		Fel (416)-848-1245 Fax (416)-848-1246 email dream@onespaceunlimited.com			
Perspectives		The Certificate of Practice number of the holder			
Perspectives					
Perspectives					
Site Plan		3403-3445 Fieldgate Drive, Development			
Existing and New Building Footprints					
Site Plan Typical Details		OCATION: 3403-3445 Fieldgate Drive Mississ	The Architect named above has exercise control with respect to design acti		
Area Plans - GFA		J	3 ,	The Architect's seal nur	
Area Plans - GFA			OBC	Reference	
Area Plans - GFA	Item	Ontario's 2012 Buildin	References are to Division B unless		
Waste Management Plans		Data Matrix Part 3	or 9		Division B unies A or [C] for Division
P2 Overall Floor Plans		Project Description	New 🔲 Part 11	■ Part 3	□ Part
P1 Overall Floor Plans	1	•	Addition 11.1 to 11.4		
Level 1 Overall Floor Plans		☐ Change of Use ☐ A		1.1.2.[A]	1.1.2.[A]
Level 2 Overall Floor Plans					
Level 3-4 Overall Floor Plans	2	lajor Occupancy(s) Group 'C' Residential (3.	· · · · · · · · · · · · · · · · · · ·	3.2.2.43.	9.10.2.
Level 5 Overall Floor Plans	3	uilding Area (m²) Existin <u>g 0</u> New	<u>7,007 m²</u> Total <u>7,007 m²</u>	1.4.1.2.[A]	1.4.1.2.[A
Level 6 Overall Floor Plans	4	Gross Area Existing 0 New	47,827 m <sup>2</sup> Total 47,827 m <sup>2</sup>	1.4.1.2.[A]	1.4.1.2.[A
Level 7 Overall Floor Plans	5	lumber of Storeys Above Grade 19	Below Grade 2	1.4.1.2.[A] & 3.2.1.	.1 1.4.1.2.[A
Level 8 Overall Floor Plans	6	lumber of Streets/Fire Fighter Access 2		3.2.2.10. & 3.2.5.	9.10.20.
Level 9 Overall Floor Plans	7	Building Classification Group 'C' Residential, (	Group 'F' Division 3	3.2.2.20-83.	9.10.2.
Level 10-13 Typical Overall Floor Plans					
Level 14-18 Typical Overall Floor Plans	8	prinkler System Proposed	J	3.2.2.20-83. 3.2.1.5.	9.10.8.2.
Level 19-22 Typical Overall Floor Plans		□ Selecte	d Compartments	3.2.2.17.	
MPH Overall Floor Plans		□ Selecte	d Floor Areas	INDEX	INDEX
Building Elevations - South & West Elevations		☐ Baseme	ent 🛘 In Lieu of Roof Rating		
Building Elevations - North & East Elevations		□ Not Red	quired		



3.2.1.1.(3)-(8) 9.10.4.1.





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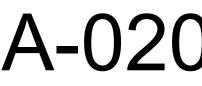
Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4

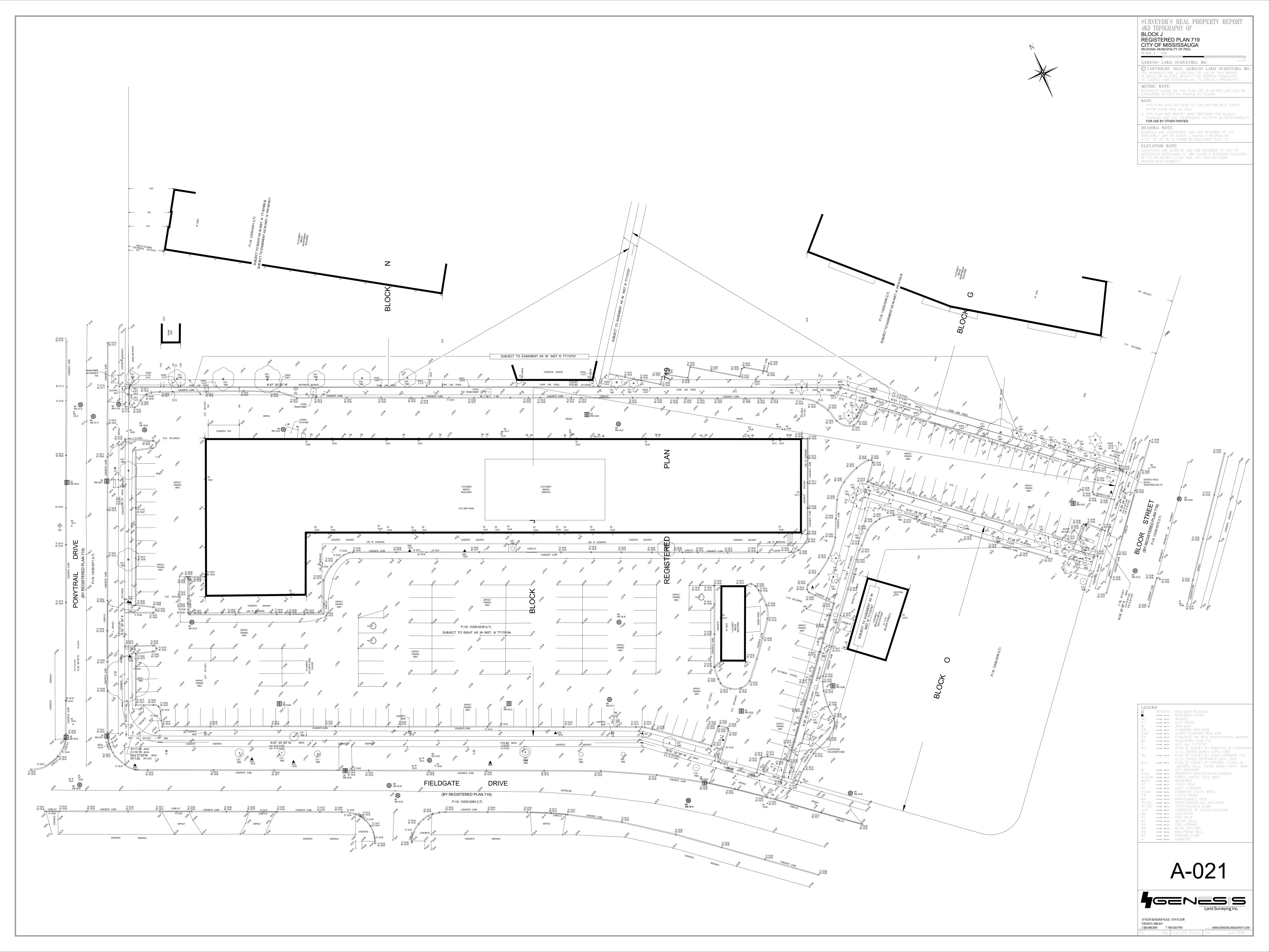
3403-3445 Fieldgate Drive Mississauga, ON

Drawing List, Site Statistics, OBC Matrix, Context Plan

PROJECT NUMBER 23063 PLOTTED DATE As indicated 08/08/2024 12:22:10

Context Plan Scale: 1 : 2000



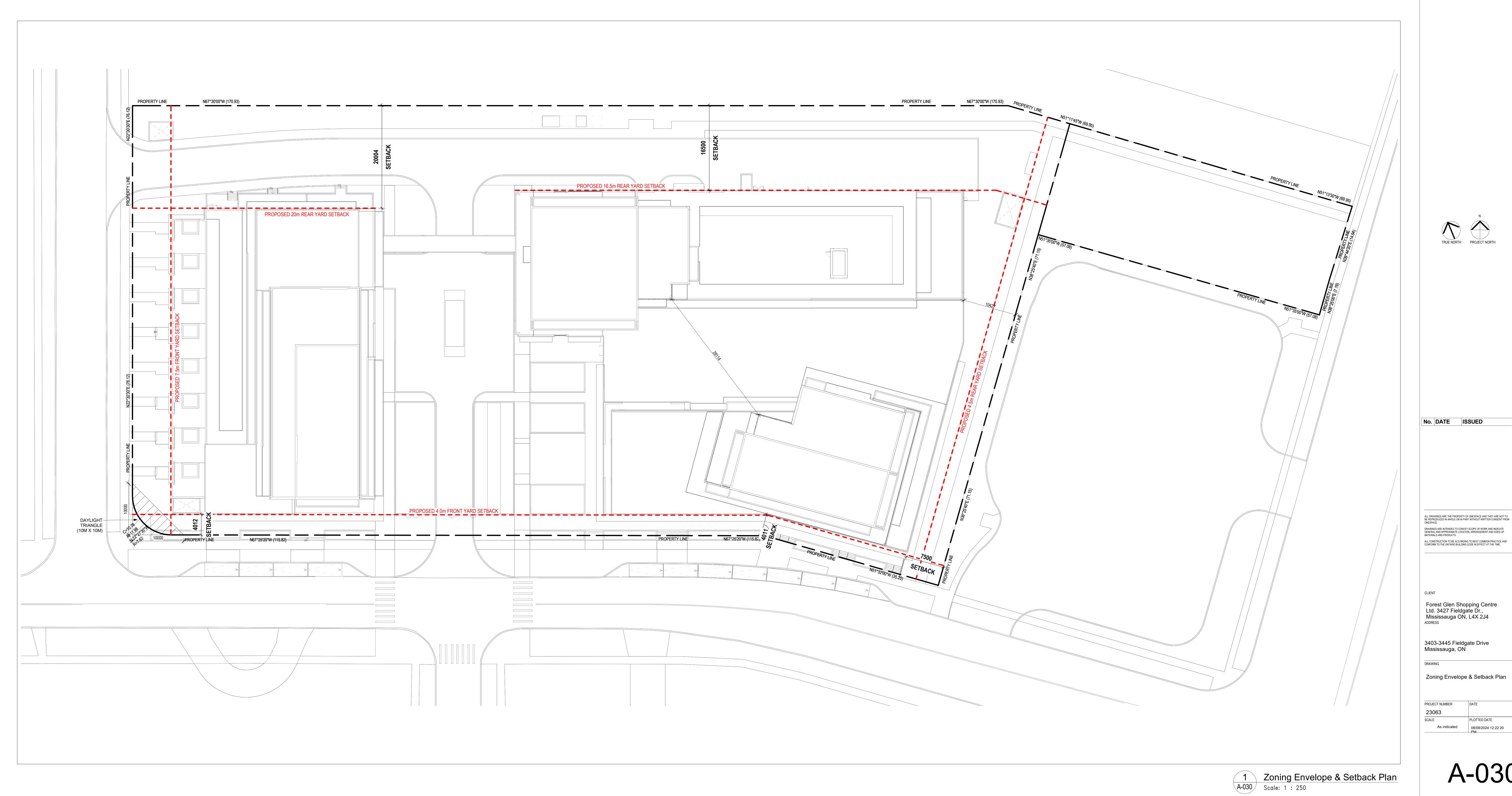


ZONING ENV	ELOPE LEGEND
••••	MINIMUM SETBACK AS PER BY-LAW
	PROJECT EASEMENTS - IF APPLICABLE
	PROJECT ROAD WIDENINGS - IF APPLICABLE



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<u>UNITS</u> RESIDENTIAL UNITS 250 SUITES onespace unlimited inc. architecture + interior design 3700 STEELES AVE WEST. SUITE 305. RESIDENTIAL UNIT COUNT WILL VARY BASED ON TYPICAL SUITE SIZE VAUGHAN,
ONTARIO L4L 8K8 TEL: (416)
848-1245 FAX: (416)
848-1246
WWW.ONESPACEUNLIMITED.COM ESTIMATED INDOOR AMENITY 700 m<sup>2</sup>
ESTIMATED OUTDOOR AMENITY 700 m<sup>2</sup>
TOTAL ESTIMATED AMENITY 1400 m<sup>2</sup> ESTIMATED AMENITY SPACE PER RESIDENTIAL UNIT 5.6 m² 
 PARKING

 GRADE
 3

 P1 LEVEL
 161

 P2 LEVEL
 161

 TOTAL
 325
 PARKING PROVIDED = 1.3 SPACES / UNIT INCLUSIVE OF VISITOR GENERAL NOTES: FULL CAD SURVEY IS REQUIRED N67°30'00"W (170.93) PROPERTY LINE N67°30'00"W (170.93) PROPOSED 16.5m REAR YARD SETBACK PROPOSED 20m REAR YARD SETBACK TRUE NORTH PROJECT NORTH No. DATE ISSUED PROPOSED 4.0m FRONT YARD SETBACK ALL DRAWINGS ARE THE PROPERTY OF ONESPACE AND THEY ARE NOT TO BE REPRODUCED IN WHOLE OR IN PART WITHOUT WRITTEN CONSENT FROM ONESPACE. DAYLIGHT TRIANGLE (10M X 10M) DRAWINGS ARE INTENDED TO CONVEY SCOPE OF WORK AND INDICATE GENERAL AND APPROXIMATE LOCATION, ARRANGEMENT AND SIZES OF MATERIALS AND PRODUCTS. ALL CONSTRUCTION TO BE ACCORDING TO BEST COMMON PRACTICE AND CONFORM TO THE ONTARIO BUILDING CODE IN EFFECT AT THE TIME. CLIENT Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4 3403-3445 Fieldgate Drive Mississauga, ON Future Development Plan PROJECT NUMBER 23063 As indicated 1 Future Development Plan
A-031 Scale: 1 : 250

SITE STATISTICS

SITE AREA 4050.12 m² (43,595 sq.ft.)

FSI (GFA / SITE AREA) 4 67

GROSS FLOOR AREA

RESIDENTIAL 18,912.96 m² (203,577 sq.ft.) (EXCLUDES MPH)

TOTAL 18,912.96 m² (203,577 sq.ft.)

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PLOTTED DATE 08/08/2024 12:22:30









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No. Date Revision

A-036

07 FEB 2024 PLOTTED DATE

08/08/2024 12:23:44







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ALL CONSTRUCTION TO BE ACCORDING TO BEST COMMON PRACTICE AND CONFORM TO THE ONTARIO BUILDING CODE IN EFFECT AT THE TIME.

CLIENT
Forest Glen Shopping

Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4 ADDRESS

3403-3445 Fieldgate Drive Mississauga, ON

DRAWING

Perspectives

PROJECT NUMBER

23063

SCALE

PLOTTED DATE

08/08/2024 12:24:36

A-037

1 West Elevation Perspective
A-037 NTS



2 Perspective Courtyard Entrance
A-038 NTS



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A-038 Perspective from Neighbour





PLOTTED DATE

08/08/2024 12:26:01

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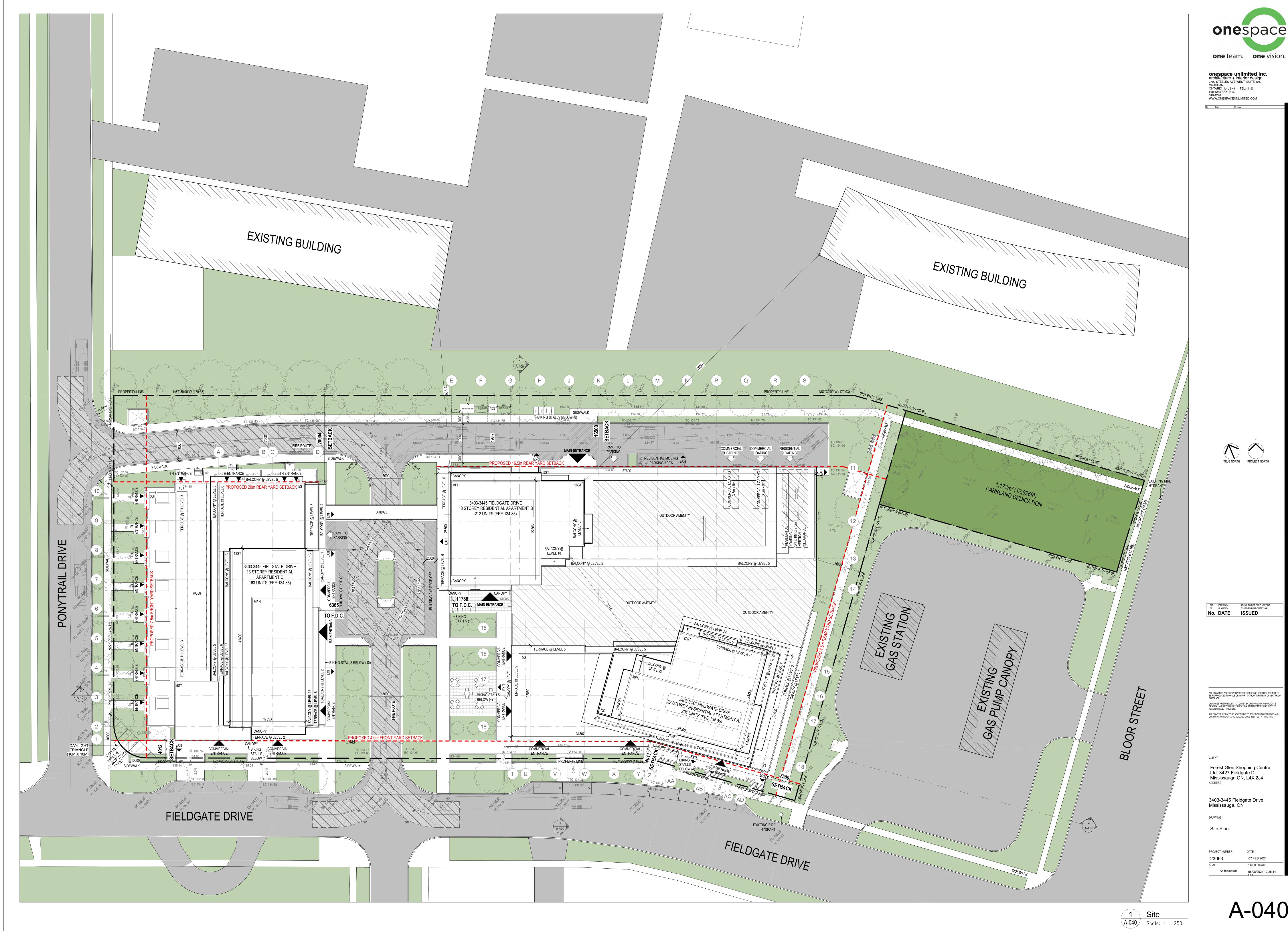
Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4

3403-3445 Fieldgate Drive Mississauga, ON

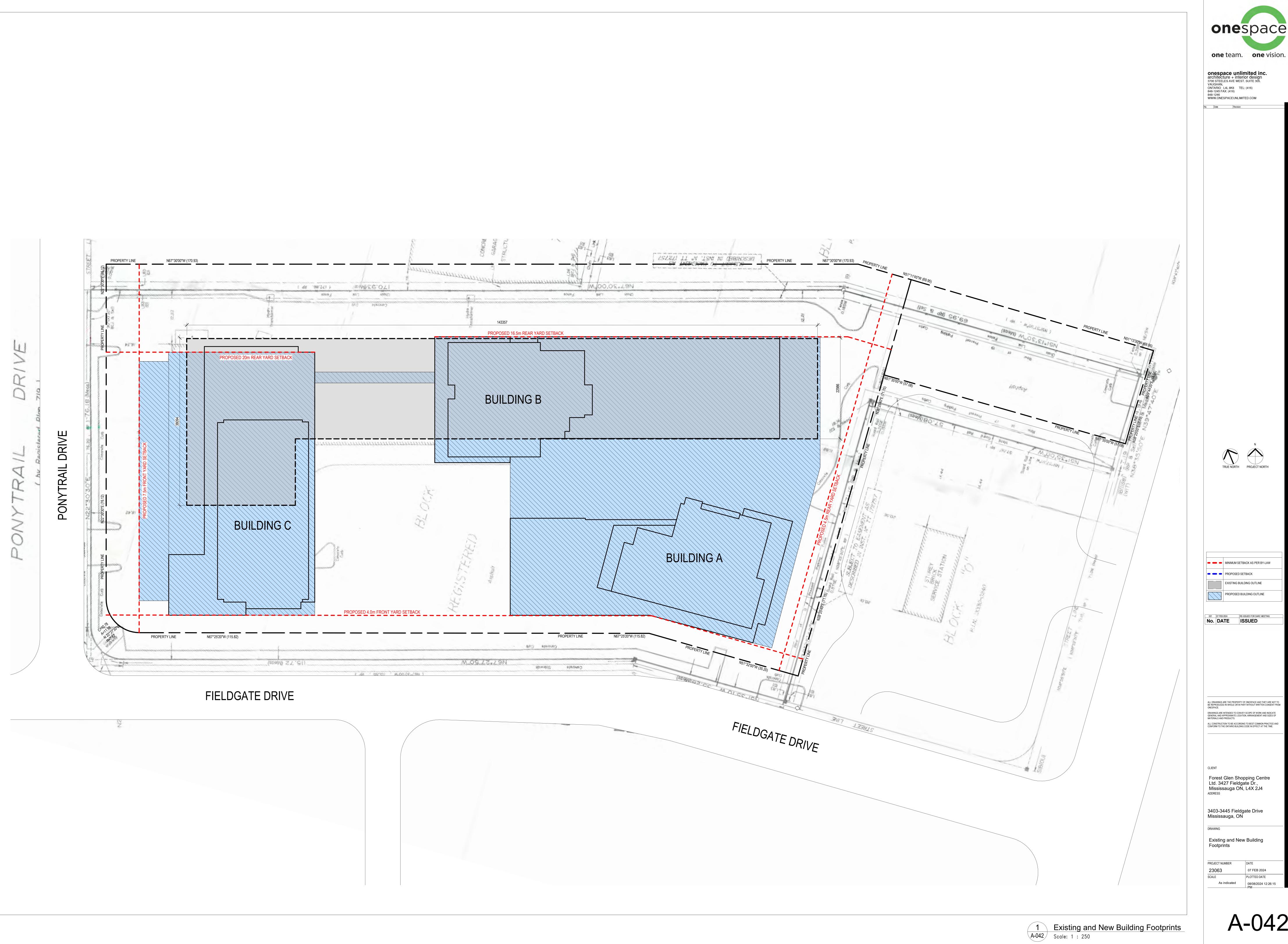
Perspectives

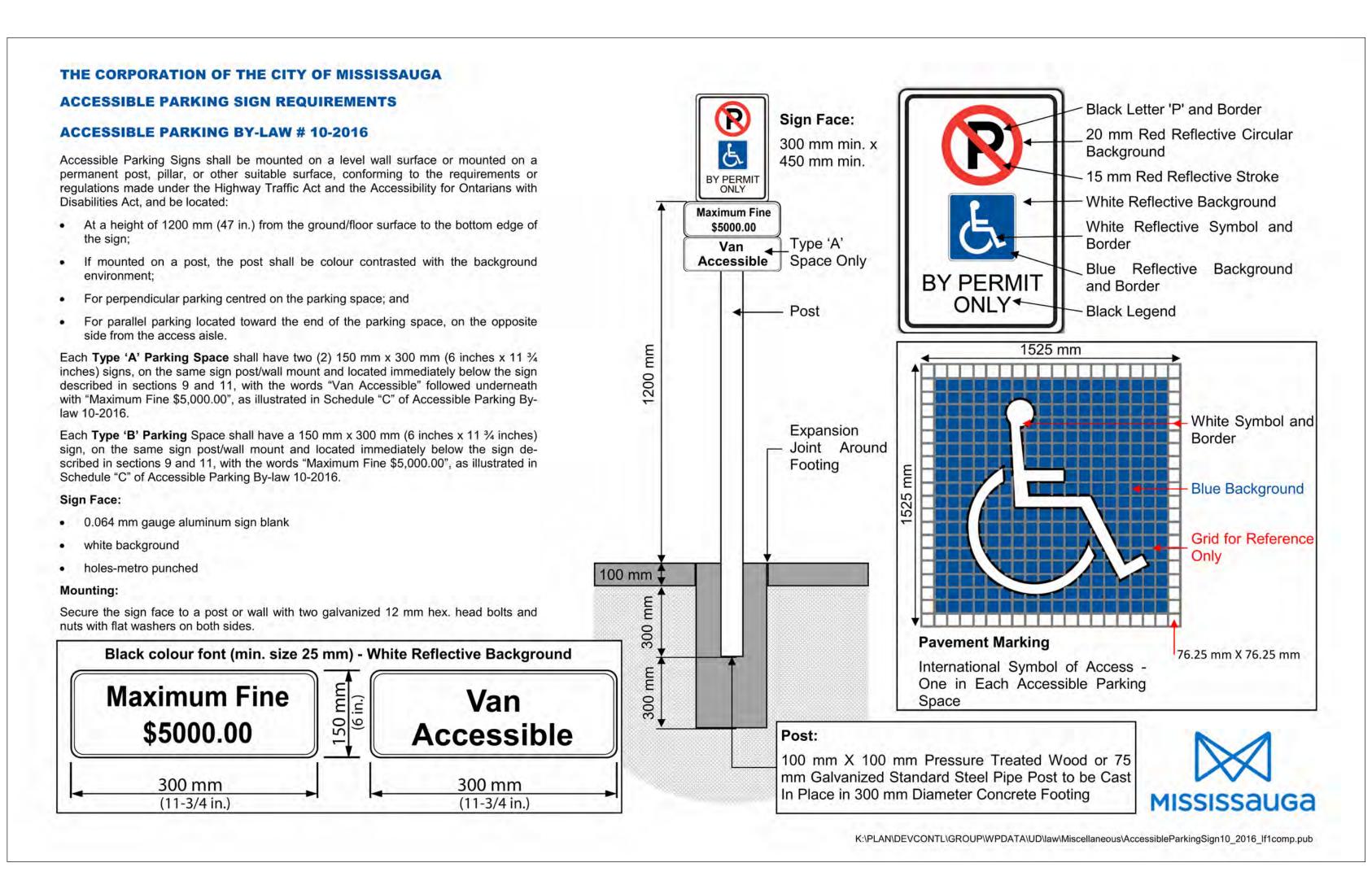
PROJECT NUMBER

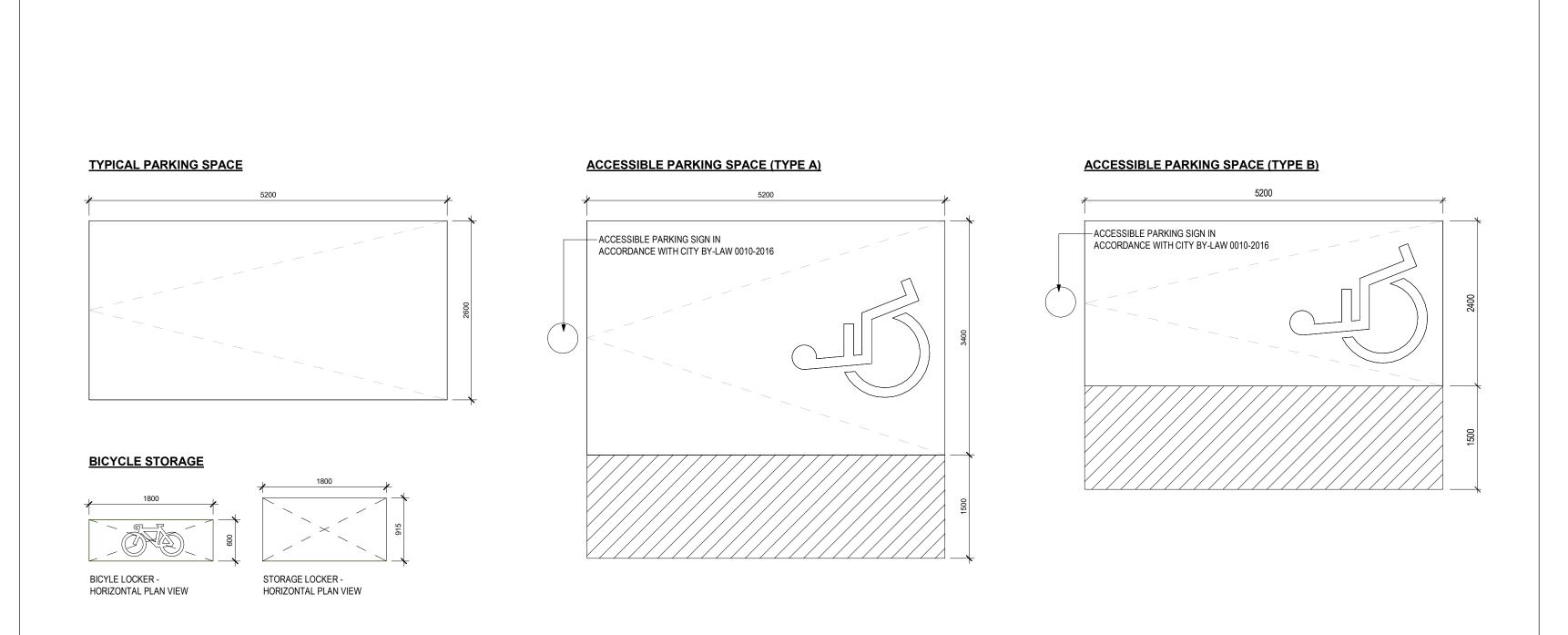
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A-040







FIRE DEPARTMENT CONNECTION SIGNAGE

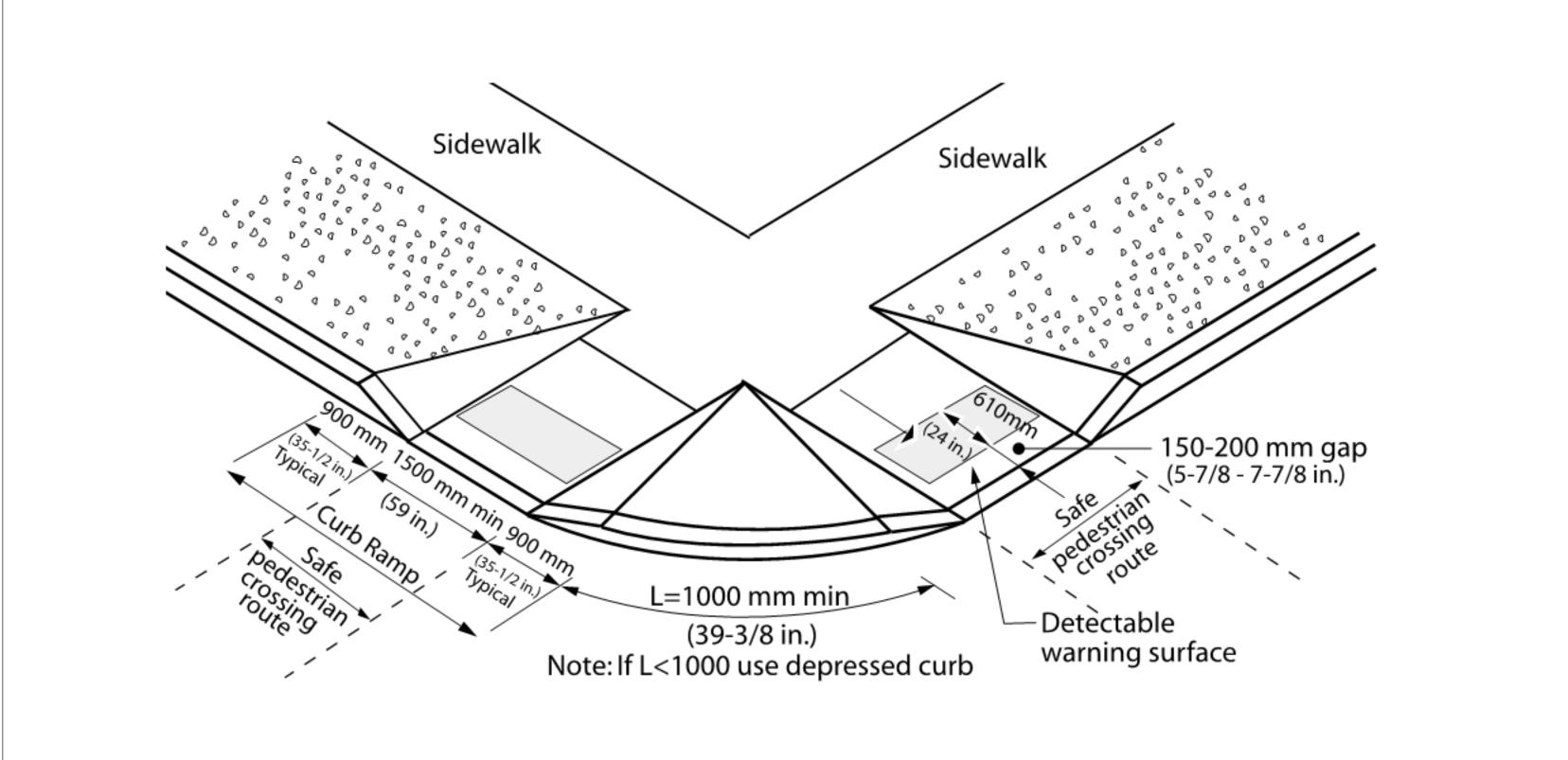
obstruct view of the sign from responding fire department).

 FDC signs shall be visible from the street or fire route to the approaching fire department.
 Signs shall be "FIRE DEPARTMENT STANDPIPE CONNECTION" in 50 mm high (minimum) letters. Signs shall be a reflective White Background with reflective red lettering.

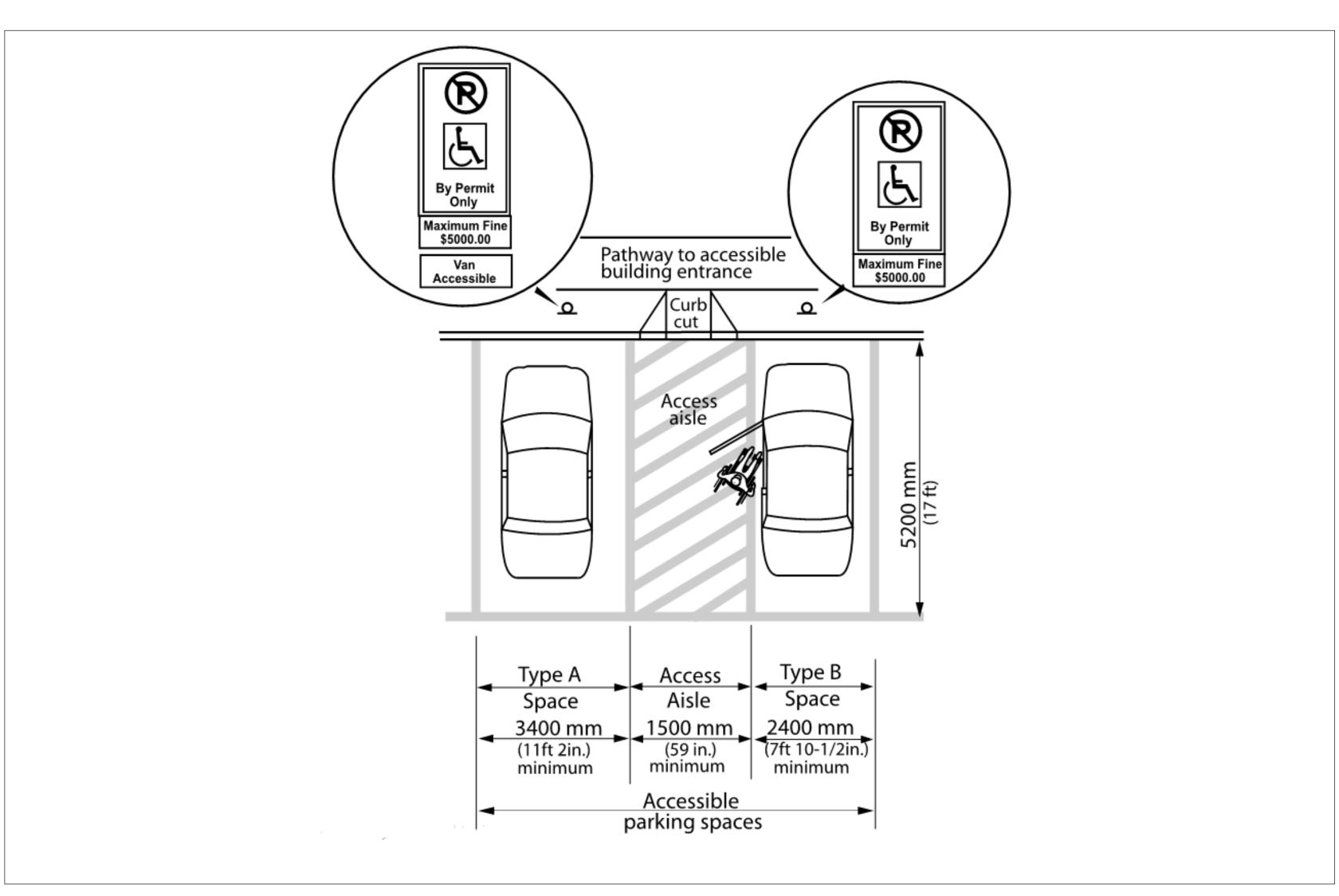
Sign shall be installed above the connection a minimum of 2.1 metres to 2.7 metres above grade to the bottom of the sign. (sign required to be installed above vehicles that may

Free-standing Fire Department Connections shall be identified with an approved sign mounted on a post. Sign shall be installed 1.8 metres minimum to bottom of sign.

# TYPICAL ACCESSIBLE PARKING SIGNAGE

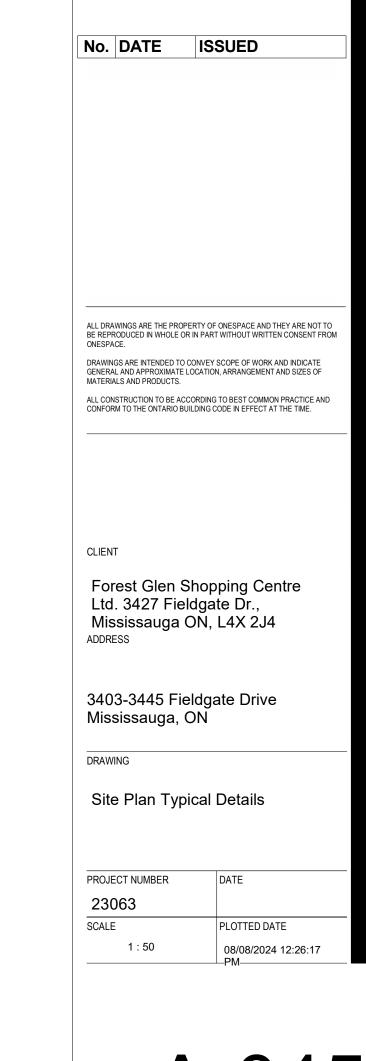


# STANDARD CURB RAMP



# **CONNECTION SIGNAGE (FDC)**

**PARKING & BICYCLE STALLS** 

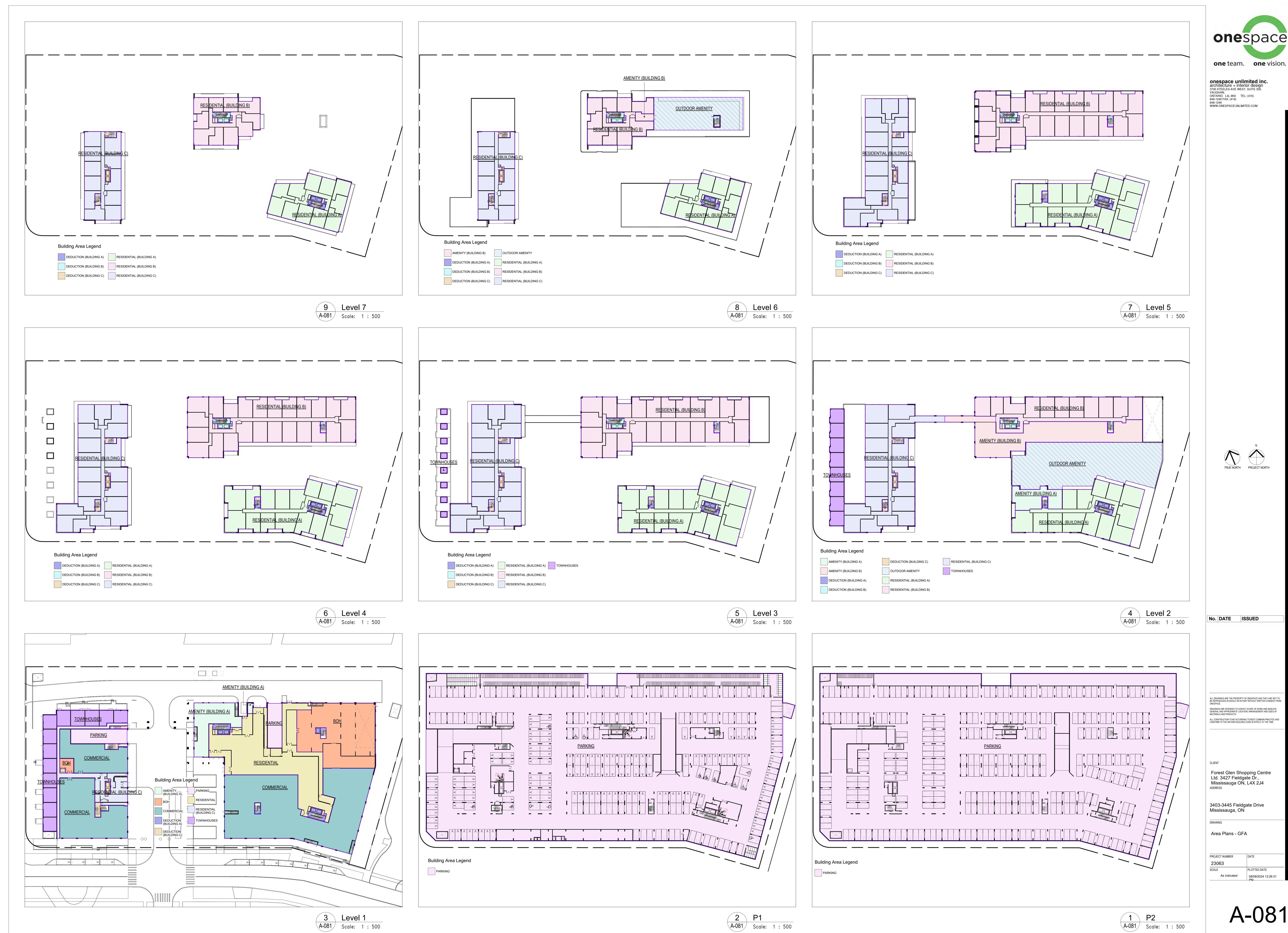


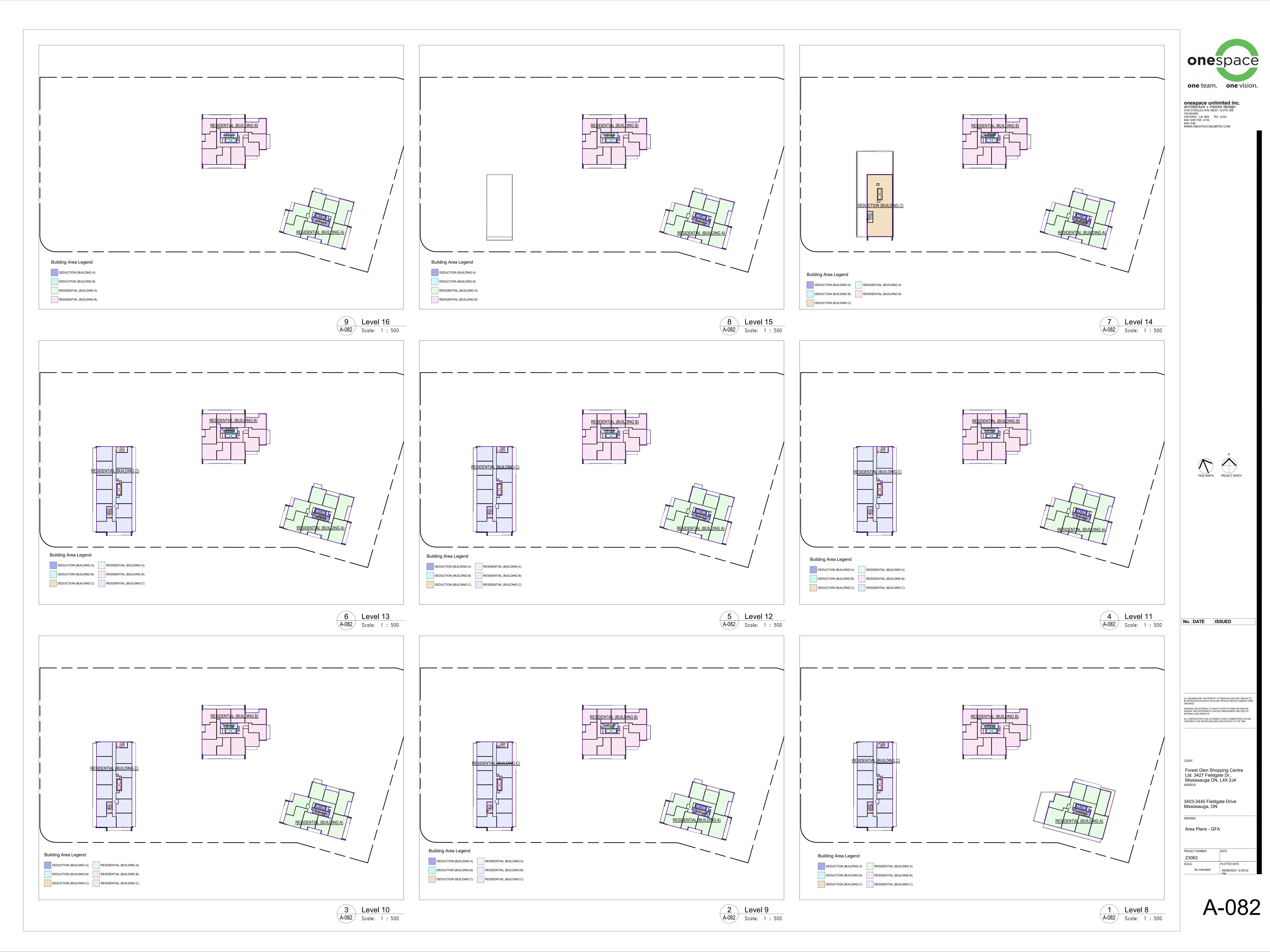
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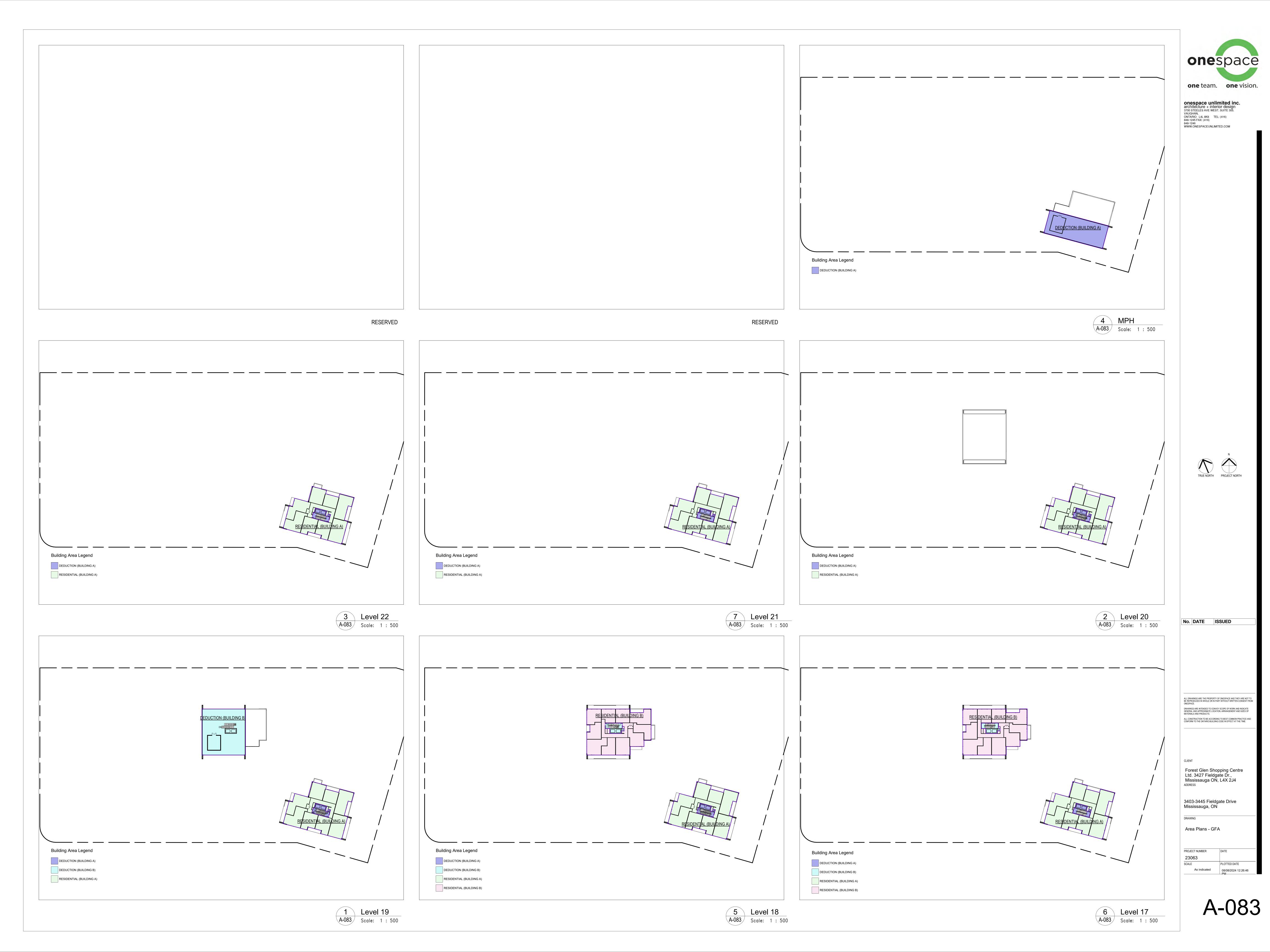
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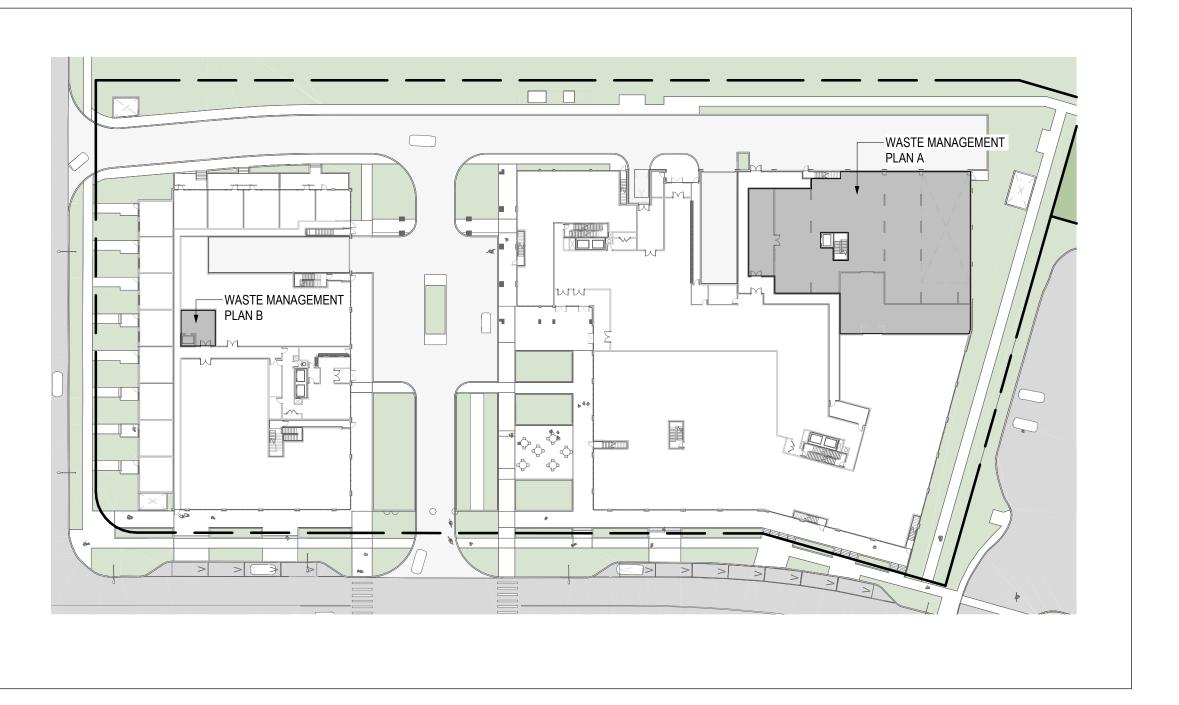
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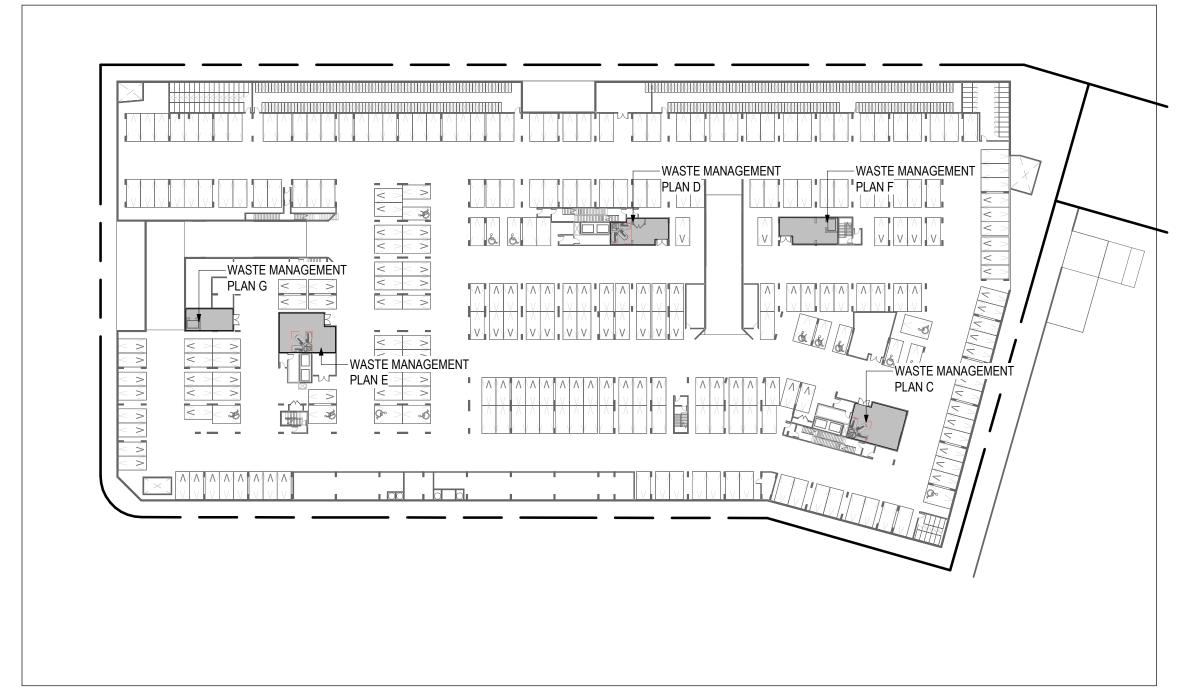




10 Level 1 Key Plan
A-085 NTS

3 Waste Management Plan C A-085 Scale: 1:100

2 Waste Management Plan B
A-085 Scale: 1: 100



		REQUIRED	PROPOSED				
		TOTAL	TOTAL	GROUND FLOOR WASTEROOM/ LOADING	WASTEROOM 1 (P1)	WASTEROOM 2 (P1)	WASTEROOM 3 (P1)
UNITS S	SERVED	592 MULTI-RESIDENTIAL UNITS (579 APARTMENTS + 13 TOWNHOUSES)	592 MULTI-RESIDENTIAL UNITS (579 APARTMENTS + 13 TOWNHOUSES)	N/A	BUILDING A (204 APARTMENTS)	BUILDING B (212 APARTMENTS)	BUILDING C & TOWNHOUSE: (163 APARTMENTS + 13 TOWNHOUSES)
ARI	EA	N/A	1209.49m² (INCLUDING BULKY WASTE AREA)	1023.75 m² (INCLUDING BULKY WASTE AREA)	68.55 m²	46.74 m²	70.45 m²
TRAVEL D	DISTANCE	MAX 45 m EXTERIOR TRAVEL DISTANCE (TOWNHOUSE TO GARBAGE CHUTE)	74 m INTERIOR TRAVEL DISTANCE (TOWNHOUSE TO GARBAGE CHUTE)	N/A	N/A	N/A	N/A
MIN.	GARBAGE MATERIAL	102 yd³ = 34 x 3 yd³ FRONT-END BIN	111 yd³ = 37 x 3 yd³ FRONT-END BIN	27 x 3 yd³ FRONT-END BIN	3 x 3 yd³ FRONT-END BIN	3 x 3 yd³ FRONT-END BIN	4 x 3 yd³ FRONT-END BIN
NUMBER OF FRONT END CONTAINERS	RECYCLABLE MATERIAL	45 yd³ = 15 x 3 yd³ FRONT-END BIN	54 yd³ = 18 x 3 yd³ FRONT-END BIN	11 x 3 yd³ FRONT-END BIN	2 x 3 yd³ FRONT-END BIN	2 x 3 yd³ FRONT-END BIN	3 x 3 yd³ FRONT-END BIN
	ORGANIC MATERIAL	$0 \text{ yd}^3 = 0 \times 3 \text{ yd}^3 \text{ FRONT-END BIN}$	0 yd³ = 0 x 3 yd³ FRONT-END BIN	0 x 3 yd³ FRONT-END BIN	0 x 3 yd³ FRONT-END BIN	0 x 3 yd³ FRONT-END BIN	0 x 3 yd <sup>3</sup> FRONT-END BIN
BULKY WAS	TE STORAGE	10 m² FOR EACH MULTI- RESIDENTIAL COMPLEX	30 m²	30 m²	0 m²	0 m²	0 m²

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GENERAL NOTES:
1.TRAINED ON-SITE STAFF MEMBER WILL BE AVAILABLE TO MANOEUVRE BINS FOR THE COLLECTION DRIVER AND ALSO ACT AS A FLAGMAN WHEN THE TRUCK IS REVERSING. IN THE EVENT THE ON-SITE STAFF IS UNAVAILABLE AT THE TIME THE CITY COLLECTION VEHICLE ARRIVES AT THE SITE, THE COLLECTION VEHICLE WILL LEAVE THE SITE AND NOT RETURN UNTIL THE NEXT SCHEDULED COLLECTION DAY.
2.THE LOADING SPACE IS LEVEL (+-2%), AND IS CONSTRUCTED OF A MINIMUM OF 200MM REINFORCED CONCRETE.
3.THE STAGING PAD ABUTTING THE FRONT OF THE LOADING SPACE IS LEVEL (+/-2%), AND IS CONSTRUCTED OF A MINIMUM OF 200 MM REINFORCED CONCRETE.
4.THAT ALL ACCESS DRIVEWAYS TO BE USED BY THE COLLECTION VEHICLE WILL BE LEVEL (MAXIMUM 8% SLOPE), HAVE A MINIMUM VERTICAL CLEARANC OF 4.4 METRES THROUGHOUT.

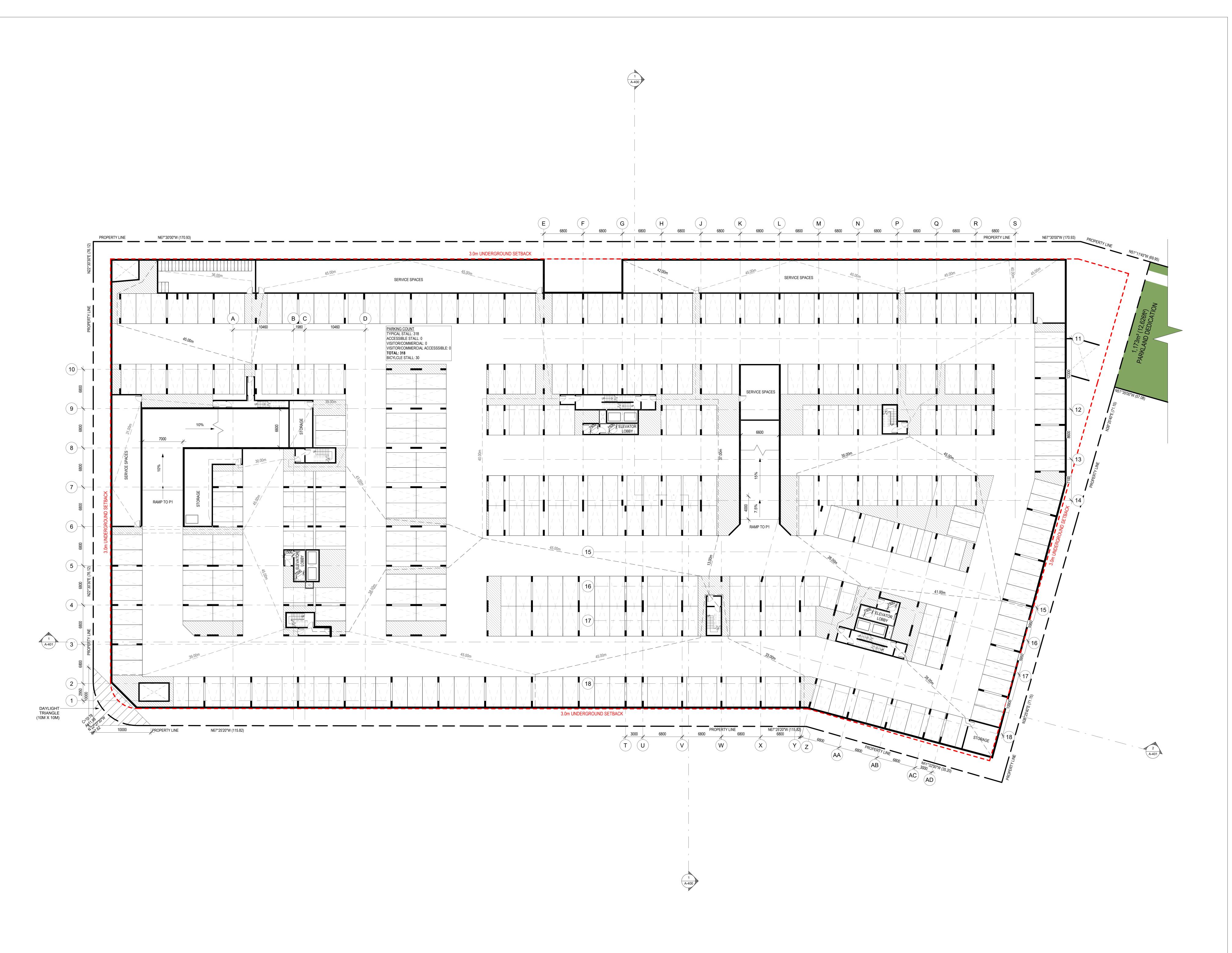


9 P1 Key Plan A-085 NTS

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08/08/2024 12:27:03

As indicated



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Forest Glen Shopping Centre
Ltd. 3427 Fieldgate Dr.,
Mississauga ON, L4X 2J4
ADDRESS

3403-3445 Fieldgate Drive Mississauga, ON

DRAWING
P2 Overall Floor Plai

P2 Overall Floor Plans

PROJECT NUMBER

23063

SCALE

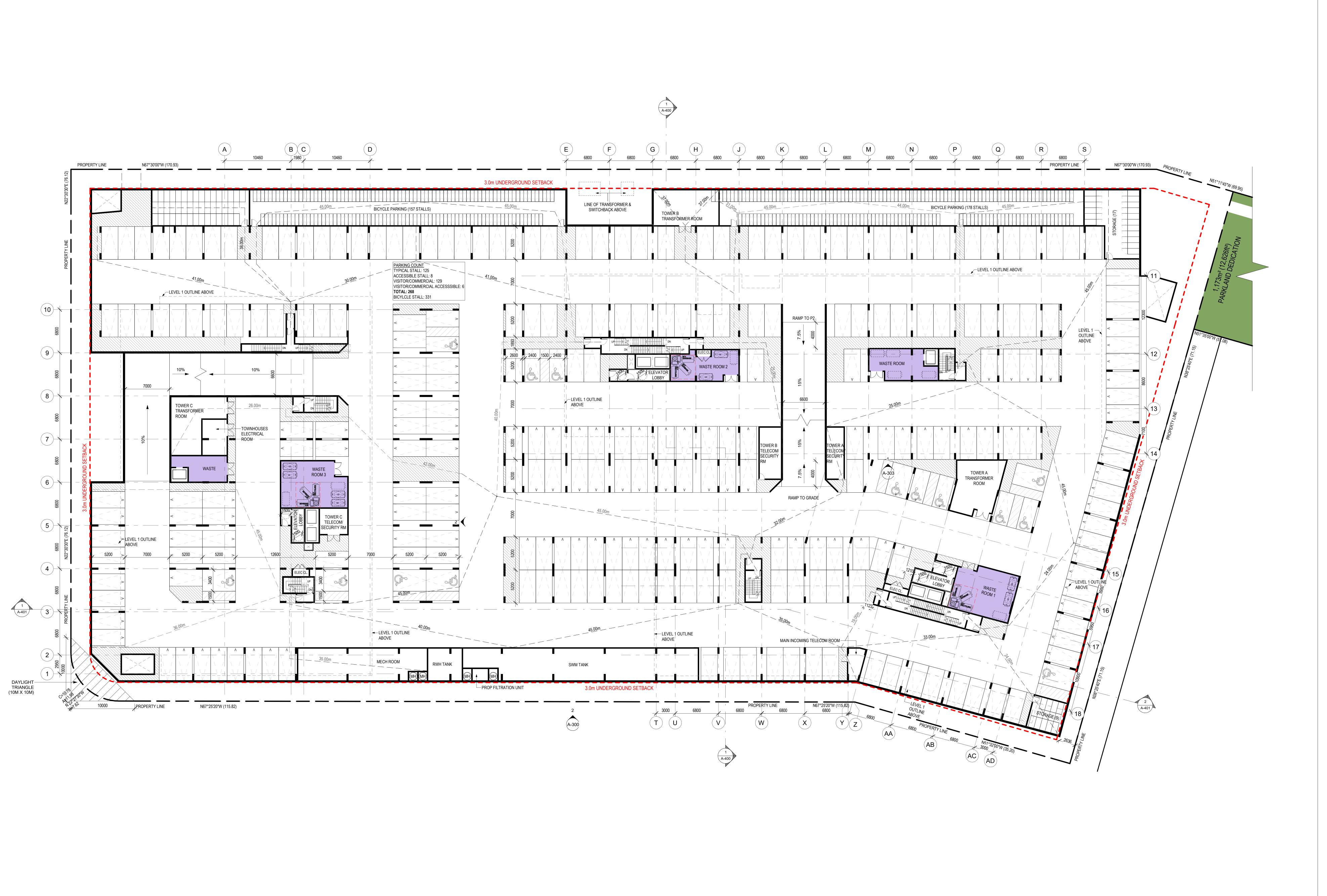
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DRAWING
P1 Overall Floor Plans

PROJECT NUMBER

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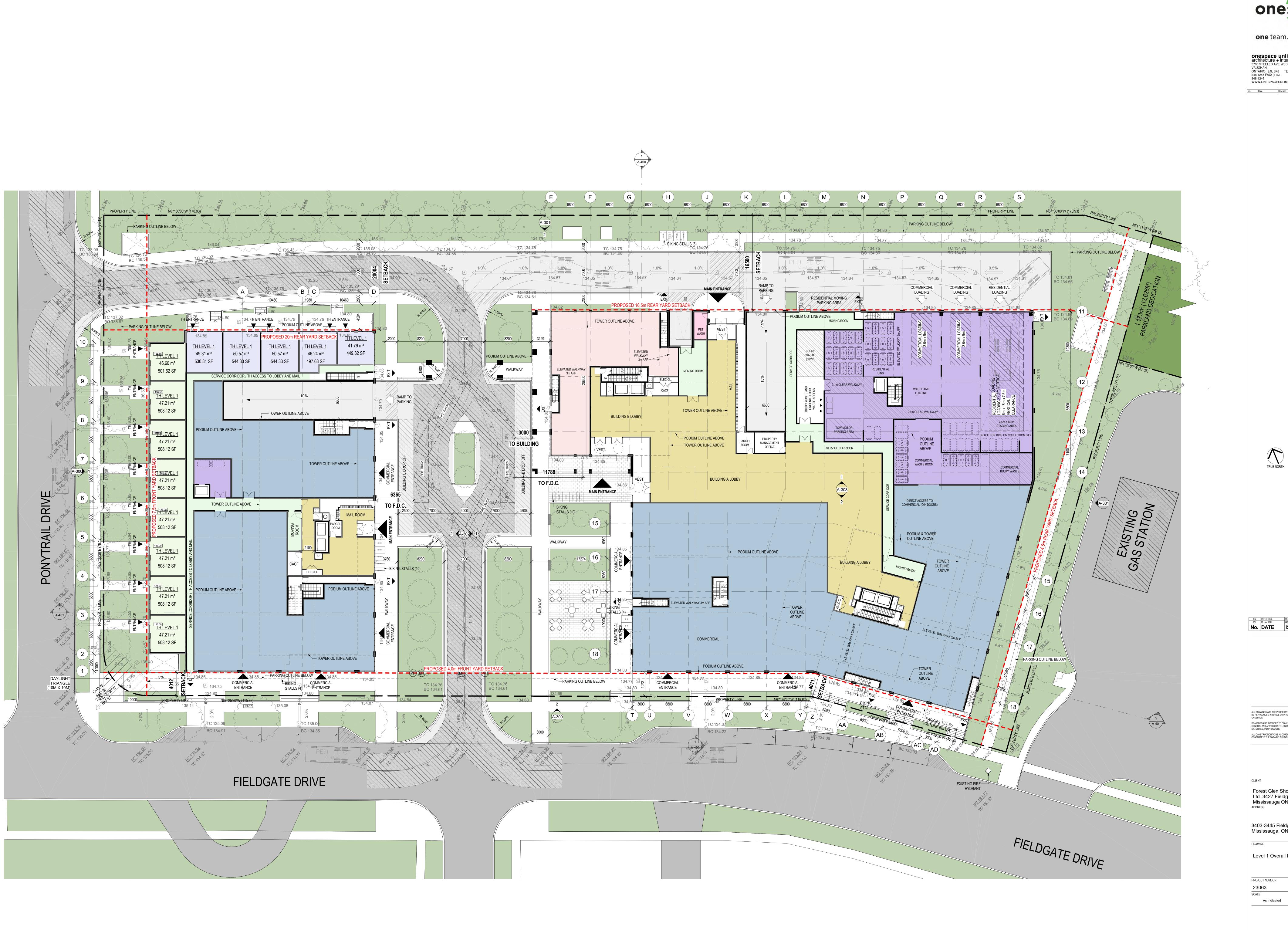
SCALE

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1 P1 A-101 Scale: 1 : 200 A-101

08/08/2024 12:27:10





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3403-3445 Fieldgate Drive Mississauga, ON

Level 1 Overall Floor Plans

PROJECT NUMBER 23063 07 FEB 2024 PLOTTED DATE 08/08/2024 12:27:25 As indicated





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Level 2 Overall Floor Plans

PROJECT NUMBER

23063

O7 FEB 2024

SCALE

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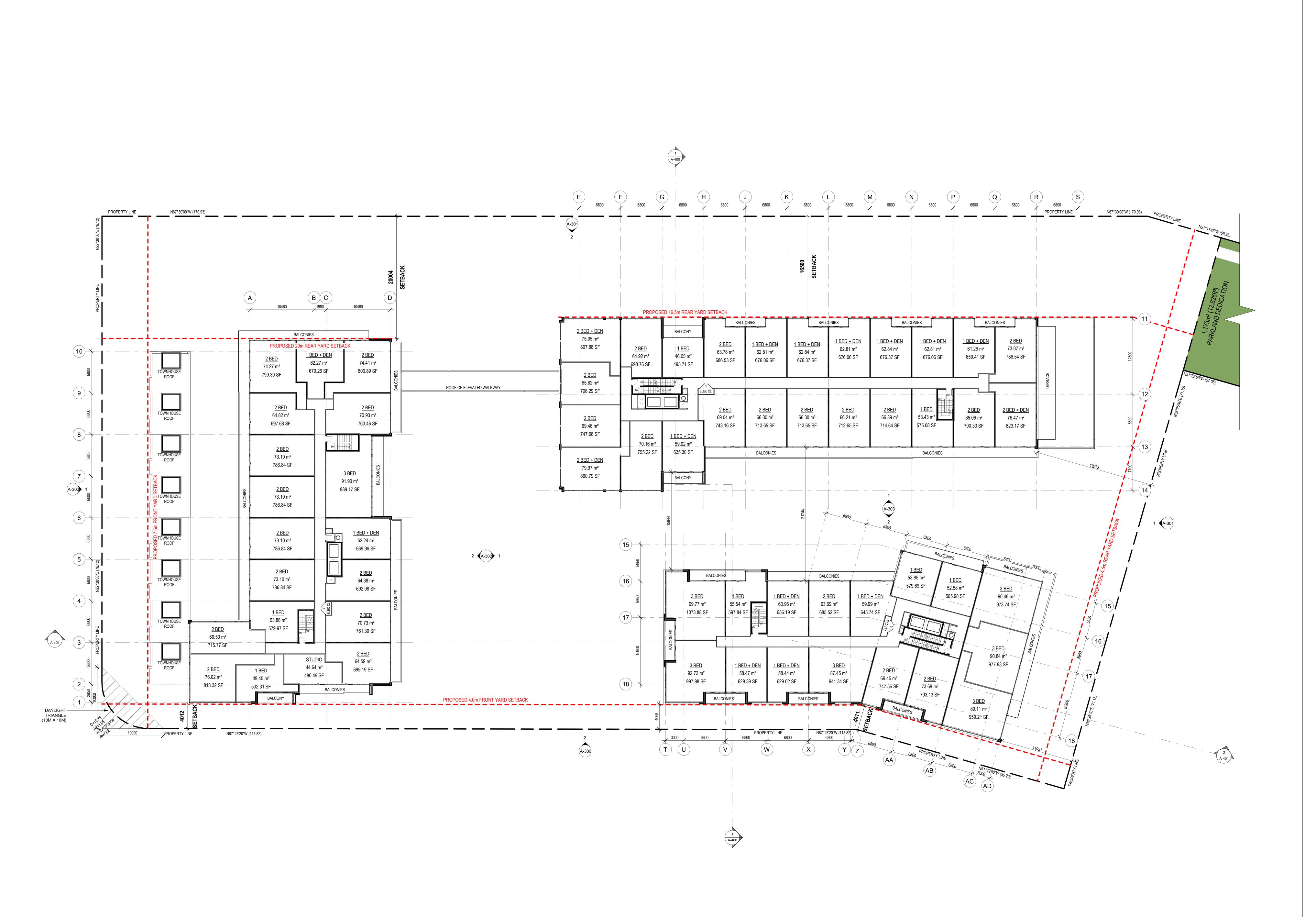
DATE

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08/08/2024 12:27:28

1 Level 2
A-103 Scale: 1 : 200



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Level 3-4 Overall Floor Plans

PROJECT NUMBER

23063

SCALE

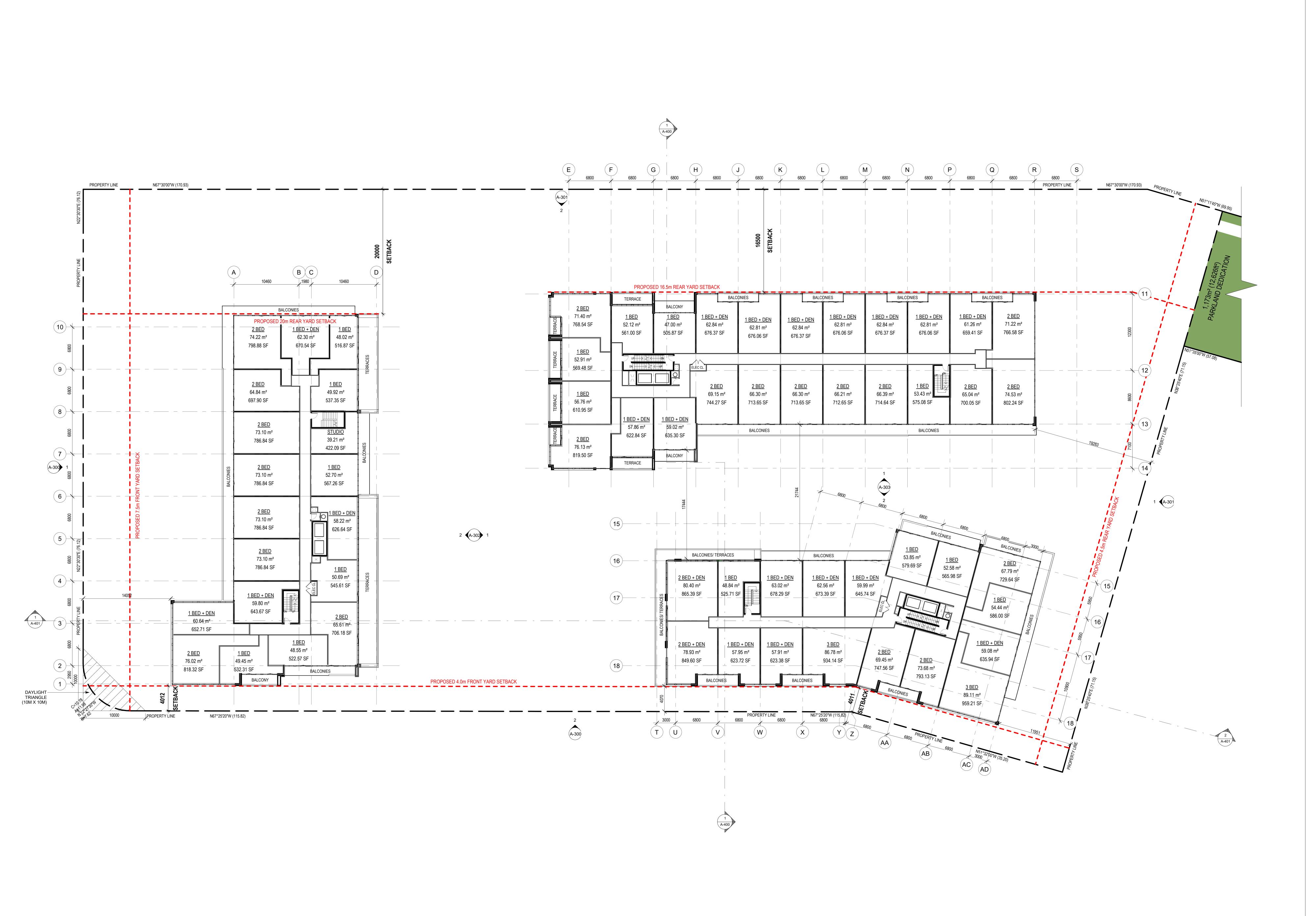
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3403-3445 Fieldgate Drive Mississauga, ON

Level 5 Overall Floor Plans

PROJECT NUMBER

23063

SCALE

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DATE

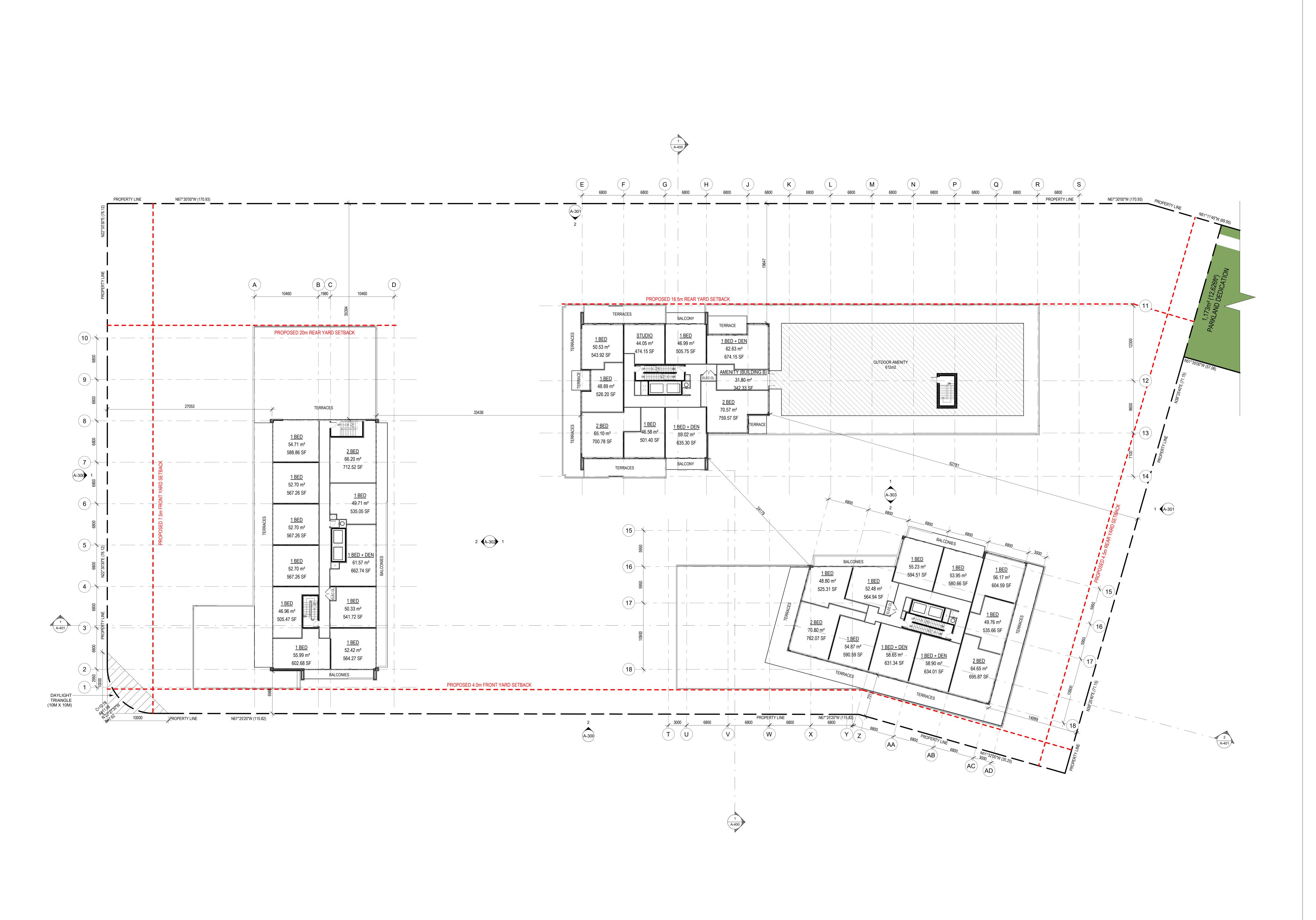
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PM

1 Level 5
A-105 Scale: 1 : 200



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3403-3445 Fieldgate Drive Mississauga, ON

DRAWING

23063

1 Level 6
A-106 Scale: 1 : 200

Level 6 Overall Floor Plans

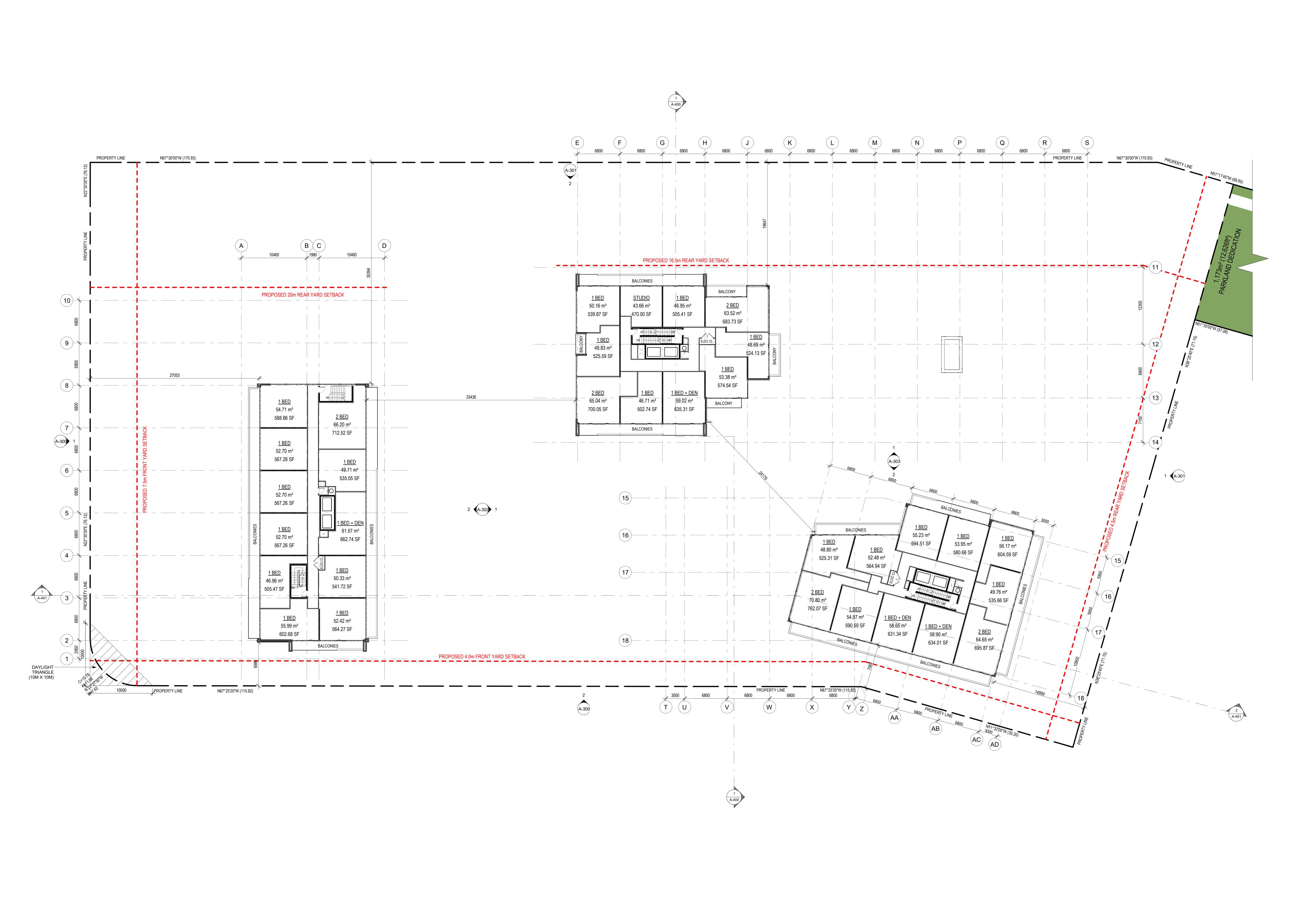
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3403-3445 Fieldgate Drive Mississauga, ON

Level 7 Overall Floor Plans

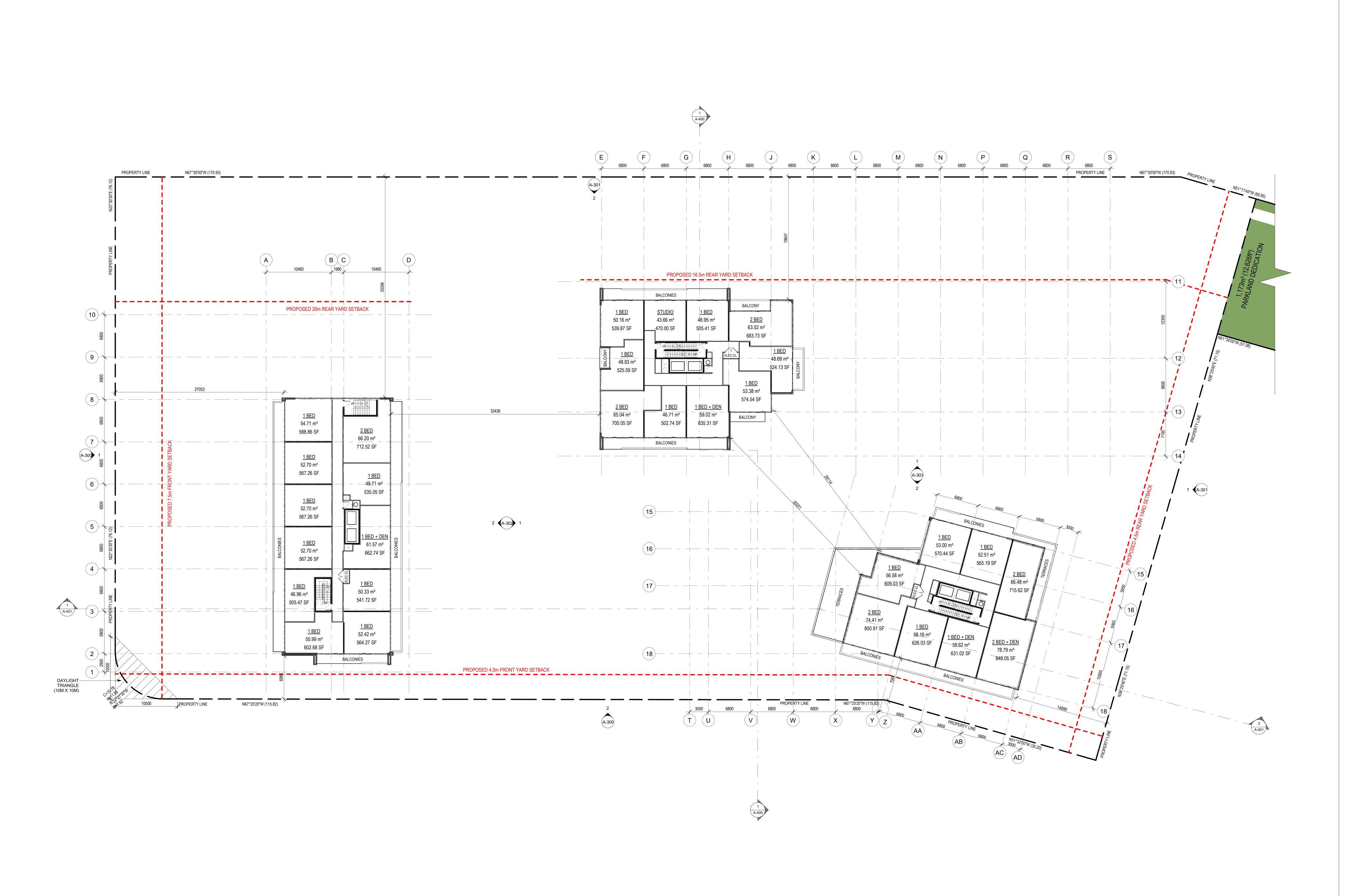
PROJECT NUMBER DATE

23063

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1 Level 7
A-107 Scale: 1 : 200



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3403-3445 Fieldgate Drive Mississauga, ON

DRAWING

Level 8 Overall Floor Plans

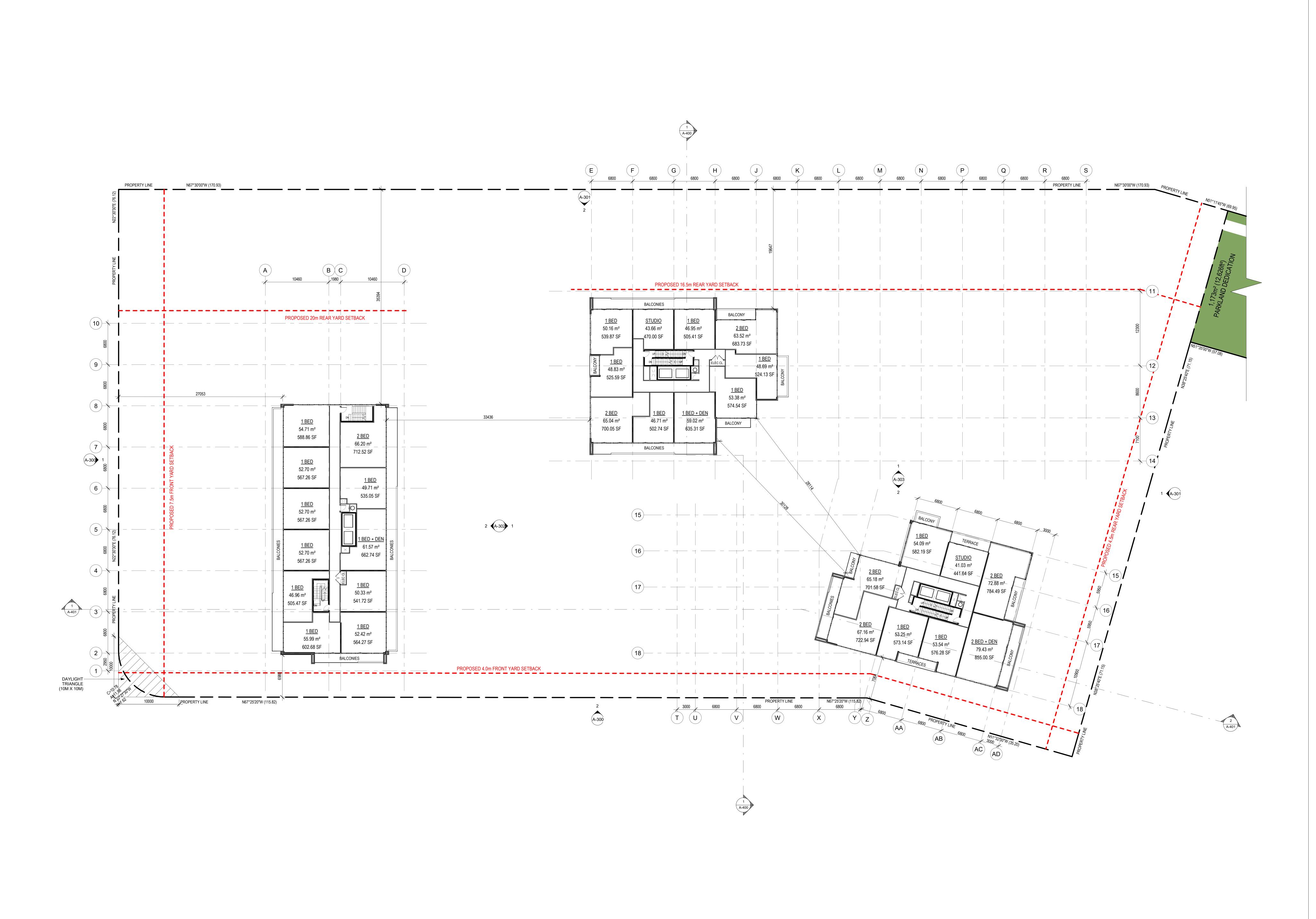
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1 Level 8
A-108 Scale: 1 : 200



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3403-3445 Fieldgate Drive Mississauga, ON

DRAWING

Level 9 Overall Floor Plans

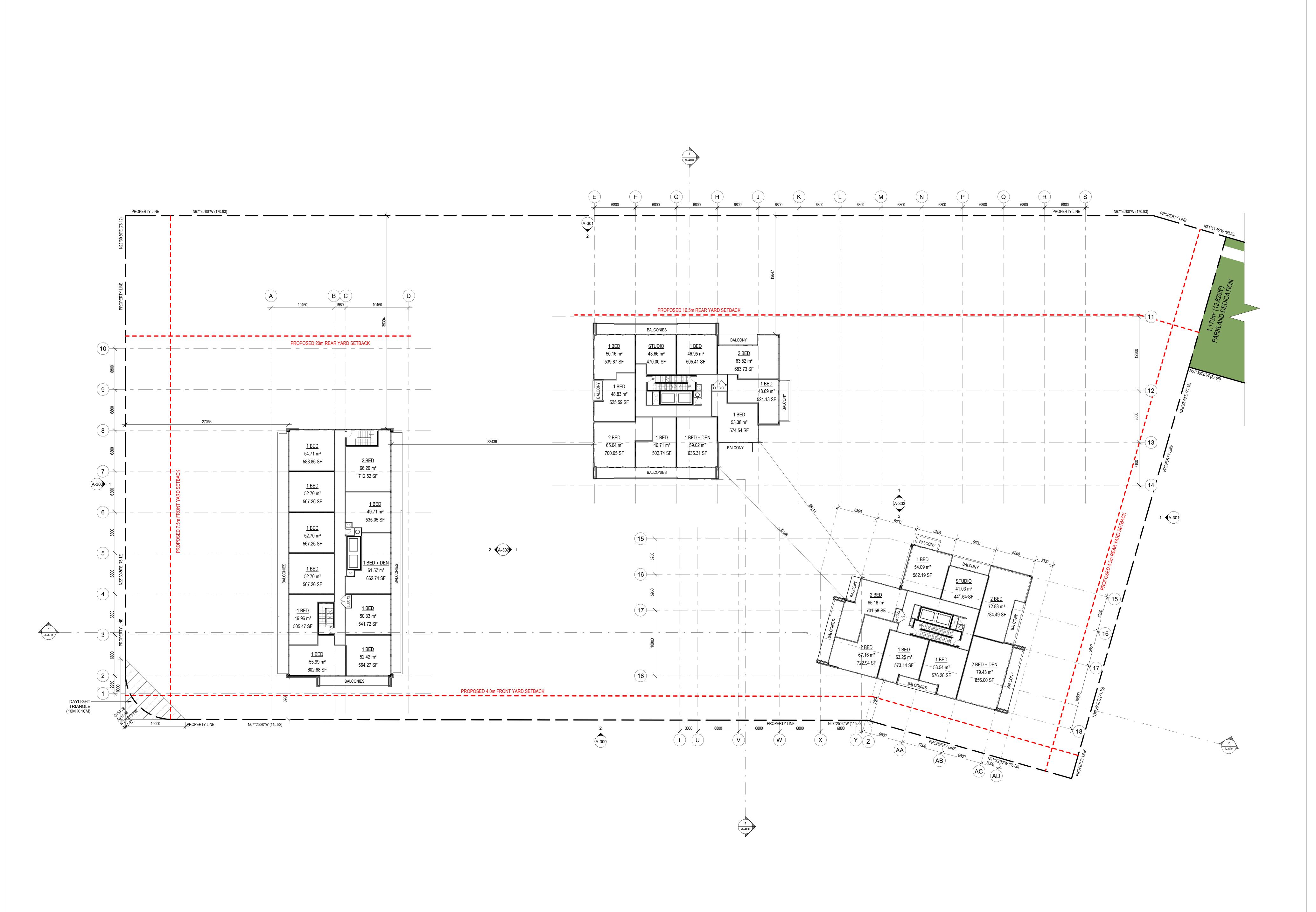
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1 Level 9
A-109 Scale: 1 : 200





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Forest Glen Shopping Centre
Ltd. 3427 Fieldgate Dr.,
Mississauga ON, L4X 2J4

3403-3445 Fieldgate Drive Mississauga, ON

Level 10-13 Typical Overall Floor Plans

PROJECT NUMBER

23063

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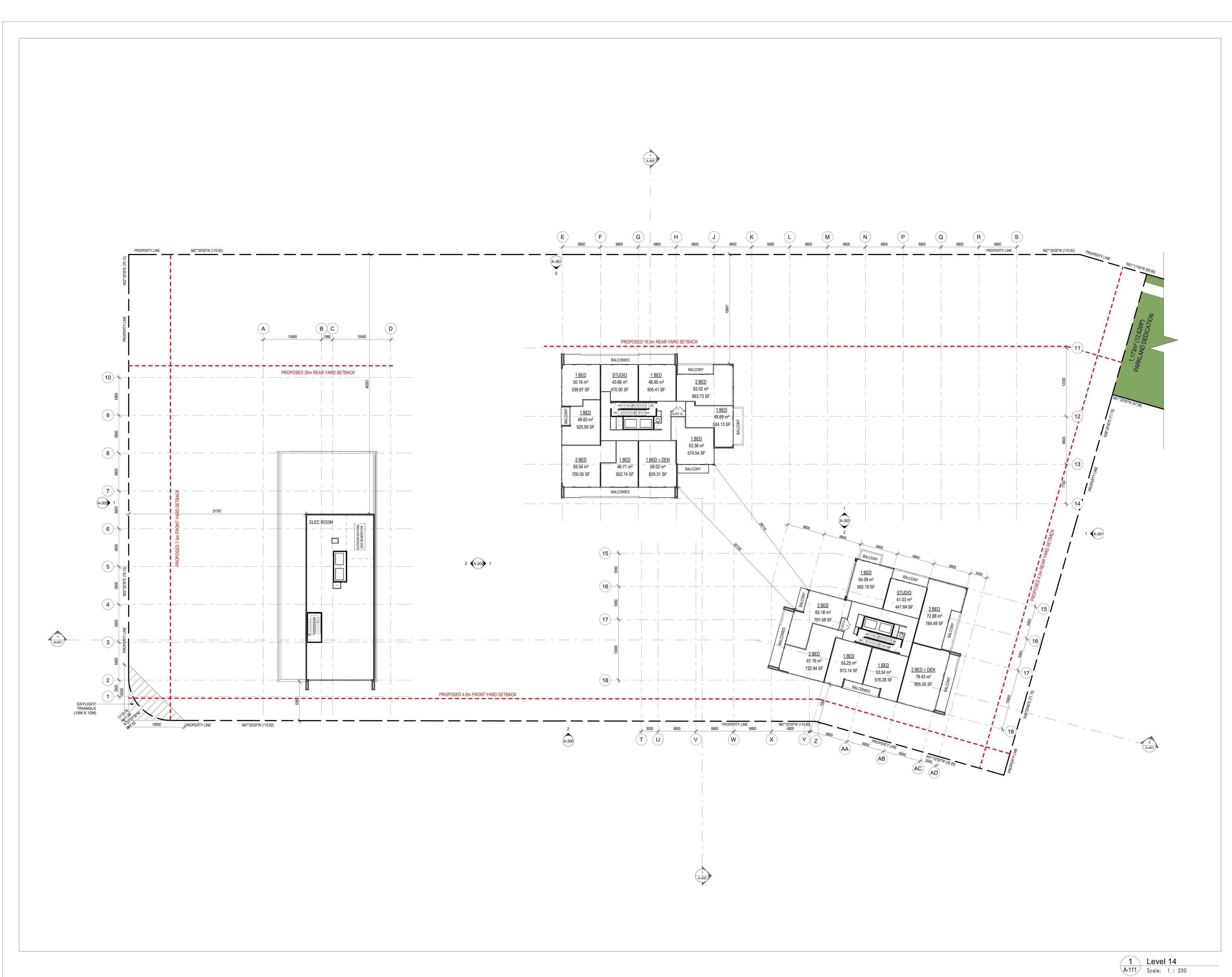
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1 Level 13 A-110 Scale: 1 : 200 A-110

08/08/2024 12:27:45





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Ltd. 3427 Fieldgate Dr.,
Mississauga ON, L4X 2J4

3403-3445 Fieldgate Drive Mississauga, ON

DRAWING

Level 14-18 Typical Overall Floor
Plans

PROJECT NUMBER

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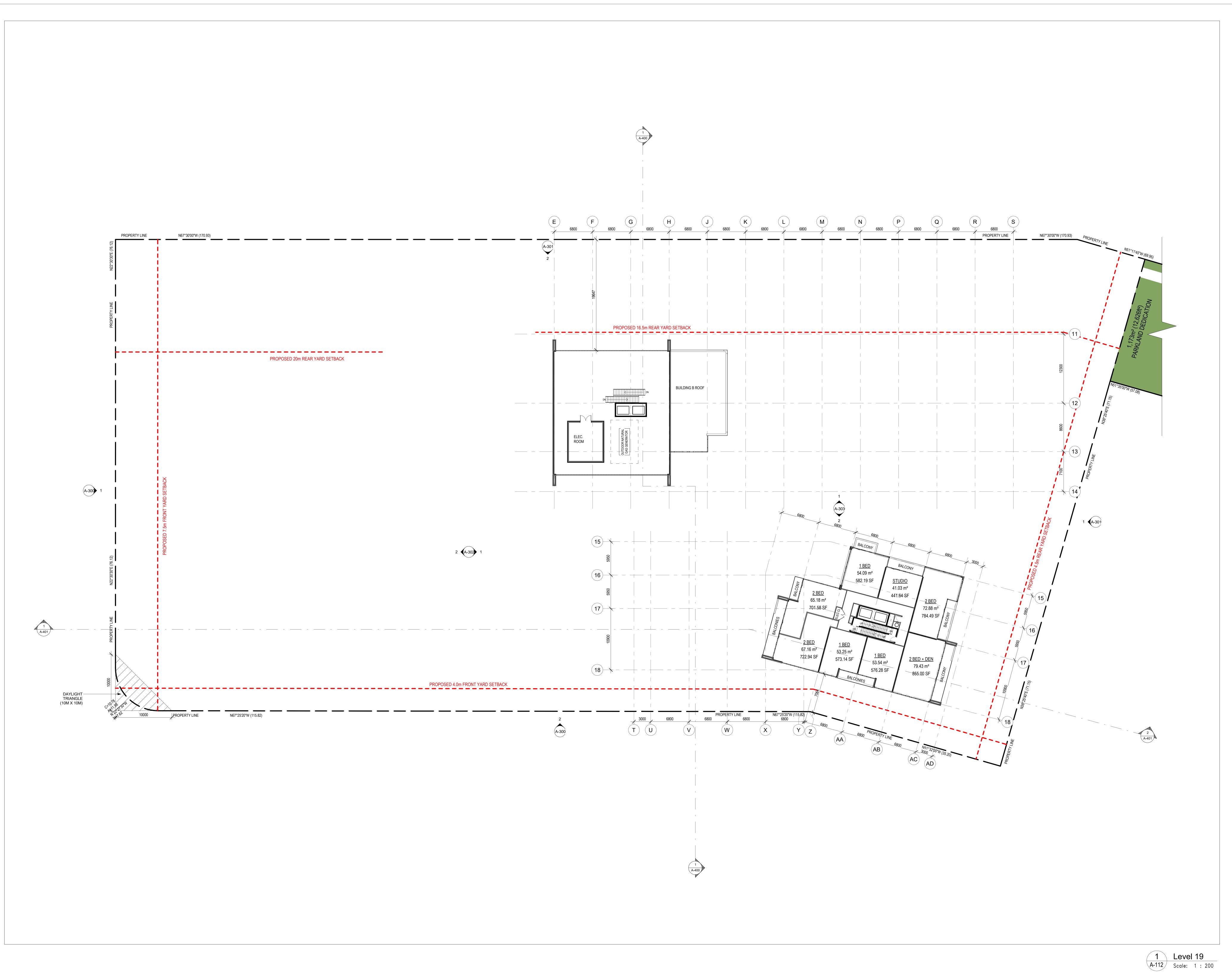
SCALE

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A-111

08/08/2024 12:27:48



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Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4

3403-3445 Fieldgate Drive Mississauga, ON

Level 19-22 Typical Overall Floor Plans

PROJECT NUMBER

23063

SCALE

DATE

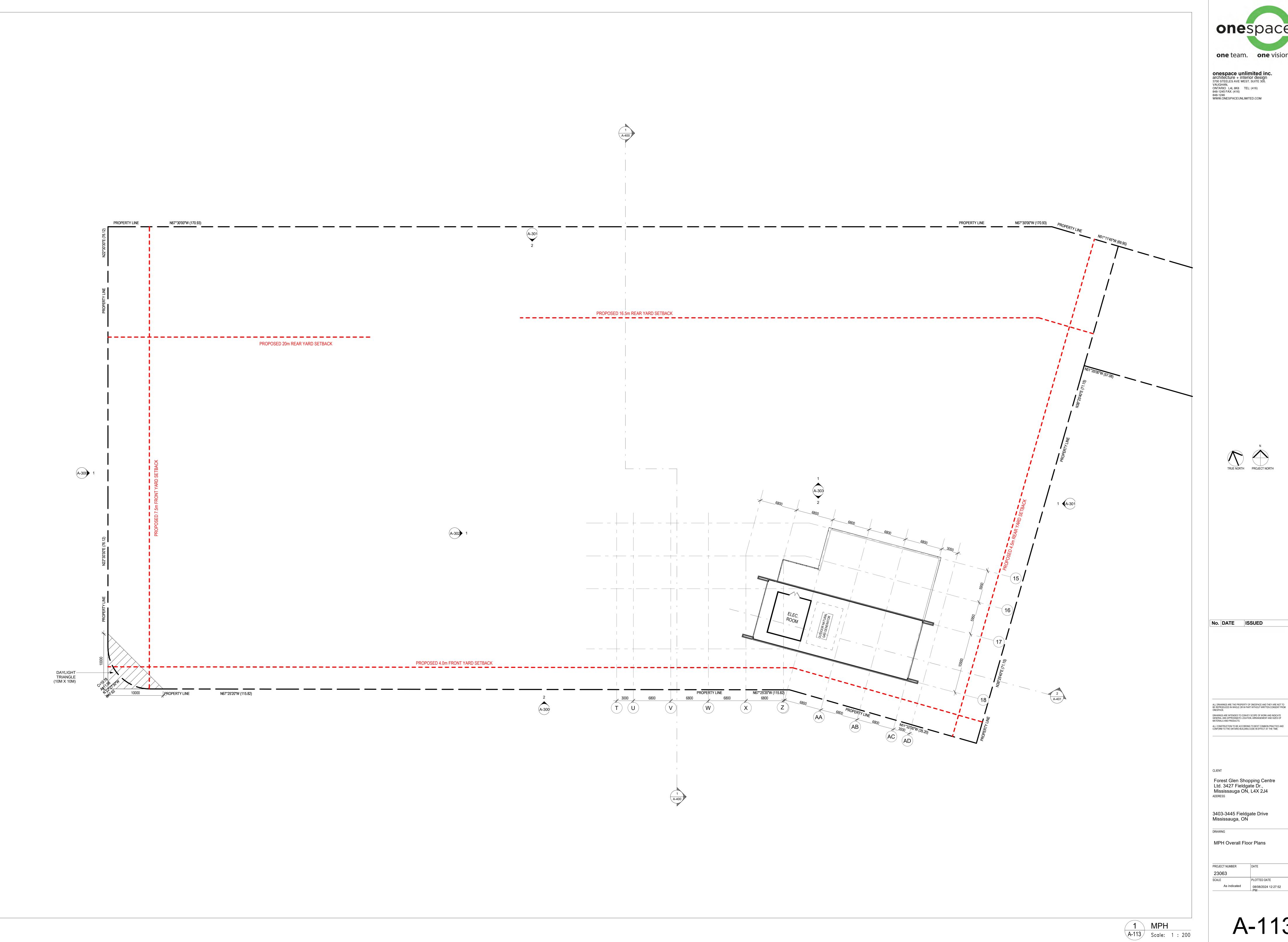
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A-112

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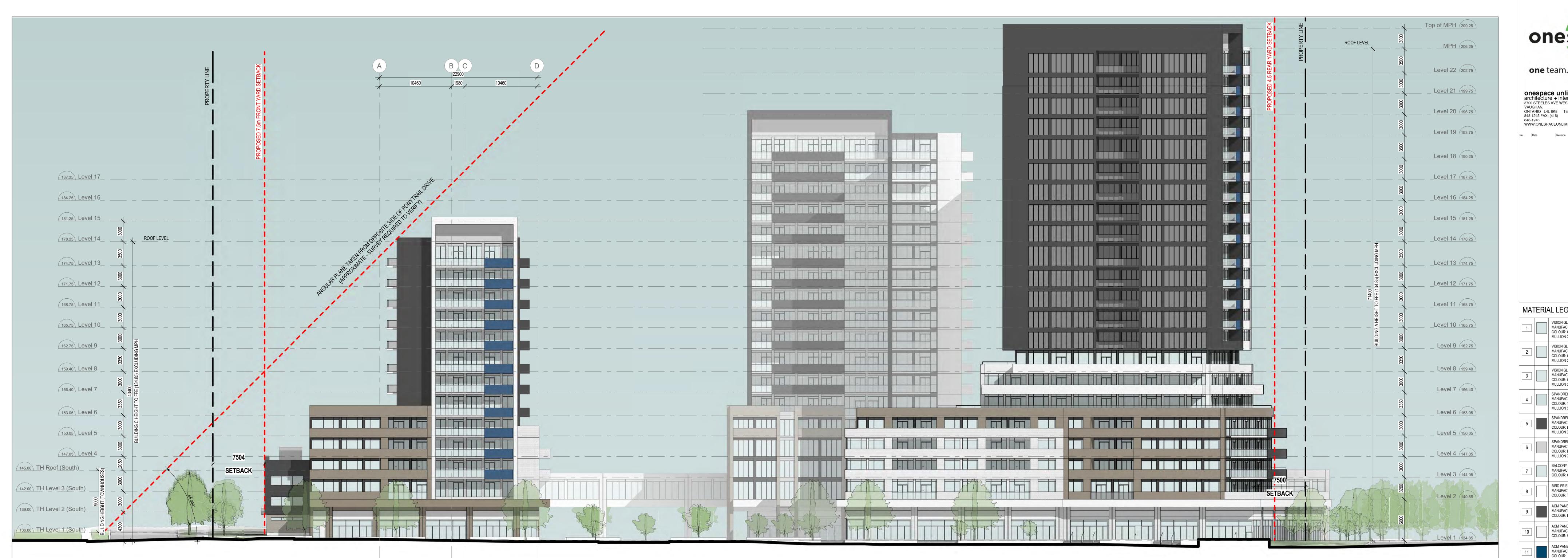


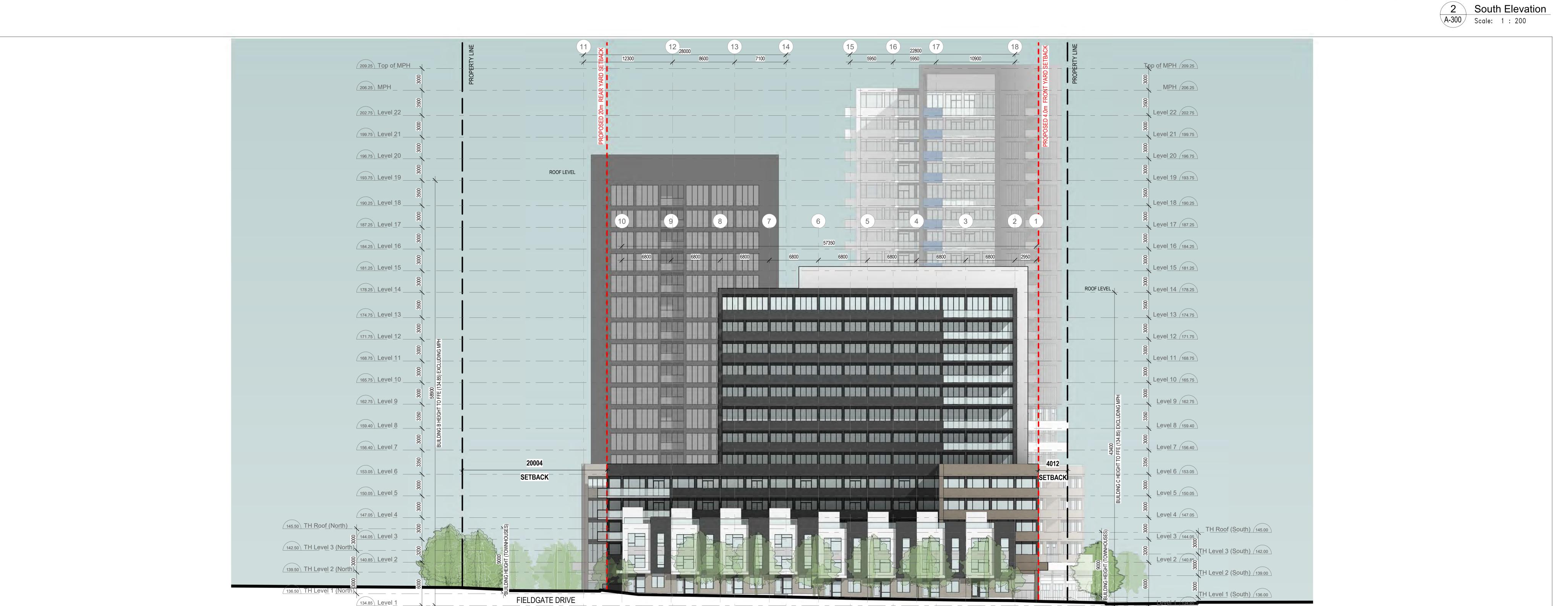
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Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4 ADDRESS 3403-3445 Fieldgate Drive Mississauga, ON MPH Overall Floor Plans

PROJECT NUMBER PLOTTED DATE As indicated 08/08/2024 12:27:52





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MATERIAL LEGEND VISION GLASS 1 MANUFACTURER: TBD COLOUR: CLEAR GLAZING
MULLION COLOUR: WHITE VISION GLASS 2 MANUFACTURER: TBD COLOUR: GLEAR GLAZING MULLION COLOUR: BLACK VISION GLASS 3 MANUFACTURER: TBD COLOUR: GLEAR GLAZING MULLION COLOUR: LIGHT GREY SPANDREL PANEL 1
MANUFACTURER: TBD
COLOUR: TO MATCH GLASS
MULLION COLOUR: WHITE SPANDREL PANEL 2
MANUFACTURER: TBD
COLOUR: BLACK MULLION COLOUR: BLACK SPANDREL PANEL 3
MANUFACTURER: TBD
COLOUR: LIGHT GREY MULLION COLOUR: LIGHT GREY BALCONY RAILING MANUFACTURER: TBD COLOUR: CLEAR GLAZING BIRD FRIENDLY GLAZING MANUFACTURER: TBD COLOUR: TBD ACM PANEL 1 MANUFACTURER: TBD COLOUR: BLACK ACM PANEL 2 MANUFACTURER: TBD COLOUR: WHITE ACM PANEL MANUFACTURER: TBD COLOUR: MASONRY 1
MANUFACTURER: TBD
COLOUR: DARK GREY

MASONRY 2
MANUFACTURER: TBD
COLOUR: LIGHT GREY

MASONRY 3

PRECAST

MANUFACTURER: TBD
COLOUR: WHITE

MANUFACTURER: TBD
COLOUR: WHITE

 002
 07 FEB 2024
 RE-ISSUED FOR DARC MEETING

 001
 25 JAN 2024
 ISSUED FOR DARC MEETING

 NO.
 DATE
 ISSUED

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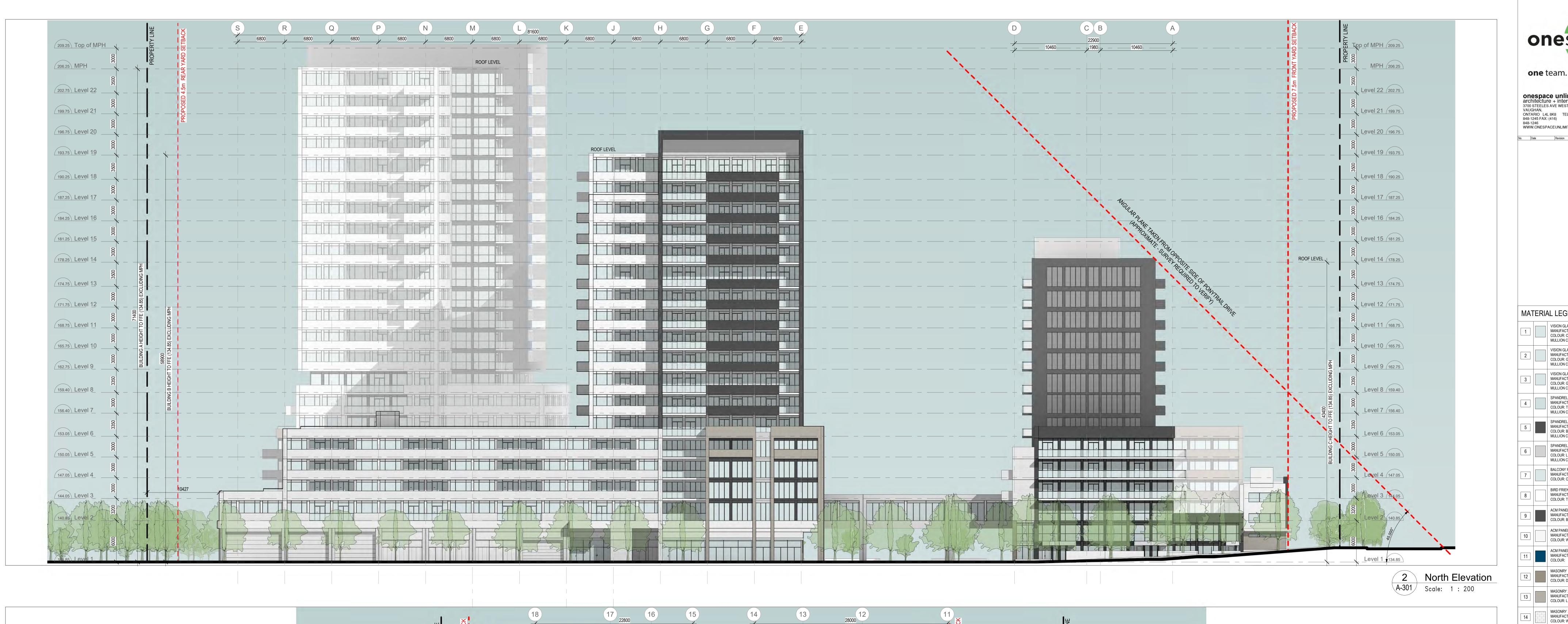
CLIENT Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4

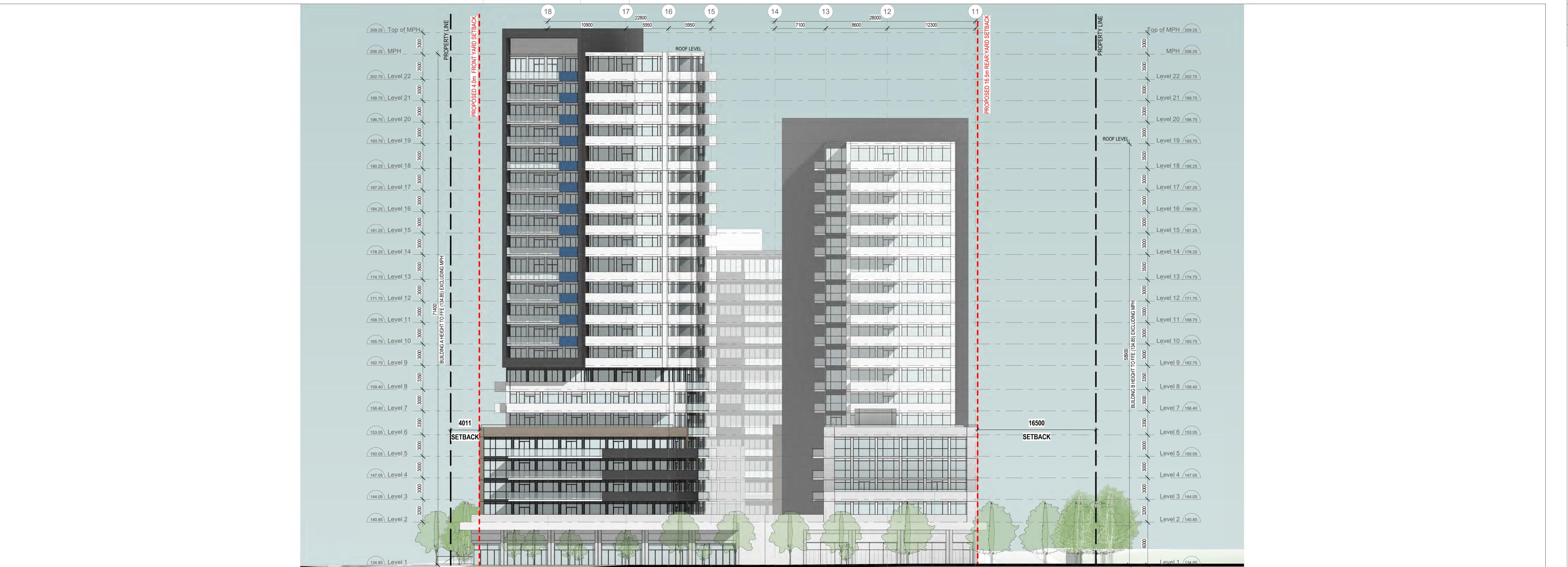
3403-3445 Fieldgate Drive

Mississauga, ON

Building Elevations - South & West Elevations

PROJECT NUMBER 23063 07 FEB 2024 PLOTTED DATE As indicated 08/08/2024 12:33:21





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MATERIAL LEGEND

VISION GLASS 1 MANUFACTURER: TBD COLOUR: CLEAR GLAZING MULLION COLOUR: WHITE VISION GLASS 2

MANUFACTURER: TBD COLOUR: GLEAR GLAZING MULLION COLOUR: BLACK VISION GLASS 3 MANUFACTURER: TBD

COLOUR: GLEAR GLAZING MULLION COLOUR: LIGHT GREY MANUFACTURER: TBD COLOUR: TO MATCH GLASS
MULLION COLOUR: WHITE

SPANDREL PANEL 2 MANUFACTURER: TBD COLOUR: BLACK MULLION COLOUR: BLACK SPANDREL PANEL 3
MANUFACTURER: TBD
COLOUR: LIGHT GREY

MULLION COLOUR: LIGHT GREY MANUFACTURER: TBD COLOUR: CLEAR GLAZING BIRD FRIENDLY GLAZING

MANUFACTURER: TBD

COLOUR: TBD

MANUFACTURER: TBD COLOUR: BLACK ACM PANEL 2

MANUFACTURER: TBD COLOUR: WHITE ACM PANEL MANUFACTURER: TBD COLOUR:

> MASONRY 1 MANUFACTURER: TBD COLOUR: DARK GREY MASONRY 2
> MANUFACTURER: TBD
> COLOUR: LIGHT GREY

MANUFACTURER: TBD
COLOUR: WHITE PRECAST

MASONRY 3

MANUFACTURER: TBD
COLOUR: WHITE

 002
 07 FEB 2024
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 001
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CLIENT

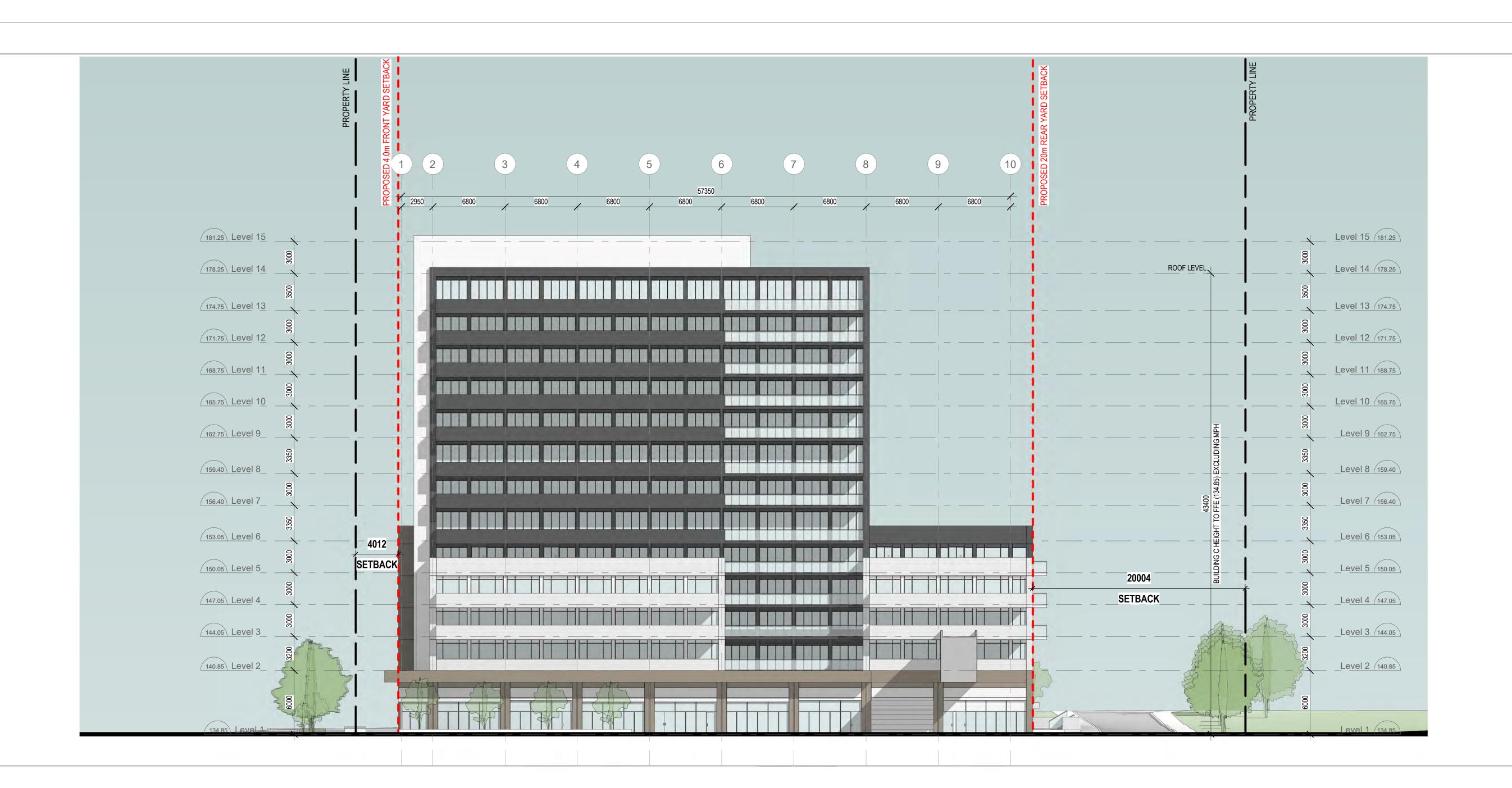
Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4

3403-3445 Fieldgate Drive Mississauga, ON

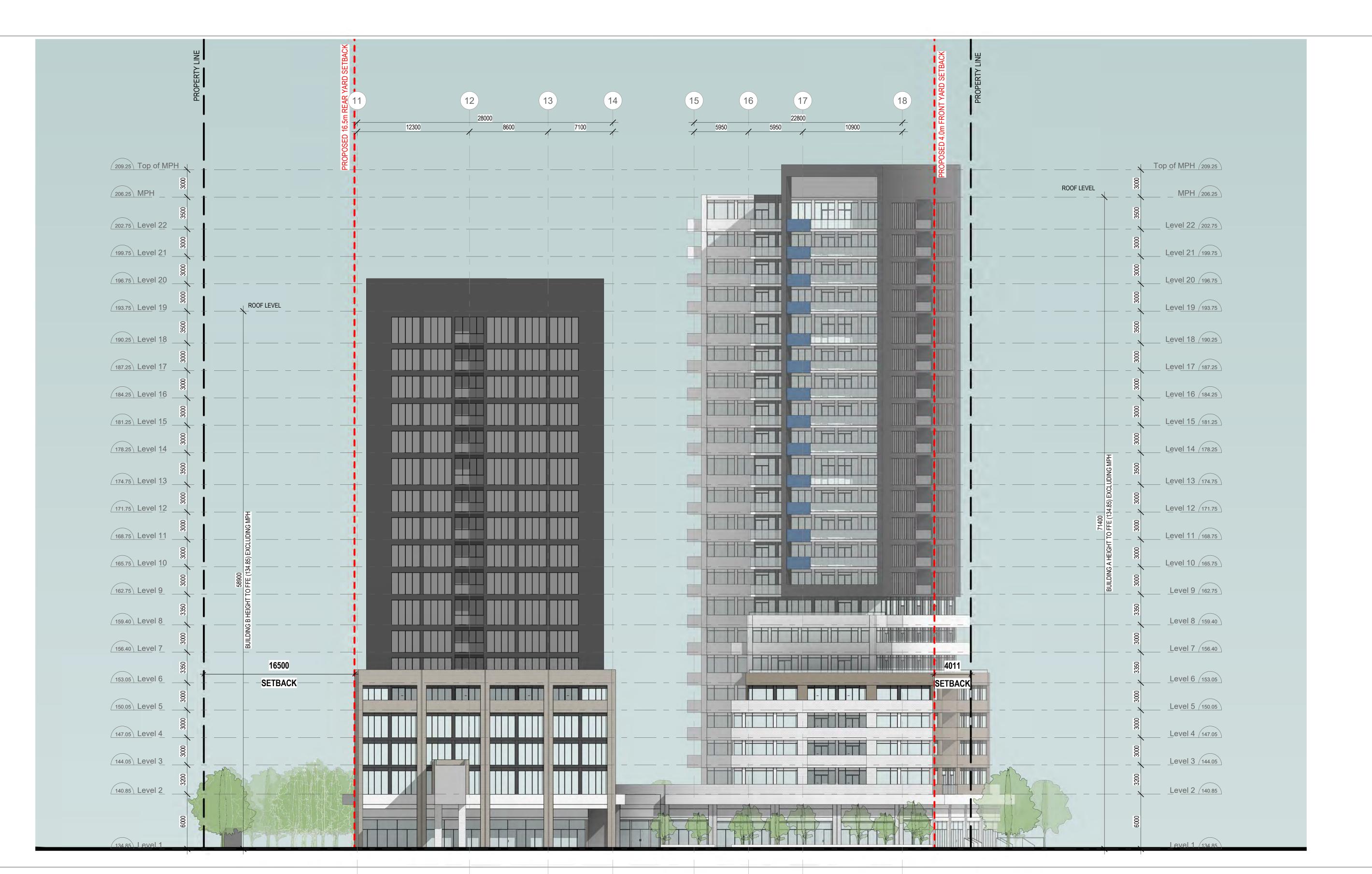
Building Elevations - North & East Elevations

PROJECT NUMBER 23063 07 FEB 2024 PLOTTED DATE As indicated 08/08/2024 12:38:34

1 East Elevation
A-301 Scale: 1:200



2 Building C East Elevation
A-302 Scale: 1: 200



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MATERIAL LEGEND

VISION GLASS 1
MANUFACTURER: TB
COLOUR: CLEAR GLA

VISION GLASS 1
MANUFACTURER: TBD
COLOUR: CLEAR GLAZING
MULLION COLOUR: WHITE

VISION GLASS 2

MULLION COLOUR: BLACK

VISION GLASS 3

MANUFACTURER: TBD

COLOUR: GLEAR GLAZING

MULLION COLOUR: LIGHT GRE

MANUFACTURER: TBD

COLOUR: GLEAR GLAZING

MULLION COLOUR: LIGHT GREY

SPANDREL PANEL 1
MANUFACTURER: TBD
COLOUR: TO MATCH GLASS
MULLION COLOUR: WHITE

SPANDREL PANEL 2

SPANDREL PANEL 2
MANUFACTURER: TBD
COLOUR: BLACK
MULLION COLOUR: BLACK
MULLION COLOUR: BLACK

SPANDREL PANEL 3
MANUFACTURER: TBD
COLOUR: LIGHT GREY

COLOUR: LIGHT GREY
MULLION COLOUR: LIGHT GREY

BALCONY RAILING
MANUFACTURER: TBD
COLOUR: CLEAR GLAZING

BIRD FRIENDLY GLAZING
MANUFACTURER: TBD
COLOUR: TBD

ACM PANEL 1
MANUFACTURER: TBD
COLOUR: BLACK

ACM PANEL 2
MANUFACTURER: TBD
COLOUR: WHITE

ACM PANEL
MANUFACTURER: TBD
COLOUR:

MASONRY 1
MANUFACTURER: TBD
COLOUR: DARK GREY

MASONRY 2
MANUFACTURER: TBD
COLOUR: LIGHT GREY

MASONRY 3
MANUFACTURER: TBD
COLOUR: WHITE

PRECAST
MANUFACTURER: TBD
COLOUR: WHITE

No. DATE ISSUED

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CLIENT
Forest Glen Shopp

Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4 ADDRESS

3403-3445 Fieldgate Drive Mississauga, ON

Building Elevations - Driveway Courtyard

PROJECT NUMBER

23063

SCALE

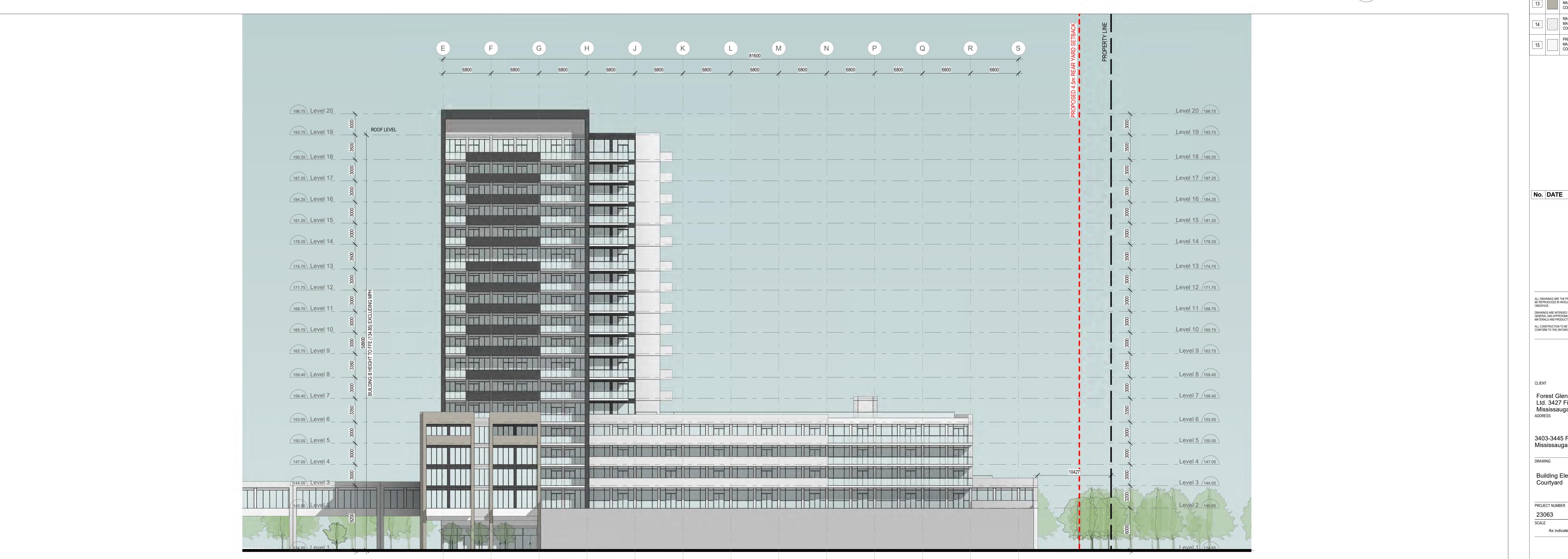
As indicated

08/08/2024 12:41:09

PM

A-302





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MATERIAL LEGEND VISION GLASS 1

MANUFACTURER: TBD COLOUR: CLEAR GLAZING MULLION COLOUR: WHITE VISION GLASS 2 MANUFACTURER: TBD COLOUR: GLEAR GLAZING

MULLION COLOUR: BLACK VISION GLASS 3 MANUFACTURER: TBD COLOUR: GLEAR GLAZING MULLION COLOUR: LIGHT GREY

> SPANDREL PANEL 1
> MANUFACTURER: TBD
> COLOUR: TO MATCH GLASS
> MULLION COLOUR: WHITE SPANDREL PANEL 2
> MANUFACTURER: TBD
> COLOUR: BLACK MULLION COLOUR: BLACK

SPANDREL PANEL 3
MANUFACTURER: TBD
COLOUR: LIGHT GREY MULLION COLOUR: LIGHT GREY BALCONY RAILING
MANUFACTURER: TBD
COLOUR: CLEAR GLAZING

BIRD FRIENDLY GLAZING MANUFACTURER: TBD
COLOUR: TBD ACM PANEL 1
MANUFACTURER: TBD
COLOUR: BLACK

ACM PANEL 2
MANUFACTURER: TBD
COLOUR: WHITE

ACM PANEL MANUFACTURER: TBD COLOUR: MASONRY 1
MANUFACTURER: TBD
COLOUR: DARK GREY

MASONRY 2
MANUFACTURER: TBD
COLOUR: LIGHT GREY MASONRY 3 MANUFACTURER: TBD COLOUR: WHITE

PRECAST
MANUFACTURER: TBD
COLOUR: WHITE

No. DATE ISSUED

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Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4

3403-3445 Fieldgate Drive Mississauga, ON

Building Elevations - Amenity Courtyard

23063 PLOTTED DATE As indicated 08/08/2024 12:43:35

209.25 Top of MPH

206.25 MPH

202.75\ Level 22

199.75\ Level 21

196.75 Level 2<u>0</u>

193.75 Level 19

190.25 Level 18

187.25\ Level 17

184.25\ Level 16

181.25 Level 15

178.25\ Level 14

174.75 Level 13

171.75 Level 12

168.75\ Level 11

165.75 Level 10

162.75 Level 9\_

159.40\ Level 8\_

156.40 Level 7\_

153.05 Level 6\_

150.05 Level 5\_

147.05 Level 4\_

144.05 Level 3\_

140.85 Level 2\_

134.85\ Level 1

130.35\ P1 \_\_\_\_\_

127.35 P2 \_ \_

ADJACENT BUILDING

-SETBACK -

PARKING

PARKING

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RESERVED

\_\_\_\_\_ Level 21 199.75 \_\_\_\_ Level 19 193.75 \_\_\_\_\_ <u>Level 18 (190.25</u> \_\_\_\_\_ <u>Level 17 (187.25</u> \_\_\_\_\_ <u>Level 16</u> 184.25 SUITE SUITE \_ \_\_\_ <u>Level 15 (181.25</u> SUITE \_\_\_\_\_ Level 14 (178.25) SUITE SUITE \_\_\_\_\_\_Level 13 (174.75) No. DATE ISSUED SUITE SUITE \_\_\_\_\_ Level 12 (171.75) SUITE Level 11 (168.75) SUITE Level 10 165.75 SUITE SUITE \_Level 9 (162.75) SUITE SUITE \_Level 8 (159.40) ALL DRAWINGS ARE THE PROPERTY OF ONESPACE AND THEY ARE NOT TO BE REPRODUCED IN WHOLE OR IN PART WITHOUT WRITTEN CONSENT FROM DRAWINGS ARE INTENDED TO CONVEY SCOPE OF WORK AND INDICATE GENERAL AND APPROXIMATE LOCATION, ARRANGEMENT AND SIZES OF MATERIALS AND PRODUCTS. SUITE SUITE \_Level 7 156.40 ALL CONSTRUCTION TO BE ACCORDING TO BEST COMMON PRACTICE AND CONFORM TO THE ONTARIO BUILDING CODE IN EFFECT AT THE TIME. SUITE SUITE \_Level 6 153.05 SUITE SUITE SUITE SUITE \_Level 5 150.05 SUITE SUITE SUITE SUITE \_Level 4 (147.05) CLIENT Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4 SUITE SUITE SUITE SUITE \_Level 3 144.05 SUITE SUITE SUITE \_Level 2 140.85 3403-3445 Fieldgate Drive Mississauga, ON ELEVATED WALKWAY COMMUNITY SPACE LOBBY LOBBY COMMERCIAL **Building Sections** PARKING \_ P1 (130.35) PROJECT NUMBER 23063 PARKING PARKING P2 (127.35) SCALE PLOTTED DATE 1:200 08/08/2024 12:43:49

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RESERVED

MPH 206.25 Level 22 (202.75) Level 20 (196.75) SUITE Level 19 (193.75) SUITE Level 18 190.25 Level 17 (187.25) SUITE Level 16 (184.25) SUITE SUITE 181.25 Level <u>1</u>5 Level 15 (181.25) 178.25 Level <u>1</u>4 \_\_\_\_\_ Level 14 (178.25) SUITE SUITE Level 13 (174.75) SUITE SUITE Level 12 (171.75) 171.75 Level <u>12</u> Level 11 (168.75) No. DATE ISSUED 168.75 Level 11 \_\_\_\_\_ 165.75 Level 10 \_\_\_\_\_\_ Level 10 (165.75) ADJACENT BUILDING SUITE SUITE SUITE 162.75 Level 9 \_\_\_\_\_\_ ADJACENT BUILDING SUITE SUITE SUITE SUITE SUITE SUITE 159.40 Level <u>8</u> Level 8 (159.40) SUITE SUITE SUITE SUITE SUITE SUITE 156.40 Level <u>7</u> Level 7 (156.40) ALL DRAWINGS ARE THE PROPERTY OF ONESPACE AND THEY ARE NOT TO BE REPRODUCED IN WHOLE OR IN PART WITHOUT WRITTEN CONSENT FROM SUITE SUITE SUITE SUITE SUITE SUITE SUITE 153.05 Level <u>6</u> Level 6 (153.05) TERRACE ROOFTOP DRAWINGS ARE INTENDED TO CONVEY SCOPE OF WORK AND INDICATE GENERAL AND APPROXIMATE LOCATION, ARRANGEMENT AND SIZES OF MATERIALS AND PRODUCTS. SUITE SUITE SUITE SUITE SUITE CORRIDOR SUITE STORAGE 150.05 Level <u>5</u> ALL CONSTRUCTION TO BE ACCORDING TO BEST COMMON PRACTICE AND CONFORM TO THE ONTARIO BUILDING CODE IN EFFECT AT THE TIME. Level 5 (150.05) SUİTE SUITE SUITE SUITE SUITE CORRIDOR Level 4 (147.05) 147.05 Level 4 \_\_\_\_\_ \_ 145.00 TH Roof (South) SUİTE SETBACK SUITE SUITE SUITE CORRIDOR STORAGE -SETBACK Level 3 (144.05) 142.00 TH Level 3 (South) SUITE SUITE SUITE SUITE SUITE STORAGE Level 2 (140.85) Forest Glen Shopping Centre Ltd. 3427 Fieldgate Dr., Mississauga ON, L4X 2J4 139.00 TH Level 2 (South) ELEVATED WALKWAY ELEVATED WALKWAY ELEVATED WALKWAY COMMERCIAL COMMERCIAL 136.00 TH Level 1 (South) COMMERCIAL COMMERCIAL COMMERCIAL COMMERCIAL Level 1 (134.85) 3403-3445 Fieldgate Drive Mississauga, ON PARKING PARKING 130.35 P1 P1 /130.35 **Building Sections** PARKING PARKING 127.35\ P2 P2 127.35 PROJECT NUMBER 1 A-400 23063 SCALE 1:200

2 Building C & A

A-401 Scale: 1 : 200

PLOTTED DATE

08/08/2024 12:44:06







# **CRITERIA**

## **Transportation Sources**

Guidance from the Ontario Ministry of the Environment, Conservation and Parks (MECP) NPC-300 Environmental Noise Guideline was used to assess environmental noise generated by transportation-related sources. There are three aspects to consider, which include the following:

- Transportation source sound levels in indoor living areas (living rooms and sleeping quarters), which
  determines building façade elements (windows, exterior walls, doors) sound insulation design
  recommendations.
- ii. Transportation source sound levels at the plane of the window, which determines air-conditioning and ventilation system recommendations and associated warning clauses which inform the future occupants that windows and doors must be closed in order to meet the indoor sound level criteria.
- iii. Transportation source sound levels in Outdoor Living Areas (OLAs), which determines OLA noise mitigation and related warning clause recommendations.

## **Road and Rail**

## **Indoor Sound Level Criteria**

For assessing sound originating from transportation sources, NPC-300 defines sound level criteria as summarized in **Table 1** for indoor areas of sensitive uses. The specified values are maximum sound levels and apply to the indicated indoor spaces with the windows and doors closed.

Table 1: Indoor Sound Level Criteria for Road and Rail Sources

		Sound Level Criteria (Indoors)		
Type of Space	Source	Daytime L <sub>eq,16-hr</sub> 07:00h – 23:00h	Nighttime L <sub>eq,8-hr</sub> 23:00h - 07:00h	
Living Quarters  Examples: Living, dining and den areas of residences, hospitals, nursing homes, schools and daycare centres	Road	45 dBA		
	Rail	40 dBA		
Sleeping Quarters	Road	45 dBA	40 dBA	
Siceping Quarters	Rail	40 dBA	35 dBA	

NPC-300 also provides guidelines for acceptable indoor sound levels that are extended to land uses and developments which are not normally considered noise sensitive. The guideline sound level criteria presented in **Table 2** are provided to inform good-practice design objectives.



Table 2: Supplementary Indoor Sound Level Criteria for Road and Rail Sources

		Sound Level Criteria (Indoors)		
Type of Space	Source	Daytime L <sub>eq,16-hr</sub> 07:00h – 23:00h	Nighttime L <sub>eq,8-hr</sub> 23:00h - 07:00h	
General offices, reception areas, retail stores, etc.	Road	50 dBA	-	
General offices, reception areas, retail stores, etc.	Rail	45 dBA	-	
Theatres, places of worship, libraries, individual or semi-	Road	45 dBA	-	
private offices, conference rooms, reading rooms, etc.	Rail	40 dBA	-	
Sleeping quarters of residences, hospitals,	Road	-	40 dBA	
nursing/retirement homes, etc.	Rail	-	35 dBA	
Sleeping quarters of hotels/motels	Road	-	45 dBA	
Sicephilis qualiters of noters/moters	Rail	-	40 dBA	

## **Outdoor Living Areas (OLAs)**

Outdoor Living Areas (OLAs) would include outdoor areas intended and designed for the quiet enjoyment of the outdoor environment and which are readily accessible from the building.

OLAs may include any common outdoor amenity spaces associated with a multi-unit residential development (e.g. courtyards, roof-top terraces), and/or private backyards and terraces with a minimum depth of 4m provided they are the only outdoor living area for the occupant. The sound level criteria for outdoor living areas is summarized in **Table 3**.

Table 3: Sound Level Criteria - Outdoor Living Area

	Sound Level Criteria (Outdoors)		
Assessment Location	Daytime L <sub>eq,16-hr</sub> 07:00h – 23:00h	Nighttime L <sub>eq,8-hr</sub> 23:00h - 07:00h	
Outdoor Living Area (OLA) (Combined Road and Rail)	55 dBA	-	

#### **Outdoor and Plane of Window Sound Levels**

In addition to the sound level criteria, noise control measures and requirements for ventilation and warning clauses requirements are recommended for residential land-uses based on predicted transportation source sound levels incident in the plane of window at bedrooms and living/dining rooms, and/or at outdoor living areas. These recommendations are summarized in **Table 4** below.

## rwdi.com



Table 4: Ventilation, Building Component, and Warning Clauses Recommendations for Road/Rail Sources

	I	nd Level (Outdoors)	
Assessment Location	Daytime L <sub>eq,16-hr</sub> Nighttime L <sub>eq,8-hr</sub> 07:00h – 23:00h 23:00h – 07:00h		Recommendations
			Installation of air conditioning to allow windows to remained closed.
wok	> 65 dBA	> 60 dBA	The sound insulation performance of building components must be specified and designed to meet the indoor sound level criteria.
Winc ad)			Warning clause "Type D" is recommended.
Plane of Window (Road)	> 55 dBA	> 50 dBA	Applicable for low and medium density development: Forced-air ventilation system to allow for the future installation of air-conditioning. Warning clause "Type C" is recommended.
			Applicable for high density development: Air conditioning to allow windows to remained closed. Warning clause "Type D" is recommended.
Plane of Window (Rail <sup>1, 2</sup> )	> 60 dBA	> 55 dBA	The acoustical performance of building façade components should be specified such that the indoor sound level limits are predicted to be achieved.
e of Wind (Rail <sup>1, 2</sup> )			Warning clause "Type D" is recommended.
Plane (1	> 60 dBA (L <sub>eq, 24hr</sub> ) and		Exterior walls consisting of a brick veneer or masonry equivalent for the first row of dwellings.
	< 100m fr	om tracks	Warning clause "Type D" is recommended.
li 3)	≤ 60 dBA	-	If sound levels are predicted to exceed 55 dBA, but are less than 60 dBA, noise controls may be applied to reduce the sound level to 55 dBA.
Living Area :oad and Rail <sup>3</sup> )	> 55 dBA		If noise control measures are not provided, a warning clause "Type A" is recommended.
Outdoor Living (Combined Road a			Noise controls (barriers) should be implemented to meet the 55 dBA criterion.
	> 60 dBA	-	If mitigation is not feasible to meet the 55 dBA criterion for technical, economic or administrative reasons, an exceedance of 5 dB may be acceptable (to a maximum sound level of 60 dBA). In this case a warning clause "Type B" would be recommended.

## Note(s):

- 1. Whistle noise is included (if applicable) in the determination of the sound level at the plane of window.
- 2. Some railway companies (e.g. CN, CP) may require that the exterior walls include a brick veneer or masonry equivalent for the façade facing the railway line, regardless of the sound level.
- 3. Whistle noise is not included in the determination of the sound level at the OLA.



## **Rail Layover Sites**

NPC-300 provides a sound level limit for rail layover sites to be the higher of the background sound level or 55 dBA Leq,1-hr, for any one-hour period.

#### **Rail Vibration Criteria**

An assessment of rail vibration is generally recommended for developments within 75m of a rail corridor or rail yard, and adjacent to or within a setback of 15m of a transit (subway or light-rail) rail line.

The generally accepted vibration criterion for sensitive land-uses is the threshold of perception for human exposure to vibration, being a vibration velocity level of 0.14 mm/s RMS in any one-third octave band centre frequency in the range of 4 Hz to 200 Hz.

This vibration criterion is based on a one-second exponential time-averaged maximum hold root-mean-square (RMS) vibration velocity level and is consistent with the Railway Associations of Canada (RAC, 2013) guideline, the U.S. Federal Transit Authority (FTA, 2018) criterion for residential land-uses, the Toronto Transit Commission (TTC) guidelines for the assessment of potential vibration impact of future expansion (MOEE/TTC, 1993).

## Aircraft

Land-use compatibility in the vicinity of airports is addressed in Ministry of the Environment, Conservation, and Parks (MECP) Guideline NPC-300 (MOE, 2013). The guideline provides recommendations for ventilation, and noise control for different Noise Exposure Forecast (NEF) values, which would be based on NEF contour maps available from the airport authority. The NEF values can be expressed as  $L_{A,eq,24hr}$  sound levels by using the expression NEF =  $L_{Aeq,24hr}$  -32 dBA.

Table 5: Indoor Sound Level Criteria for Aircraft Sources

Assessment Location	Indoor Sound Level Criteria NEF (L <sub>eq, 24hr</sub> ) <sup>1</sup>
Living/dining/den areas of residences, hospitals, schools, nursing/retirement homes, daycare centres, etc.	NEF- 5 (37 dBA)
Sleeping quarters	NEF-0 (32 dBA)

NPC-300 also provides guidelines for acceptable indoor sound levels that are extended to land uses and developments which are not normally considered noise sensitive. The guideline sound level criteria presented in **Table 6** are provided to inform good-practice design objectives.

**Table 6: Supplementary Indoor Sound Level Criteria for Aircraft Sources** 

Assessment Location	Indoor Sound Level Criteria <sup>1</sup>
General offices, reception areas, retail stores, etc.	NEF-15 (47 dBA)
Individual or semi-private offices, conference rooms, etc.	NEF-10 (42 dBA)
Sleeping quarters of hotels/motels, theatres, libraries, places of worship, etc.	NEF-5 (37 dBA)



**Table 7: NPC-300 Sound Level Criteria for Aircraft (Outdoors)** 

Assessment Location	Outdoor Sound Level Criteria <sup>1</sup>
Outdoor areas, including OLA	NEF-30 (62 dBA)

Table 8: Ventilation, Building Component, and Warning Clauses Recommendations for Aircraft Sources

lable 8: Ventilation, Building Component, and Warning Clauses Recommendations for Aircraft Sources						
Assessment	Aircraft Sound Level	NPC-300 Requirements				
Location	NEF (L <sub>EQ,24-hr</sub> )	•				
	≥NEF 30	Air conditioning to allow windows to remained closed.  The sound insulation performance of building components must be specified and designed to meet the indoor sound				
	ZINLI 30	level criteria.  Warning clauses "Type D" and "Type B" are recommended				
Outdoors		The sound insulation performance of building components must be specified and designed to meet the indoor sound level criteria.				
	< NEF 30	Applicable for low and medium density development: Forced-air ventilation system to allow for the future				
	≥ NEF 25	installation of air-conditioning. Warning clause "Type C" is recommended.				
		Applicable for high density development: Air conditioning to allow windows to remained closed. Warning clause "Type D" is recommended.				
	< NEF 25	Further assessment not required				

# **Stationary Sources**

## **NPC-300 Sound Level Criteria – Stationary Sources**

Guidance from the MECP NPC-300 Environmental Noise Guideline is used to assess environmental noise generated by stationary sources, for example industrial and commercial facilities.

Noise from stationary sources is treated differently from transportation sources and requires sound levels be assessed for the predictable worst-case one-hour average sound level ( $L_{eq}$ ) for each period of the day. For assessing sound originating from stationary sources, NPC-300 defines sound level criteria for two types of Points of Reception (PORs): outdoor and plane of window.

The assessment criteria for all PORs is the higher of either the exclusion limit per NPC-300 or the minimum background sound level that occurs or is likely to occur at a POR. The applicable exclusion limit is determined based on the level of urbanization or "Class" of the area. The NPC-300 exclusion limits for continuously operating stationary sources are summarized in **Table 9**.



Table 9: NPC-300 Exclusion Limits - Continuous and Quasi-Steady Impulsive Stationary Sources (LAeq-1hr)

Time	Class 1 Area		Class 2 Area		Class 3 Area		Class 4 Area	
Period	Outdoor	Plane of Window	Outdoor	Plane of Window	Outdoor	Plane of Window	Outdoor	Plane of Window
Daytime 0700-1900h	50 dBA	50 dBA	50 dBA	50 dBA	45 dBA	45 dBA	55 dBA	60 dBA
Evening 1900-2300h	50 dBA	50 dBA	45 dBA	50 dBA	40 dBA	40 dBA	55 dBA	60 dBA
Nighttime 2300-0700h		45 dBA		45 dBA		40 dBA		55 dBA

#### Note(s):

- The applicable sound level criterion is the background sound level or the exclusion limit, whichever is higher. 1.
- 2. Class 1, 2 and 3 sound level criteria apply to a window that is assumed to be open.
- 3.
- Class 4 area criteria apply to a window that is assumed closed. Class 4 area requires formal designation by the land-use planning authority.

  Sound level criteria for emergency backup equipment (e.g. generators) operating in non-emergency situations such as testing or maintenance are 5 dB greater than the applicable sound level criteria for stationary sources.

For impulsive sound, other than quasi-steady impulsive sound, from a stationary source, the sound level criteria at a POR is expressed in terms of the Logarithmic Mean Impulse Sound Level (LLM), and is summarized in **Table** 10.



Table 10: NPC-300 Exclusion Limits - Impulsive Stationary Sources (LLM)

Table 10: NPC-30	Number of	Class 1 and		Class 3		Class 4	Areas
Time Period	Impulses in Period of One-Hour	Outdoor	Plane of Window	Outdoor	Plane of Window	Outdoor	Plane of Window
Daytime (0700-2300h)	9 or more	50 dBAI	50 dBAI	45 dBAI	45 dBAI	55 dBAI	60 dBAI
Nighttime (2300–0700h)	9 of more	-	45 dBAI	-	40 dBAI	-	55 dBAI
Daytime (0700-2300h)	7 to 8	55 dBAI	55 dBAI	50 dBAI	50 dBAI	60dBAI	65 dBAI
Nighttime (2300-0700h)	7 10 8	-	50 dBAI	-	45 dBAI	-	60 dBAI
Daytime (0700-2300h)	5 to 6	60 dBAI	60 dBAI	55 dBAI	55 dBAI	65 dBAI	70 dBAI
Nighttime (2300-0700h)	5 10 6	-	55 dBAI	-	50 dBAI	-	65 dBAI
Daytime (0700-2300h)	4	65 dBAI	65 dBAI	60 dBAI	60 dBAI	70 dBAI	75 dBAI
Nighttime (2300-0700h)	4	-	60 dBAI	-	55 dBAI	-	70 dBAI
Daytime (0700-2300h)	3	70 dBAI	70 dBAI	65 dBAI	65 dBAI	75 dBAI	80 dBAI
Nighttime (2300-0700h)	3	-	65 dBAI	-	60 dBAI	-	75 dBAI
Daytime (0700-2300h)	. 2	75 dBAI	75 dBAI	70 dBAI	70 dBAI	80 dBAI	85 dBAI
Nighttime (2300-0700h)	2	-	70 dBAI	-	65 dBAI	-	80 dBAI
Daytime (0700-2300h)	1	80 dBAI	80 dBAI	75 dBAI	75 dBAI	85 dBAI	90 dBAI
Nighttime (2300-0700h) Note(s):		-	75 dBAI	-	70 dBAI	-	85 dBAI

Note(s):

<sup>.</sup> The applicable sound level criterion is the background sound level or the exclusion limit, whichever is higher.



## **D-Series Guidelines**

The MECP D-series guidelines (MOE, 1995) provide direction for land use planning to maximize compatibility of industrial uses with adjacent land uses. The goal of Guideline D-6 is to minimize encroachment of sensitive land uses on industrial facilities and vice versa, in order to address potential incompatibility due to adverse effects such as noise, odour and dust.

For each class of industry, the guideline provides an estimate of potential influence area and states that this influence area shall be used in the absence of the recommended technical studies. Guideline D-6 also recommends a minimum separation distance between each class of industry and sensitive land uses (see **Table 11**). Section 4.10 of D-6 identifies exceptional circumstances with respect to redevelopment, infill and mixed-use areas. In these cases, the guideline suggests that separation distances at, or less than, the recommended minimum separation distance may be acceptable if a justifying impact assessment is provided.

Table 11: Summary of Guideline D-6

Industry Class	Definition	Potential Influence Area	Recommended Minimum Separation Distance (property line to property line)
Class I	Small scale, self-contained, daytime only, infrequent heavy vehicle movements, no outside storage.	70 m	20 m
Class II	Medium scale, outdoor storage of wastes or materials, shift operations and frequent heavy equipment movement during the daytime.	300 m	70 m
Class III	Large scale, outdoor storage of raw and finished products, large production volume, continuous movement of products and employees during daily shift operations.	1000 m	300 m

Guideline D-6 provides criteria for classifying industrial land uses, based on their outputs, scale of operations, processes, schedule and intensity of operations. **Table 12** provides the classification criteria and examples.



Table 12: Guideline D-6 Industrial Categorization Criteria

Criteria	Class I	Class II	Class III
Outputs	<ul> <li>Sound not audible off property</li> <li>Infrequent dust and/ or odour emissions and not intense</li> <li>No ground-borne vibration</li> </ul>	<ul> <li>Sound occasionally audible off property</li> <li>Frequent dust and/ or odour emissions and occasionally intense</li> <li>Possible ground-borne vibration</li> </ul>	<ul> <li>Sound frequently audible off property</li> <li>Persistent and intense dust and/ or odour emissions</li> <li>Frequent ground-borne vibration</li> </ul>
Scale	<ul> <li>No outside storage</li> <li>Small scale plant or scale is irrelevant in relation to all other criteria</li> </ul>	<ul><li>Outside storage permitted</li><li>Medium level of production</li></ul>	<ul><li>Outside storage of raw and finished products</li><li>Large production levels</li></ul>
Process	<ul> <li>Self-contained plant or building which produces / stores a packaged product</li> <li>Low probability of fugitive emissions</li> </ul>	<ul> <li>Open process</li> <li>Periodic outputs of minor annoyance</li> <li>Low probability of fugitive emissions</li> </ul>	<ul> <li>Open process</li> <li>Frequent outputs of major annoyances</li> <li>High probability of fugitive emissions</li> </ul>
Operation / Intensity	<ul> <li>Daytime operations only</li> <li>Infrequent movement of products and/or heavy trucks</li> </ul>	<ul> <li>Shift operations permitted</li> <li>Frequent movements of products and/or heavy trucks with majority of movements during daytime hours</li> </ul>	<ul> <li>Continuous movement of products and employees</li> <li>Daily shift operations permitted</li> </ul>
Examples	<ul> <li>Electronics Manufacturing</li> <li>Furniture refinishing</li> <li>Beverage bottling</li> <li>Auto parts</li> <li>Packaging services</li> <li>Dairy distribution</li> <li>Laundry and linen supply</li> </ul>	<ul> <li>Magazine printing</li> <li>Paint spray booths</li> <li>Metal command</li> <li>Electrical production</li> <li>Dairy product manufacturing</li> <li>Feed packing plant</li> </ul>	<ul> <li>Paint and varnish manufacturing</li> <li>Organic chemicals manufacturing</li> <li>Breweries</li> <li>Solvent recovery plant</li> <li>Soap manufacturing</li> <li>Metal manufacturing</li> </ul>



# APPENDIX C

	Date:	7-Jun-24		NOISE REPORT FOR PRO	POSED DEVELOPMENT	
	REQ	JESTED BY:	Location:	Bloor St from Fieldgate Dr to Etobicoke	: Creek	
MISSISSAUGA	Name:	Amy Patenaude		G		
Со	mpany:	RWDI				
	Fax#					
	PRE	PARED BY:				
	Name:	Simranpreet Singh				
	Tel#:	905-615-3200 ext.5917	ID#	622		
			ON SITE '	TRAFFIC DATA		
Specific				Street Names		
		Bloor St				
AADT:		20600				
# of Lanes:		4				
% Trucks:		2%				
Medium/Heavy Truck	Ratio:	55/45				
Day/Night Split:		90/10				
Posted Speed Limit:		50 km/hr				
Gradient of Road:		2%				
Ultimate R.O.W.:		30 m				
Comments:		Ultimate Traffic Only (2041)				

From: Brandon Bradt < <a href="mailto:bbradt@cfcrozier.ca">bbradt@cfcrozier.ca</a> Sent: Tuesday, June 4, 2024 11:26 AM

<adhanani@cfcrozier.ca>

Subject: RE: Fieldgate Plaza - Kick-off Meeting

Hey Kathryn,

Please find our data attached for your use. Note that we only have peak period turning movement counts as part of our work (no ATR data), but you can convert those periods into an AADT for your use as needed. Heavy and medium vehicle percentages are included within the attached counts for all movements.

The assumed growth rates were 0% for all roadways except for Burnhamthorpe Road, which is detailed in the below screenshot. Note that these growth rates are compounded annually. Also note that Bloor Street is losing an eastbound and westbound lane along the site frontage due to proposed bike lanes (at least currently) and that's why it has an assumed 0% growth rate.

Table 6: Growth Rate¶

Compounde	Compounded Annual Growth Rate from Existing to 2029a										
Burnhamthorpe Road¤	Eastbound¤	Westbound¤	Č								
AM Peaka	1.0%a	2.0%o	C								
PM Peaka	1.5%a	1.0%o	C								

Note: The growth rate has only been applied to through movements.

The speed limits are as follows in the area:

- Bloor Street: 50 km/h
- Fieldgate Drive: 40 km/h (south of Ponytrail Dr) & 30 km/h (north of Ponytrail Dr)

- Williamsport Drive: unposted, 50 km/h assumed per Mississauga By-law
- Ponytrail Drive: 30 km/h

Let me know if this everything you need or if there's anything else I can assist with!

Cheers,

Brandon

Brandon Bradt, M.Eng. CEM, P.Eng.

Manager (Planning), Transportation

Office: 416.842.0033

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From: Kathryn Kim < Kathryn.Kim@rwdi.com>

**Sent:** Monday, June 3, 2024 5:47 PM

To: Brandon Bradt < <a href="mailto:bbradt@cfcrozier.ca">bbradt@cfcrozier.ca</a>; Morgan Dundas < <a href="mailto:Morgan@sajeckiplanning.com">Morgan@sajeckiplanning.com</a>>
Cc: Natalie Firth < <a href="mailto:Natalie.Firth@rwdi.com">Natalie.Firth@rwdi.com</a>>; Scott Bell < <a href="mailto:Scott.Bell@rwdi.com">Scott.Bell@rwdi.com</a>>; Aarzoo Dhanani

<adhanani@cfcrozier.ca>

Subject: RE: Fieldgate Plaza - Kick-off Meeting

Bicycle %

# Turning Movement Count Location Name: FIELDGATE DR & PONYTRAIL DR Date: Thu, May 09, 2024 Deployment Lead: David Chu

Crozier & Associates ACCOUNTS PAYABLE TORONTO - SELECT PROVINCE -, M1W1Y6 - SELECT COUNTRY -

#### Turning Movement Count (3 . FIELDGATE DR & PONYTRAIL DR) N Approach E Approach W Approach Int. Total Int. Total PONYTRAIL DR FIELDGATE DR FIELDGATE DR (15 min) (1 hr) Start Time Right Left UTurn Right Thru UTurn Thru UTurn Peds Peds Left Peds Approach Total Approach Total Approach Total N:W N:E N:N N: E:N E:W E:E E: W:E W:N W:W W: 07:00:00 07:15:00 07:30:00 07:45:00 08:00:00 08:15:00 08:30:00 08:45:00 09:00:00 09:15:00 09:30:00 09:45:00 \*\*\*BREAK\*\*\* 16:00:00 16:15:00 16:30:00 16:45:00 17:00:00 17:15:00 17:30:00 17:45:00 18:00:00 18:15:00 18:30:00 18:45:00 **Grand Total** Approach% 40.1% 59.9% 0% 33.8% 66.2% 0% 77.4% 22.6% 0% Totals % 9.6% 14.3% 0% 23.9% 12.6% 24.7% 0% 37.3% 30% 8.8% 0% 38.7% Heavy Heavy % 2.3% 3.9% 0% 2.9% 1.8% 0% 1.5% 3% 0% **Bicycles**

# Turning Movement Count Location Name: FIELDGATE DR & PONYTRAIL DR Date: Thu, May 09, 2024 Deployment Lead: David Chu

Crozier & Associates ACCOUNTS PAYABLE TORONTO - SELECT PROVINCE -, M1W1Y6 - SELECT COUNTRY -

				P	eak Hour: 08:00	AM - 09:0	0 AM 0	Weather	: Scatte	red Clouds (8.33 °	C)					
Start Time				<b>proach</b> ΓRAIL DR					oroach GATE DR					proach GATE DR		Int. Total (15 min)
	Right	Left	UTurn	Peds	Approach Total	Right	Thru	UTurn	Peds	Approach Total	Thru	Left	UTurn	Peds	Approach Total	
08:00:00	13	18	0	20	31	13	40	0	0	53	30	8	0	11	38	122
08:15:00	10	16	0	80	26	18	75	0	0	93	66	16	0	30	82	201
08:30:00	14	34	0	12	48	25	37	0	1	62	51	15	0	48	66	176
08:45:00	15	31	0	5	46	26	32	0	0	58	18	8	0	10	26	130
Grand Total	52	99	0	117	151	82	184	0	1	266	165	47	0	99	212	629
Approach%	34.4%	65.6%	0%		-	30.8%	69.2%	0%		-	77.8%	22.2%	0%		-	-
Totals %	8.3%	15.7%	0%		24%	13%	29.3%	0%		42.3%	26.2%	7.5%	0%		33.7%	-
PHF	0.87	0.73	0		0.79	0.79	0.61	0		0.72	0.63	0.73	0		0.65	-
Heavy	1	8	0		9	5	3	0		8	3	3	0		6	
Heavy %	1.9%	8.1%	0%		6%	6.1%	1.6%	0%		3%	1.8%	6.4%	0%		2.8%	-
Lights	51	91	0		142	77	180	0		257	162	44	0		206	
Lights %	98.1%	91.9%	0%		94%	93.9%	97.8%	0%		96.6%	98.2%	93.6%	0%		97.2%	-
Single-Unit Trucks	0	2	0		2	1	0	0		1	1	0	0		1	-
Single-Unit Trucks %	0%	2%	0%		1.3%	1.2%	0%	0%		0.4%	0.6%	0%	0%		0.5%	-
Buses	1	6	0		7	4	3	0		7	2	3	0		5	-
Buses %	1.9%	6.1%	0%		4.6%	4.9%	1.6%	0%		2.6%	1.2%	6.4%	0%		2.4%	-
Bicycles on Road	0	0	0		0	0	1	0		1	0	0	0		0	-
Bicycles on Road %	0%	0%	0%		0%	0%	0.5%	0%		0.4%	0%	0%	0%		0%	-
Pedestrians	-	-	-	113	-	-	-	-	1	-	-	-	-	99	-	-
Pedestrians%	-	-	-	52.1%		-	-	-	0.5%		-	-	-	45.6%		-
Bicycles on Crosswalk	-	-	-	4	-	-	-	-	0	-	-	-	-	0	-	-
Bicycles on Crosswalk%	-	-	-	1.8%		-	-	-	0%		-	-	-	0%		-

Bicycles on Crosswalk%

# Turning Movement Count Location Name: FIELDGATE DR & PONYTRAIL DR Date: Thu, May 09, 2024 Deployment Lead: David Chu

Crozier & Associates ACCOUNTS PAYABLE TORONTO - SELECT PROVINCE -, M1W1Y6 - SELECT COUNTRY -

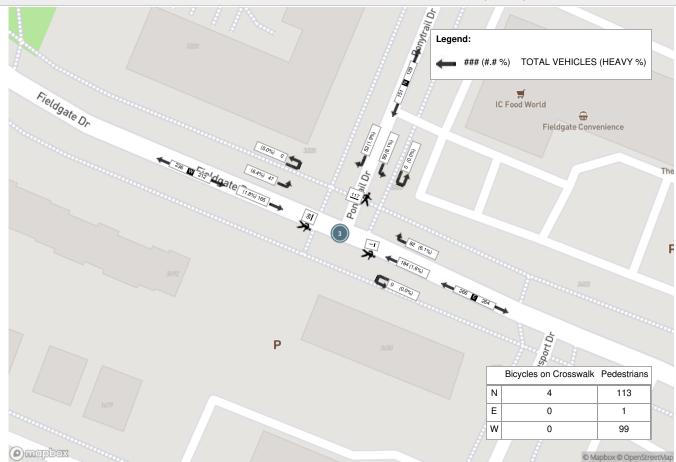
0%

					Peak Hour: 04:45	PM - 05:	45 PM	Weather	: Broker	n Clouds (14.49 °C	<b>;</b> )					
Start Time				roach RAIL DR				oroach GATE DR			<b>W Approach</b> FIELDGATE DR					
	Right	Left	UTurn	Peds	Approach Total	Right	Thru	UTurn	Peds	Approach Total	Thru	Left	UTurn	Peds	Approach Total	
16:45:00	3	18	0	12	21	18	17	0	0	35	37	18	0	9	55	111
17:00:00	11	12	0	4	23	16	25	0	1	41	51	10	0	2	61	125
17:15:00	19	15	0	8	34	23	27	0	0	50	42	14	0	6	56	140
17:30:00	12	19	0	7	31	21	30	0	0	51	49	11	0	1	60	142
Grand Total	45	64	0	31	109	78	99	0	1	177	179	53	0	18	232	518
Approach%	41.3%	58.7%	0%		-	44.1%	55.9%	0%		-	77.2%	22.8%	0%		-	-
Totals %	8.7%	12.4%	0%		21%	15.1%	19.1%	0%		34.2%	34.6%	10.2%	0%		44.8%	-
PHF	0.59	0.84	0		0.8	0.85	0.83	0		0.87	0.88	0.74	0		0.95	-
Heavy	1	0	0		1	2	1	0		3	0	0	0		0	
Heavy %	2.2%	0%	0%		0.9%	2.6%	1%	0%		1.7%	0%	0%	0%		0%	-
Lights	43	64	0		107	75	98	0		173	178	53	0		231	
Lights %	95.6%	100%	0%		98.2%	96.2%	99%	0%		97.7%	99.4%	100%	0%		99.6%	-
Single-Unit Trucks	1	0	0		1	2	1	0		3	0	0	0		0	-
Single-Unit Trucks %	2.2%	0%	0%		0.9%	2.6%	1%	0%		1.7%	0%	0%	0%		0%	-
Buses	0	0	0		0	0	0	0		0	0	0	0		0	-
Buses %	0%	0%	0%		0%	0%	0%	0%		0%	0%	0%	0%		0%	-
Bicycles on Road	1	0	0		1	1	0	0		1	1	0	0		1	-
Bicycles on Road %	2.2%	0%	0%		0.9%	1.3%	0%	0%		0.6%	0.6%	0%	0%		0.4%	-
Pedestrians	-	-	-	31	-	-	-	-	1	-	-	-	-	18	-	-
Pedestrians%	-	-	-	62%		-	-	-	2%		-	-	-	36%		-
Bicycles on Crosswalk	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-	-

0%

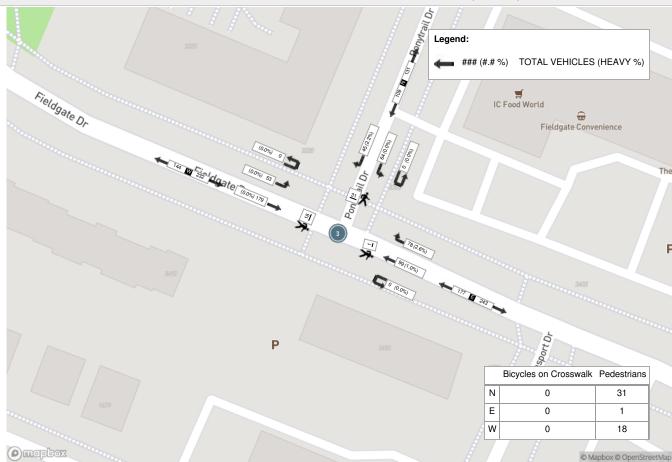
Crozier & Associates ACCOUNTS PAYABLE TORONTO - SELECT PROVINCE -, M1W1Y6 - SELECT COUNTRY -

## Peak Hour: 08:00 AM - 09:00 AM Weather: Scattered Clouds (8.33 °C)





## Peak Hour: 04:45 PM - 05:45 PM Weather: Broken Clouds (14.49 °C)





**ORNAMENT in Cadna and STAMSON Comparison** 

True North Drawn by: JM Figure: C-1

Date Revised: Jun 27, 2024

Approx. Scale: 1:1,200

Map Projection: NAD 1983 UTM Zone 17N 3403-3445 Fieldgate Drive - Mississauga, ON

Project #: 2406207









# NOISE MITIGATION GUIDANCE

## **Acoustic/Noise Barrier**

Generally, noise controls to attenuate transportation sound levels at Outdoor Living Areas (OLAs) would consist of the implementation of acoustic/noise barriers with materials that would meet the guidance included in NPC-300, for example:

- A wall, berm, wall/berm combination or similar structure, used as a noise control measure, and high enough to break the line-of-sight between the source and the receptor.
- The minimum surface density (face weight) is 20 kg/m<sup>2</sup>
  - Many materials could satisfy the surface density requirement, e.g. wood, glass, concrete, Plexiglas, Acrylite.
  - The required thickness can be determined by dividing the 20 kg/m² face weight by the material density (kg/m³). Typically, this would imply:
    - 50 mm (2") thickness of wood
    - 13 mm (0.5") thickness of lighter plastic (like Plexiglas or PVC)
    - 6 mm (0.25") thickness of heavier material (like aluminum, glass, concrete)
- The barrier should be structurally sound, appropriately designed to withstand wind and snow load, and constructed without cracks or surface gaps. Joints between panels may need to be overlapped to ensure surfaces are free of gaps, particularly for wood construction.
- Any gaps under the barrier that are necessary for drainage purposes should be minimized and localized, so that the acoustical performance of the barrier is maintained.
- If a sound absorptive face is to be included in the barrier design, the minimum noise reduction coefficient is recommended to be NRC 0.7.

## **Building Ventilation and Air Conditioning**

The use of air conditioning itself is not a noise control measure; however, it allows for windows and doors to remain closed, thereby reducing the indoor sound levels.

NPC-300 provides the following guidance with respect to implementation of building ventilation and air conditioning:

- a. the noise produced by the proposed ventilation system in the space served does not exceed 40 dBA. In practice, this condition usually implies that window air conditioning units are not acceptable;
- b. the ventilation system complies with all national, provincial and municipal standards and codes;
- c. the ventilation system is designed by a heating and ventilation professional; and
- d. the ventilation system enables the windows and exterior doors to remain closed.

Air conditioning systems also need to comply with Publication NPC-216, and/or any local municipal noise by-law that has provisions relating to air conditioning equipment.



# APPENDIX E



Stationary Sources Location of Stationary Sources in Relation to the Proposed Development

1 Tue North

True North Drawn by: JM Figure: E-1

Approx. Scale: 1:2,000

Date Revised: Jun 27, 2024

KW

Map Projection: NAD 1983 UTM Zone 17N

Project #: 2406207

3403-3445 Fieldgate Drive - Mississauga, ON



# APPENDIX F



# WARNING CLAUSES

Warning clauses are recommended to be included on all development agreements, offers of purchase and agreements of purchase and sale or lease. Warning clauses may be used individually or in combination.

The following warning clauses are recommended based on the applicable guidelines; however, wording may be modified/customized during consultation with the planning authority to best suit the proposed development:

## **Transportation Sources**

**NPC-300 Type A:** Recommended to address surface transportation sound levels in OLAs if sound level is in the range of >55 dBA but  $\le 60$  dBA, and noise controls have <u>not</u> been provided.

"Purchasers/tenants are advised that sound levels due to increasing road traffic (rail traffic) (air traffic) may occasionally interfere with some activities of the dwelling occupants as the sound levels exceed the sound level limits of the Municipality and the Ministry of the Environment."

**NPC-300 Type B:** Recommended to address surface transportation sound levels in OLAs if the sound level is in the range of >55 dBA but  $\leq 60$  dBA, and noise controls have been provided. Recommended to address outdoor aircraft sound levels  $\geq$  NEF 30.

"Purchasers/tenants are advised that despite the inclusion of noise control features in the development and within the building units, sound levels due to increasing road traffic (rail traffic) (air traffic) may on occasions interfere with some activities of the dwelling occupants as the sound levels exceed the sound level limits of the Municipality and the Ministry of the Environment."

**NPC-300 Type C:** Applicable for low and medium density developments only, recommended to address transportation sound levels at the plane of window.

"This dwelling unit has been designed with the provision for adding central air conditioning at the occupant's discretion. Installation of central air conditioning by the occupant in low and medium density developments will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of the Environment."

**NPC-300 Type D:** Recommended to address transportation sound levels at the plane of window.

"This dwelling unit has been supplied with a central air conditioning system which will allow windows and exterior doors to remain closed, thereby ensuring that the indoor sound levels are within the sound level limits of the Municipality and the Ministry of the Environment."



**Proximity to Railway Line:** Metrolinx/CN/CP/VIA Warning Clause for developments that are within 300 metres of the right-of-way

"Warning: [Canadian National Railway Company] [Metrolinx / GO] [Canadian Pacific Railway Company] [VIA Rail Canada Inc.] or its assigns or successors in interest has or have a right-of-way within 300 metres from the land the subject hereof. There may be alterations to or expansions of the rail facilities on such right-of-way in the future including the possibility that the railway or its assigns or successors as aforesaid may expand its operations, which expansion may affect the living environment of the residents in the vicinity, notwithstanding the inclusion of any noise and vibration attenuating measures in the design of the development and individual dwelling(s). CNR/Metrolinx/GO/CPR/VIA will not responsible for any complaints or claims arising from use of such facilities and/or operations on, over or under the aforesaid right-of-way."

## **Stationary Sources**

NPC-300 Type E (Modified): Recommended to address proximity to commercial/industrial land-use

"Purchasers/tenants are advised that due to the proximity of the adjacent commercial land-uses, noise from the commercial land-uses may at times be audible. This dwelling unit has been supplied with a ventilation/air conditioning system which will allow windows and exterior doors to remain closed, protecting the indoor areas."

**NPC-300 Type F:** Recommended to for Class 4 Area Notification

"Purchasers/tenants are advised that sound levels due to the adjacent industry (facility) (utility) are required to comply with sound level limits that are protective of indoor areas and are based on the assumption that windows and exterior doors are closed. This dwelling unit has been supplied with a ventilation/air conditioning system which will allow windows and exterior doors to remain closed."